



Department of Transportation

Office of Project Development

700 E Broadway Avenue

Pierre, South Dakota 57501-2586 605/773-3268

FAX: 605/773-2614

February 25, 2016

ADDENDUM NO. 1

RE: Item #13, March 2, 2016 Letting - IM 0903(102)112, NH 0014(216)112, PCN 03W0, 05EE, Jackson, Pennington County - Cold Milling Asphalt Concrete & Asphalt Concrete Resurfacing

TO WHOM IT MAY CONCERN:

The following addenda to the plans shall be inserted and made a part of your proposal for the referenced project.

SPECIAL PROVISIONS: NO CHANGE

BID ITEM FILE: NO CHANGE

PLANS: Please add Section C.

Sheets 1 through 9: Section C was added.

Sincerely,

Sam Weisgram
Engineering Supervisor

SW/cj

CC: Todd Seaman, Rapid City Region Engineer
Mike Carlson, Rapid City Area Engineer

SECTION C: TRAFFIC CONTROL PLANS

| STATE OF SOUTH DAKOTA | PROJECT | SHEET | TOTAL SHEETS |
|-----------------------|-----------------|-------|--------------|
| | IM 0903(102)112 | C1 | C9 |

Plotting Date: 01/04/2016

INDEX OF SHEETS

- C1 General Layout with Index
- C2-C4 Estimate With General Notes & Tables
- C5 Ramp Resurfacing Details
- C6 Ramp Signing Details
- C7 Crossover Truck Crossing Details
- C8-C9 Standard Plates

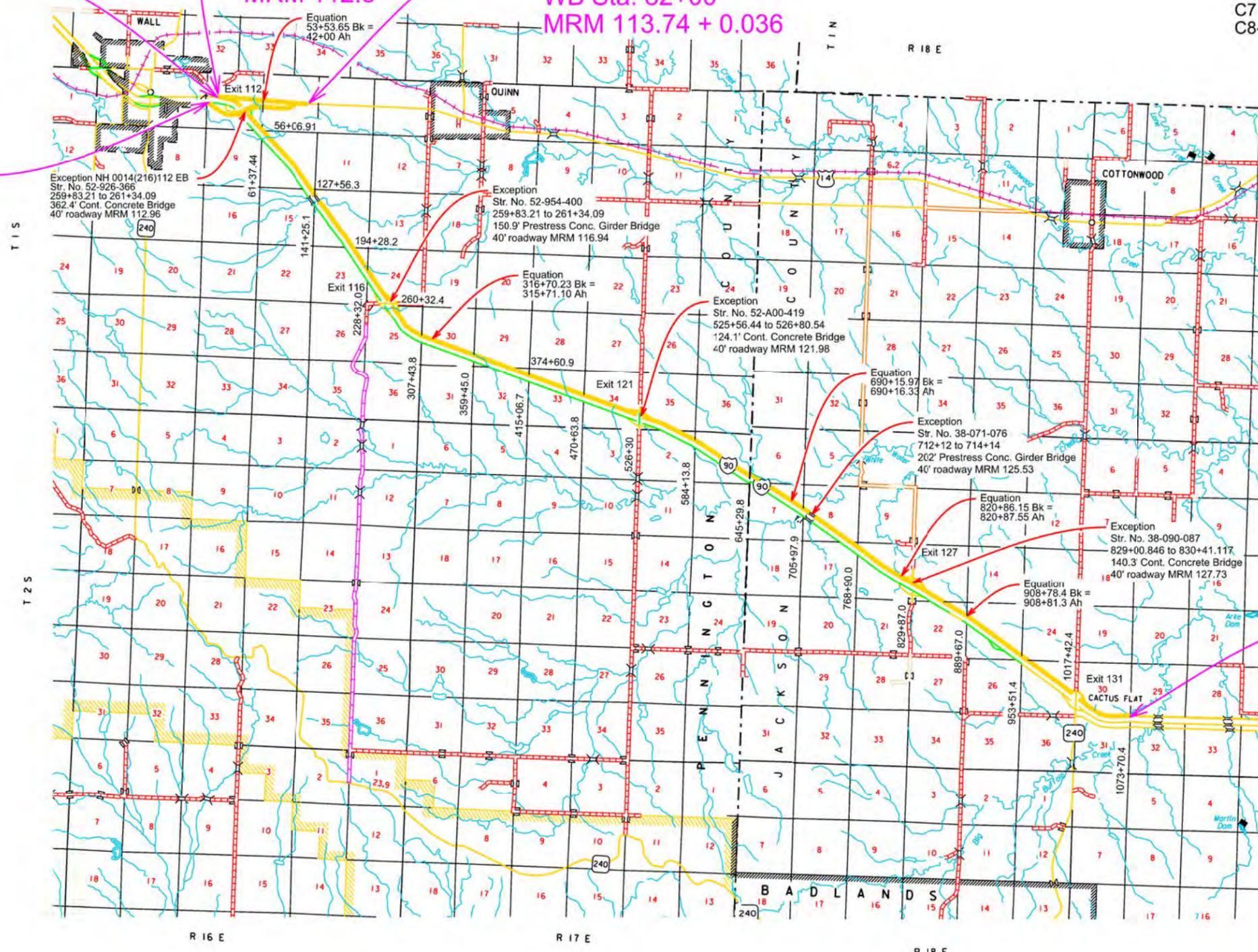
BEGIN PROJECT
IM 0903(102)112
Sta. 8+04.61
MRM 112.12 + 0.180

BEGIN PROJECT
NH 0014(216)112
WB Sta. 8+57
MRM 112.5

END PROJECT
NH 0014(112)
WB Sta. 82+00
MRM 113.74 + 0.036

BEGIN PROJECT
NH 0014(216)112
EB Sta. 8+02.81
MRM 112.3

END PROJECT
IM 0903(102)112
Sta. 1063+89
MRM 132.0 + 0.165



Plotted From: Irrc11640 Plot Scale: 1:200 File: ...Traffic Control\TitleC.dgn

SECTION C - ESTIMATE OF QUANTITIES

PCN 03W0

| BID ITEM NUMBER | ITEM | QUANTITY | UNIT |
|-----------------|---|----------|------|
| 634E0010 | Flagging | 2,000.0 | Hour |
| 634E0110 | Traffic Control Signs | 1,196 | SqFt |
| 634E0120 | Traffic Control, Miscellaneous | Lump Sum | LS |
| 634E0135 | Traffic Control Supervisor | Lump Sum | LS |
| 634E0285 | Type 3 Barricade, 8' Double Sided | 3 | Each |
| 634E0320 | Temporary Flexible Vertical Markers (Tabs) | 19.9 | Mile |
| 634E0420 | Type C Advance Warning Arrow Board | 3 | Each |
| 634E0630 | Temporary Pavement Marking | 19.9 | Mile |
| 634E1215 | Contractor Furnished Portable Changeable Message Sign | 2 | Each |
| 634E1255 | Contractor Furnished Speed Monitoring Radar Trailer | 2 | Each |

PCN 05EE

| BID ITEM NUMBER | ITEM | QUANTITY | UNIT |
|-----------------|--|----------|------|
| 634E0010 | Flagging | 200.0 | Hour |
| 634E0110 | Traffic Control Signs | 436 | SqFt |
| 634E0120 | Traffic Control, Miscellaneous | Lump Sum | LS |
| 634E0135 | Traffic Control Supervisor | Lump Sum | LS |
| 634E0285 | Type 3 Barricade, 8' Double Sided | 2 | Each |
| 634E0320 | Temporary Flexible Vertical Markers (Tabs) | 1.8 | Mile |
| 634E0630 | Temporary Pavement Marking | 1.8 | Mile |

TRAFFIC CONTROL – GENERAL NOTES

1. Requests to deviate from the sequence of operations shall be submitted in writing to the Engineer for review. Approval of an alternate sequence of operations will only be allowed when the proposed changes meet with the Department's intent for traffic control and sequencing of the work. An alternate sequence shall be submitted for review a minimum of one week prior to potential implementation.
2. Unless otherwise stated in these plans, no work will be allowed during hours of darkness.
3. Indiscriminate driving and parking of vehicles within the right-of-way will not be permitted. Any damage of the vegetation, surfacing, embankment, delineators, and existing signs resulting from such indiscriminate use shall be repaired and/or restored by the Contractor, at no expense to the State, and to the satisfaction of the Engineer.
4. Existing guide, route, informational logo, regulatory, warning signs and delineation shall be temporarily reset and maintained during construction as directed by the Engineer. Removing, relocating, salvaging and resetting of the above items shall be the responsibility of the Contractor.

5. Non-applicable traffic control devices shall be completely covered or removed during periods of inactivity. Periods of inactivity shall be defined as no work taking place for a period of more than 48 hours.
6. Construction signing mounted on portable supports shall not be used for a duration of more than 3 days, unless approved by the Engineer. Construction signing that remains in the same location for more than 3 days shall be mounted on fixed location, ground mounted, breakaway supports. All regulatory signs shall have a minimum mounting height of 5' in rural locations, even when mounted on portable supports.
7. Any delineators and signs damaged or lost shall be replaced by the Contractor at no cost to the State.
8. All materials and equipment shall be stored a minimum distance of 30' from the traveled way during nonworking hours.
9. The Contractor shall provide installation details at the preconstruction meeting for all breakaway sign support assemblies.
10. All haul trucks shall be equipped with a second flashing amber light that is visible from the backside of the haul truck. The costs for the flashing amber lights shall be incidental to the various related contract bid items.
11. All construction operations shall be conducted in the general direction of traffic movement.
12. If there is a discrepancy between the traffic control plans, standard plates, and the MUTCD – whichever is more stringent shall be used, as determined by the Engineer.
13. Temporary Flexible Vertical Markers (Tabs) shall be used for lane closure tapers or lane shift tapers and shall be installed at 5' spacing. Tabs used for tapers and shifts will not be measured for payment. All costs associated to furnish, install, maintain (including replacement as required by the Engineer at no added cost to the Department), and remove all markers will be incidental to the contract lump sum price for Traffic Control, Miscellaneous.
14. Drums are required in all lane closure tapers, and temporary gore areas of interchange ramps.
15. Bump Signs (W8-1, black on orange) with appropriate Advisory Speed Plaque (W13-1P, black on orange) shall be placed 500' in advance of the bump or as approved by the Engineer for adequate sight distance. Type I Object Markers (orange - 18"x18") shall be placed at the bump location.
16. The Contractor shall not allow mainline traffic to run on a milled surface at any location on the project for more than 14 calendar days.
17. The Contractor shall place Grooved Pavement (W8-15) with Next XX Miles (W7-3aP) and Motorcycles Use Extreme Caution (black on orange – 48"x48") signs at the end of the project. The Contractor shall also place Grooved Pavement (W8-15) with Motorcycle plaque (W8-15P) signs on every Interchange On-Ramp as the milling operation progresses or as directed by the Engineer.
18. Interstate 90 traffic shall not be stopped at any time.

Revised 1/07/16 - BP

| | | | |
|-----------------------|-----------------|-------|--------------|
| STATE OF SOUTH DAKOTA | PROJECT | SHEET | TOTAL SHEETS |
| | IM 0903(102)112 | C2 | C9 |

19. Interstate 90 shall be kept open to one lane of traffic at all times in each direction.
20. Construction traffic shall only enter and exit Interstate 90 by the use of existing interchanges – see Maintenance Crossover Truck Crossing note.
21. The Contractor shall minimize the time traffic is allowed on the shoulders during all work operations. The Contractor shall have flag persons and workers available to move drums or cones both ahead and behind work operations to minimize the time traffic is allowed on the shoulders
22. The Contractor's employee vehicles shall not be allowed to park on the Interstate median at any time.
23. At no time shall Interstate traffic be exposed to differential elevations in traveling lanes or shoulders due to either the milling or paving operation. If differential elevations will exist, then the lane or lane adjacent to the shoulder shall remain closed. The single exception to allowing differential elevations along a longitudinal joint will be at Interstate Exit and Entrance Ramp merge points.
24. The cold milling operation shall be conducted in a manner that evens up the milling operation full roadway width at the end of each day, i.e. - mill one lane then drop back the same day and mill the adjacent lane.
25. The Class S Asphalt Concrete paving operation shall be conducted such that one lane is paved one day, then the adjacent lane is paved the next day.
26. All transitions shall be paved/milled for a smooth ride as approved by the Engineer.
27. Before a milled section of roadway is opened to traffic a changeable message sign shall be installed in advance of the milled surface advising motorists of the roadway surface conditions. The speed through milled sections of Interstate 90 shall be posted at 65 MPH at the discretion of the Engineer.
28. Exit Gore signs shall be a 7' minimum height to the bottom of the sign.
29. There are enough traffic control signs in the plans for three (3) lane closures on the Interstate, this total includes any lane closures used for maintenance crossover truck crossings. If the Contractors operations require additional lane closures it shall be approved by the Engineer and all costs shall be at the Contractors expense.
30. A 16' width restriction will be put in place on I-90.
31. Ramp work shall be completed ½ at a time to maintain traffic flows.
32. Guardrail replacement shall be completed during paving and milling operations. Section 630.3F of the Specifications shall be adhered to.

SEQUENCE OF OPERATIONS

The Contractor shall provide a Sequence of Operations, at least one week in advance of the preconstruction meeting, to the Engineer for approval. The following requirements/restrictions shall apply:

- Standard Plate 634.63, Ramp Driving Lane Resurfacing, Maintenance Crossover Truck Crossing Detail, and Ramp Entrance and Exit Details shall be used for traffic control.

MAINTENANCE CROSSOVER TRUCK CROSSING

At the discretion of the Engineer, the use of maintenance crossovers will be allowed when the following criteria are met:

- The passing lanes in both directions are closed and signed as per the "Maintenance Crossover Truck Crossing Detail" traffic control sheet.
- Flaggers shall be used to prevent thru traffic from entering the passing lanes used by the turning trucks.
- A maintenance crossover shall not be used if it is within one mile of an existing interchange.
- Traffic shall not be subject to unnecessary weaving when milling and/or paving operations occupy the driving lane. A minimum of one mile shall always be maintained from the end of an active construction work zone (in the driving lane) to the beginning of the lane taper (in the passing lane) for the truck maintenance crossover.
- All maintenance crossover use is subject to approval by the Engineer and will not be allowed if deemed unsafe for the physical conditions and traffic.
- All damage to the maintenance crossovers shall be repaired after mainline paving is completed. Payment for the required repairs will be made under the appropriate contract items.

TRAFFIC CONTROL SUPERVISOR

The Contractor shall designate an individual, other than the project superintendent, to be the Traffic Control Supervisor. The Traffic Control Supervisor shall be certified as a worksite traffic control supervisor by the American Traffic Safety Services Association (ATSSA) and shall have a current SD DOT flagger certification card. The Traffic Control Supervisor shall be familiar with the requirements of the Department traffic control plans and specifications. The Traffic Control Supervisor shall also have at least 12 months experience with traffic control plans, layouts, and maintenance. A copy of the Traffic Control Supervisor's certifications, flagger certification, and work experience shall be provided to the Engineer at the preconstruction meeting.

The Traffic Control Supervisor (TCS) shall be onsite and supervise all significant changes to the traffic patterns, which include but are not limited to, detour setups, changes to access points, pedestrian access routes, and changes to lane closures.

The TCS or an ATSSA certified Traffic Control Technician (TCT) shall be onsite and perform the maintenance of all traffic control on a daily basis.

The TCS or TCT shall be available 24 hour/day, 7 days/week to perform maintenance of the traffic control. The names and cellular telephone

numbers of these individuals shall be given to the Engineer at the preconstruction conference.

The TCS or TCT shall make night inspections at the initial set up of traffic control and every week thereafter to ensure the adequacy, legibility and reflectivity of traffic control. A written summary of each inspection shall be given to the Engineer within 24 hours after completion of the inspection. Items identified on the inspection report that require maintenance shall be performed within 24 hours after receiving the inspection report. Critical items requiring maintenance shall be completed during the inspection.

If a TCT performs the duties listed above, a copy of the TCT's certifications and SD DOT flagger certification shall be provided to the Engineer at the preconstruction meeting.

The TCS shall supervise the TCT. There will be no separate payment for the TCT, if used.

All costs associated with all work noted above shall be incidental to the contract lump sum price for "Traffic Control Supervisor".

TEMPORARY PAVEMENT MARKING (PAINT)

Temporary pavement marking paint shall be used on cold milled surfaces. Temporary pavement marking paint shall be used to mark centerline skips, edgelines (including ramps), lane lines, and gore lines for the entire project length on both the milled surface and blade laid surface. Temporary pavement marking paint shall not be used on the top lift of asphalt concrete.

The Contractor shall be responsible for maintaining visible and reflective pavement markings throughout the project. Any pavement marking covered or damaged shall be replaced prior to the end of the day.

Quantity of Temporary Pavement Marking (Paint) consists of:

- One pass on top of the milled surface on I-90 – 19.9 miles
- One pass on top of the milled surface on Hwy 14 – 1.8 miles

TEMPORARY FLEXIBLE VERTICAL MARKERS (TABS)

Temporary Flexible Vertical Markers (Tabs) shall be used on the top lift of asphalt surfacing. Temporary Flexible Vertical Markers (Tabs) shall be used to mark centerline skips, edgelines (including ramps), lane lines, and gore lines for the entire project length. Three tabs in width shall be used to delineate the 12" gore markings.

The Contractor shall be responsible for maintaining visible and reflective pavement markings throughout the project. Any marking covered or damaged shall be replaced prior to the end of the day at no cost to the State.

All costs for furnishing, installing, maintaining, and removing the tabs when no longer needed shall be included in the contract unit price per mile for "Temporary Pavement Marking".

Quantity of Temporary Flexible Vertical Markers (Tabs) consists of:

- Tabs installed on the top lift of asphalt on I-90 – 19.9 miles
- Tabs installed on the top lift of asphalt on Hwy 14 – 1.8 miles

CONTRACTOR FURNISHED SPEED MONITORING RADAR TRAILER

The Contractor shall provide 2 speed trailers to monitor traffic speeds on designated routes at locations specified in the field by the Engineer.

The radar speed feedback sign assembly shall include a speed limit sign mounted in conjunction with the radar speed feedback display. The speed display shall not flash either the vehicle speeds exceeding the speed limit or any other messages.

All costs associated with furnishing, maintaining, transporting, relocating if necessary, and removing the speed trailers from locations specified by the Engineer shall be included in the contract unit price per each for Contractor Furnished Speed Monitoring Radar Trailer.

PRESS RELEASE ANNOUNCEMENTS

The SDDOT will prepare a Press Release to be released 5 days prior to any phase change or any other major change that affects traffic flow. The SDDOT will be responsible to keep law enforcement, emergency services, and the traveling public notified of changes in project access. The Contractor shall provide the Engineer with pertinent information 7 days prior to any phase change or any other major changes that affect traffic flow.

ITEMIZED LISTS FOR TRAFFIC CONTROL DEVICES

PCN 03W0

| SIGN CODE | SIGN DESCRIPTION | EXPRESSWAY / INTERSTATE | | | |
|---|----------------------------------|-------------------------|-----------|---------------|-------------|
| | | NUMBER | SIGN SIZE | SQFT PER SIGN | SQFT |
| R1-2 | YIELD | 1 | 36" x 36" | 4 | 4 |
| R2-1 | SPEED LIMIT | 13 | 36" x 48" | 12 | 156 |
| R2-6aP | FINES DOUBLE (plaque) | 3 | 36" x 24" | 6 | 18 |
| W3-2 | YIELD AHEAD (symbol) | 1 | 48" x 48" | 16 | 16 |
| W3-4 | BE PREPARED TO STOP | 2 | 48" x 48" | 16 | 32 |
| W3-5 | SPEED REDUCTION AHEAD (MPH) | 9 | 48" x 48" | 16 | 144 |
| W4-1 | MERGE (symbol) | 2 | 48" x 48" | 16 | 32 |
| W4-2 | LEFT or RIGHT LANE ENDS (symbol) | 6 | 48" x 48" | 16 | 96 |
| W5-4 | RAMP NARROWS | 1 | 48" x 48" | 16 | 16 |
| W7-3aP | NEXT MILES (plaque) | 3 | 36" x 30" | 8 | 24 |
| W8-1 | BUMP | 6 | 48" x 48" | 16 | 96 |
| W8-15 | GROOVED PAVEMENT | 4 | 48" x 48" | 16 | 64 |
| W8-15P | MOTORCYCLE (plaque) | 4 | 30" x 24" | 5 | 20 |
| W13-1P | ADVISORY SPEED (plaque) | 7 | 30" x 30" | 6 | 42 |
| W20-1 | ROAD WORK AHEAD | 8 | 48" x 48" | 16 | 128 |
| W20-5 | LEFT or RIGHT LANE CLOSED AHEAD | 6 | 48" x 48" | 16 | 96 |
| W20-7 | FLAGGER (symbol) | 3 | 48" x 48" | 16 | 48 |
| G20-1 | ROAD WORK NEXT MILES | 2 | 48" x 24" | 8 | 16 |
| G20-2 | END ROAD WORK | 8 | 48" x 24" | 8 | 64 |
| SPECIAL | EXIT WITH 45 DEGREE ARROW | 1 | 36" x 32" | 8 | 8 |
| OM1-3 | TYPE I OBJECT MARKER | 6 | 18" x 18" | 2 | 12 |
| SPECIAL | MOTORCYCLES USE EXTREME CAUTION | 4 | 48" x 48" | 16 | 64 |
| EXPRESSWAY / INTERSTATE TRAFFIC CONTROL SIGNS SQFT | | | | | 1196 |

TYPE 3 BARRICADES

| ITEM DESCRIPTION | QUANTITY |
|-----------------------------------|----------|
| Type 3 Barricade, 8' Double Sided | 3 Each |

ARROW BOARDS

| ITEM DESCRIPTION | QUANTITY |
|--------------------|----------|
| Type C Arrow Board | 3 Each |

ITEMIZED LISTS FOR TRAFFIC CONTROL DEVICES, CONTINUED

Revised 1/07/16 - BP

| | | | |
|-----------------------------|-----------------|-------|-----------------|
| STATE OF SOUTH DAKOTA | PROJECT | SHEET | TOTAL SHEETS |
| | IM 0903(102)112 | C4 | C9 |

PCN 05EE

| SIGN CODE | SIGN DESCRIPTION | EXPRESSWAY / INTERSTATE | | | |
|---|---------------------------------|-------------------------|-----------|---------------|------------|
| | | NUMBER | SIGN SIZE | SQFT PER SIGN | SQFT |
| R1-2 | YIELD | 1 | 36" x 36" | 4 | 4 |
| R2-1 | SPEED LIMIT | 2 | 36" x 48" | 12 | 24 |
| W1-4 | REVERSE CURVE (L or R) | 2 | 48" x 48" | 16 | 32 |
| W3-2 | YIELD AHEAD (symbol) | 1 | 48" x 48" | 16 | 16 |
| W3-4 | BE PREPARED TO STOP | 2 | 48" x 48" | 16 | 32 |
| W3-5 | SPEED REDUCTION AHEAD (MPH) | 1 | 48" x 48" | 16 | 16 |
| W4-1 | MERGE (symbol) | 2 | 48" x 48" | 16 | 32 |
| W5-4 | RAMP NARROWS | 1 | 48" x 48" | 16 | 16 |
| W7-3aP | NEXT MILES (plaque) | 2 | 36" x 30" | 8 | 16 |
| W8-1 | BUMP | 4 | 48" x 48" | 16 | 64 |
| W8-15 | GROOVED PAVEMENT | 2 | 48" x 48" | 16 | 32 |
| W20-1 | ROAD WORK AHEAD | 4 | 48" x 48" | 16 | 64 |
| W20-7 | FLAGGER (symbol) | 2 | 48" x 48" | 16 | 32 |
| G20-2 | END ROAD WORK | 2 | 48" x 24" | 8 | 16 |
| OM1-3 | TYPE I OBJECT MARKER | 4 | 18" x 18" | 2 | 8 |
| SPECIAL | MOTORCYCLES USE EXTREME CAUTION | 2 | 48" x 48" | 16 | 32 |
| EXPRESSWAY / INTERSTATE TRAFFIC CONTROL SIGNS SQFT | | | | | 436 |

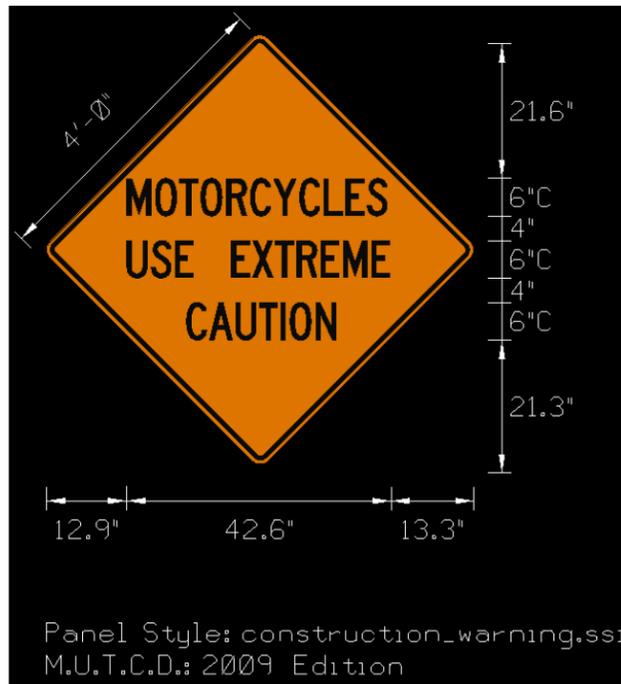
TYPE 3 BARRICADES

| ITEM DESCRIPTION | QUANTITY |
|-----------------------------------|----------|
| Type 3 Barricade, 8' Double Sided | 2 Each |

ARROW BOARDS

| ITEM DESCRIPTION | QUANTITY |
|--------------------|----------|
| Type C Arrow Board | 0 Each |

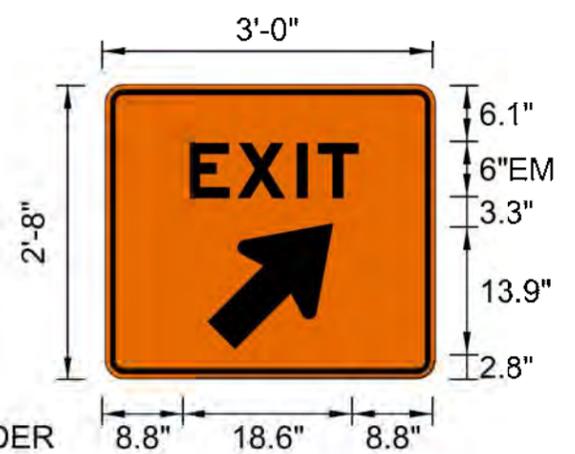
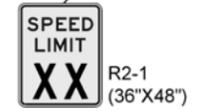
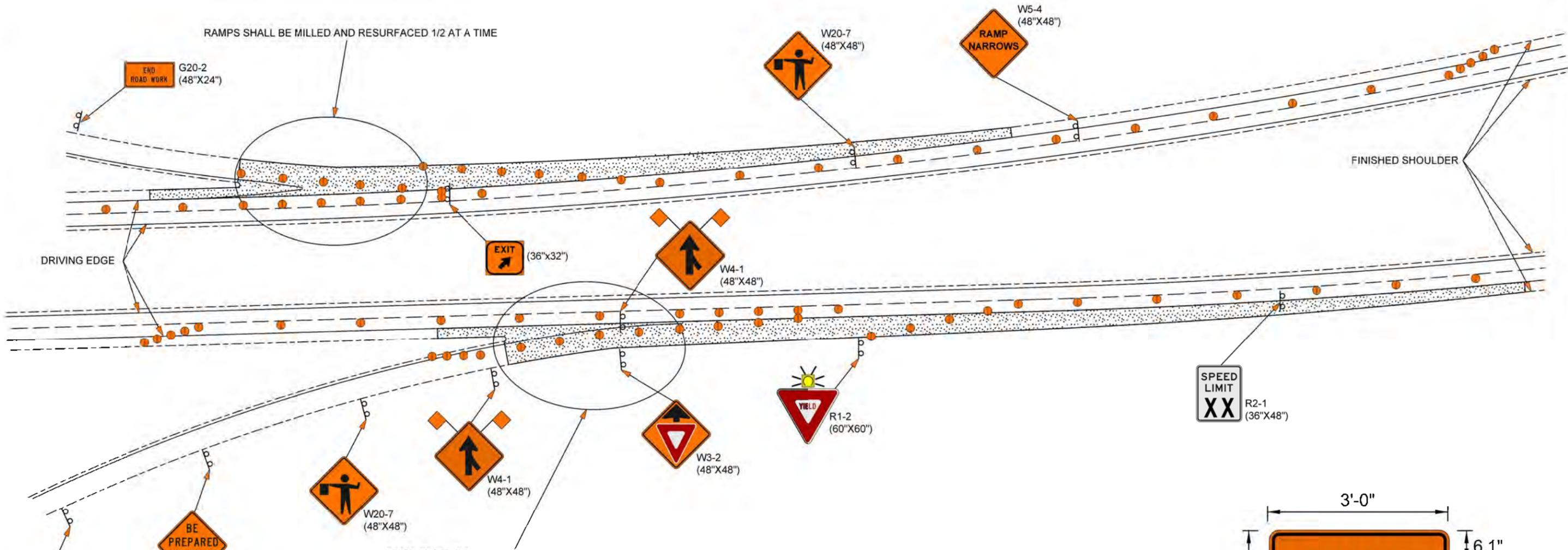
SPECIAL SIGN DETAIL



TRAFFIC CONTROL

RAMP DRIVING LANE RESURFACING

Plot Scale - 1:40
Plotted From - Irrc11640



BORDER
R=1.5"
TH=0.63"
IN=0.47"

Panel Style: construction_guide.ssi
M.U.T.C.D.: 2009 Edition

- THE WARNING LIGHT SHALL BE A SHIELDED TYPE B, IN ACCORDANCE WITH THE MUTCD
- CHANNELIZING DEVICE (DRUMS OR 42" CONES)
- WORK AREA
- SIGN SPACING, CHANNELIZING DEVICE SPACING AND TAPER LENGTHS SHALL CONFORM TO STANDARD PLATES.

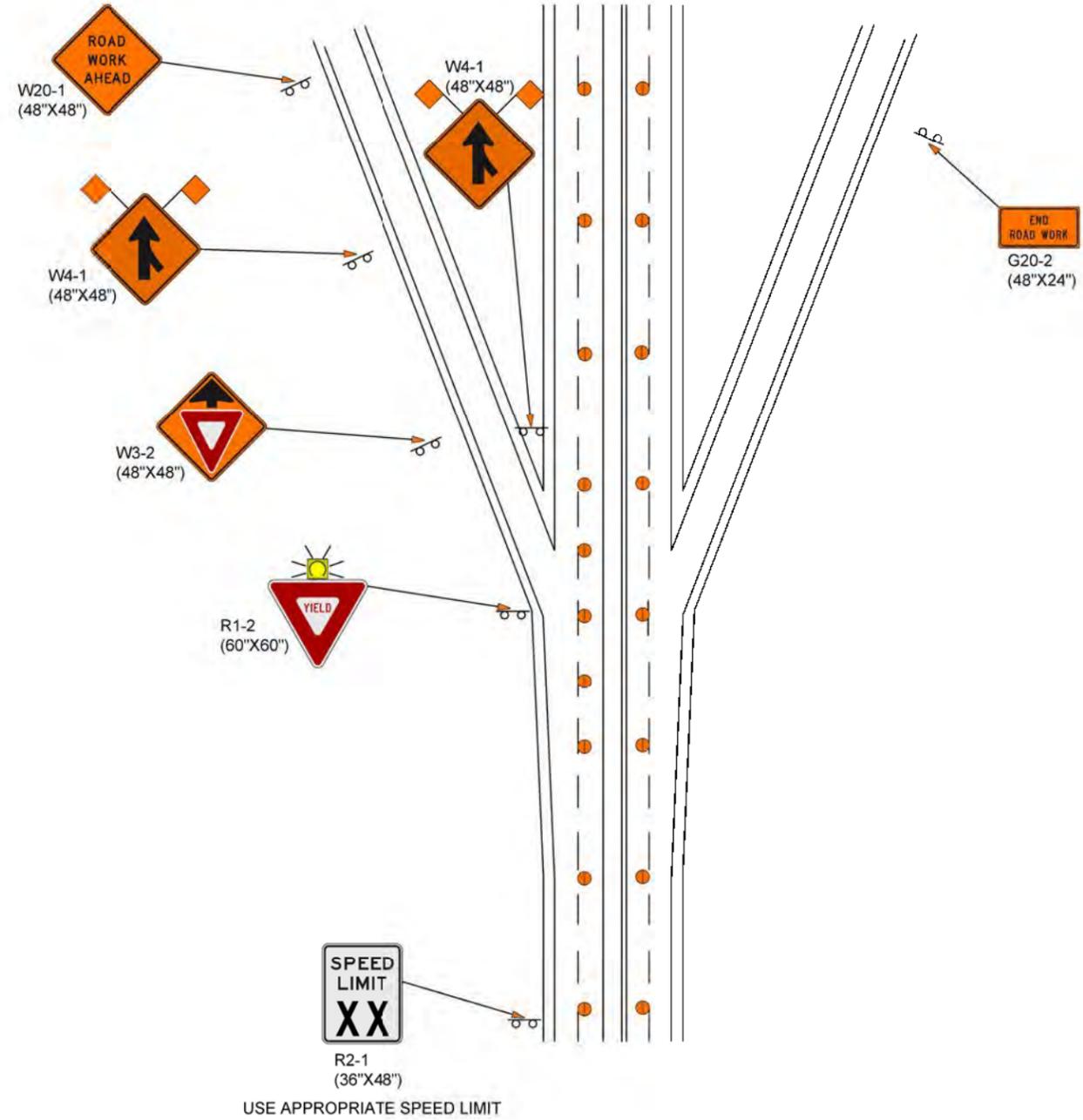
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| STATE OF SOUTH DAKOTA | PROJECT | SHEET | TOTAL SHEETS |
| | IM 0903(102)112 | C6 | C9 |

Plotting Date: 11/18/2015

TRAFFIC CONTROL

RAMP ENTRANCE AND EXIT SIGNING DETAILS #2



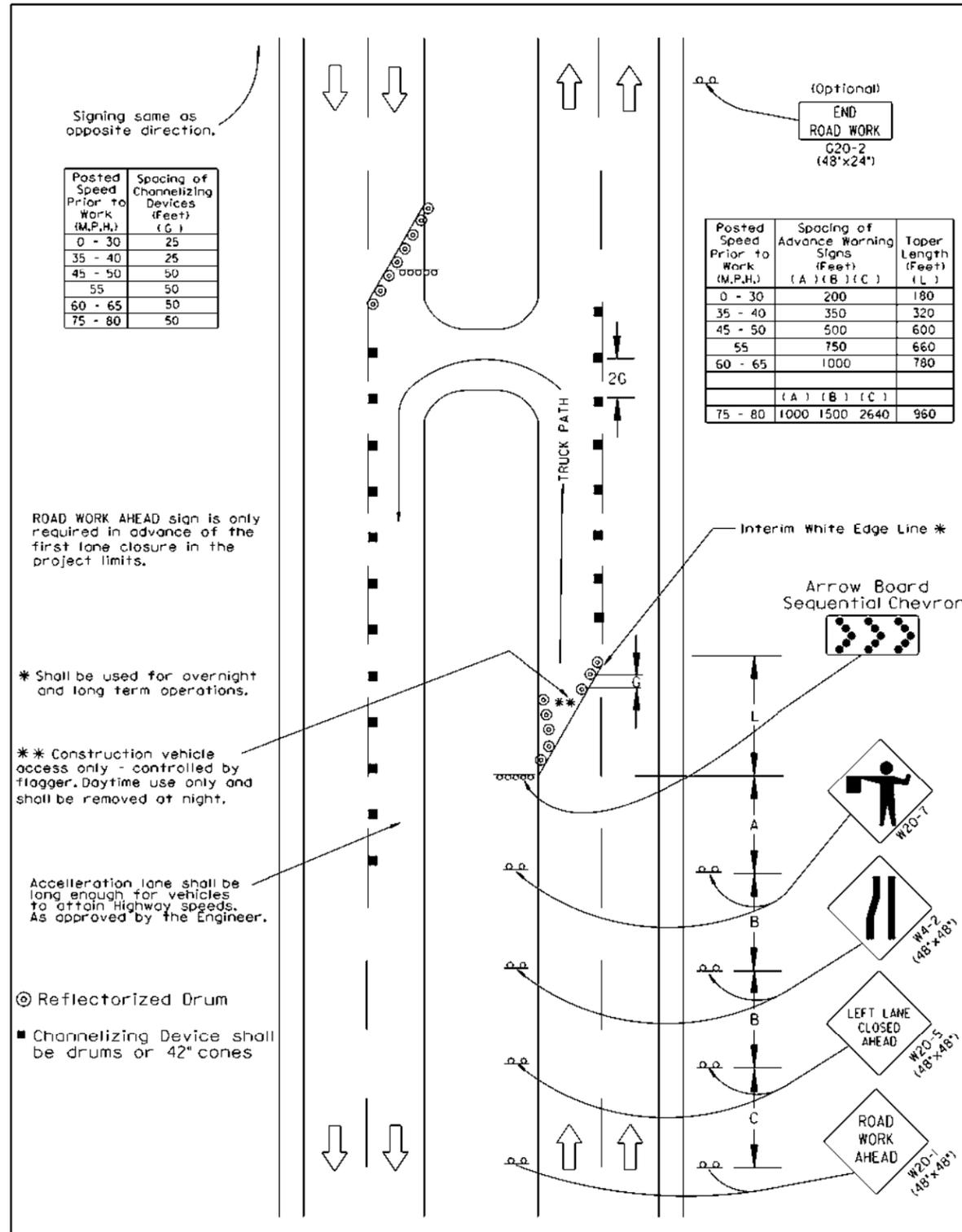
 THE WARNING LIGHT SHALL BE A SHIELDED TYPE B, IN ACCORDANCE WITH THE MUTCD

TRAFFIC CONTROL

MAINTENANCE CROSSOVER TRUCK CROSSING DETAIL

Plot Scale - 1:40

Plotted From - lrrc11640

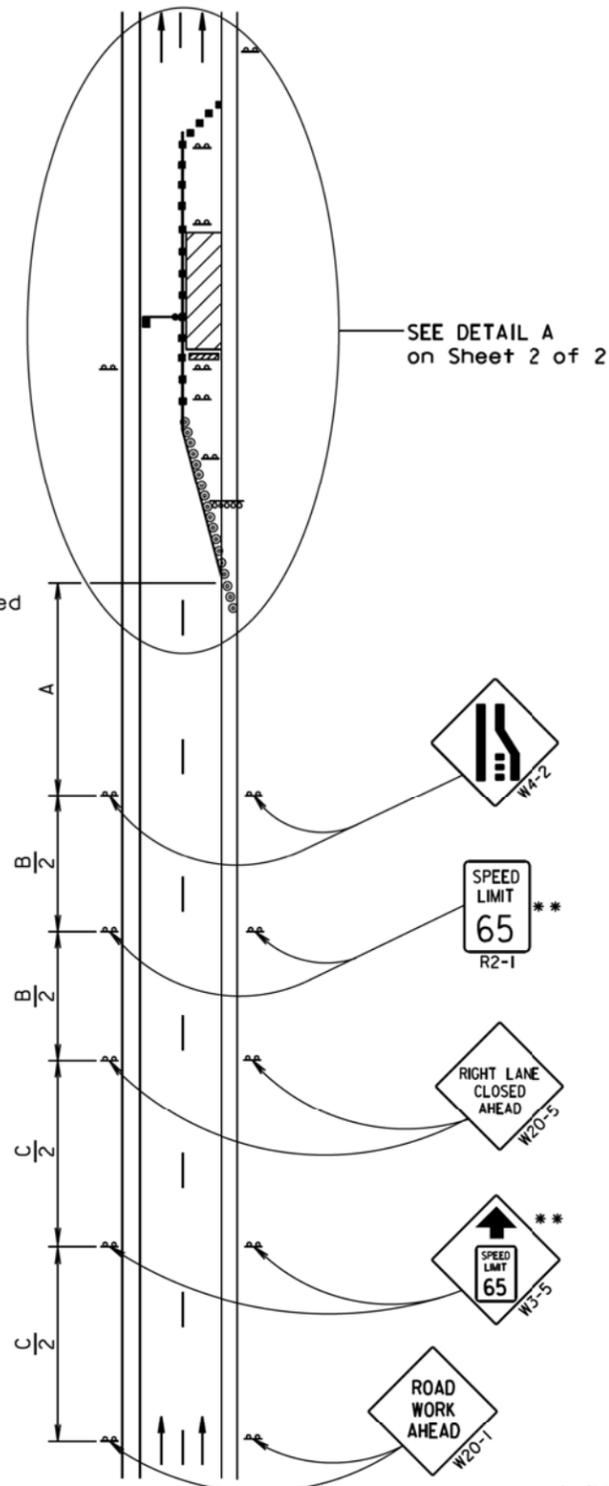


| Posted Speed Prior to Work (M.P.H.) | Spacing of Advance Warning Signs (Feet) | | |
|-------------------------------------|---|------|------|
| | (A) | (B) | (C) |
| 0 - 30 | 200 | | |
| 35 - 40 | 350 | | |
| 45 - 50 | 500 | | |
| 55 | 750 | | |
| 60 - 65 | 1000 | | |
| | (A) | (B) | (C) |
| 70 - 80 | 1000 | 1500 | 2640 |

- ** Speed appropriate for location.
- ⊙ Reflectorized Drum
- Channelizing Device

ROAD WORK AHEAD sign is only required in advance of the first lane closure.

High speed is defined as having a posted speed limit greater than 45 mph.



April 15, 2015

| | | |
|----------------------------------|--|-------------------------------|
| S D D O T | WORK ZONE SPEED REDUCTION FOR INTERSTATE AND HIGH SPEED MULTI-LANE HIGHWAYS | PLATE NUMBER 634.63 |
| | Published Date: 4th Qtr. 2015 | Sheet 1 of 2 |

| Posted Speed Prior to Work (M.P.H.) | Spacing of Channelizing Devices (Feet) (G) | Taper Length (Feet) (L) |
|-------------------------------------|--|-------------------------|
| 0 - 30 | 25 | 180 |
| 35 - 40 | 25 | 320 |
| 45 - 50 | 50 * | 600 |
| 55 | 50 * | 660 |
| 60 - 65 | 50 * | 780 |
| 70 - 80 | 50 * | 960 |

- * Spacing is 40' for 42" cones.
- ** Speed appropriate for location.
- *** Use speed limit designated for the condition when workers are present in the work space. Signs shall be covered or removed when workers are not present.

- Flagger (As Necessary)
- ⊙ Reflectorized Drum
- Channelizing Device

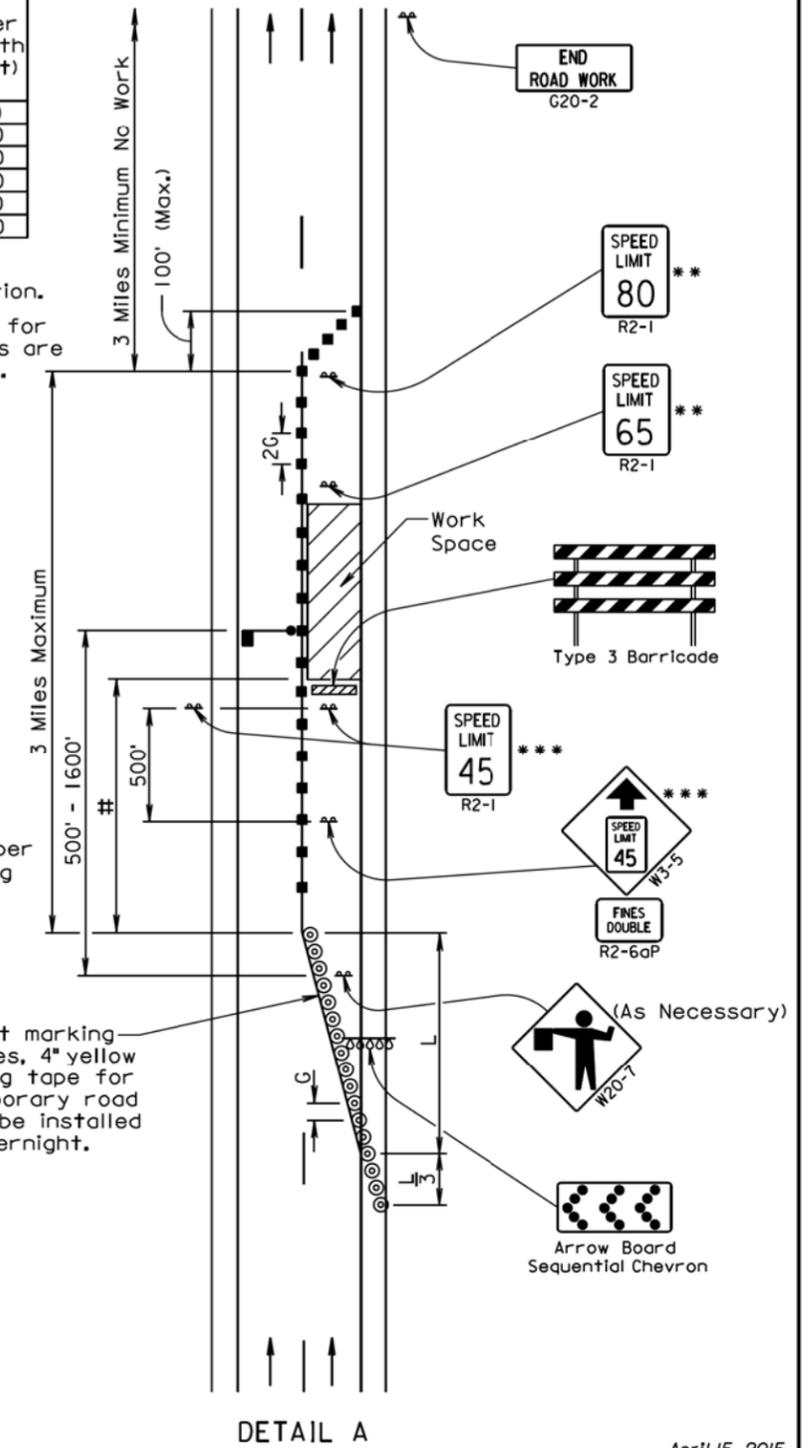
The Work Space shall be a minimum of 500' from the end of the taper.

The FLAGGER sign shall be used whenever there is a Flagger present.

The channelizing devices shall be 42" cones or drums.

42" cones may be used in place of the drums shown in the taper if setup will not be used during night time hours.

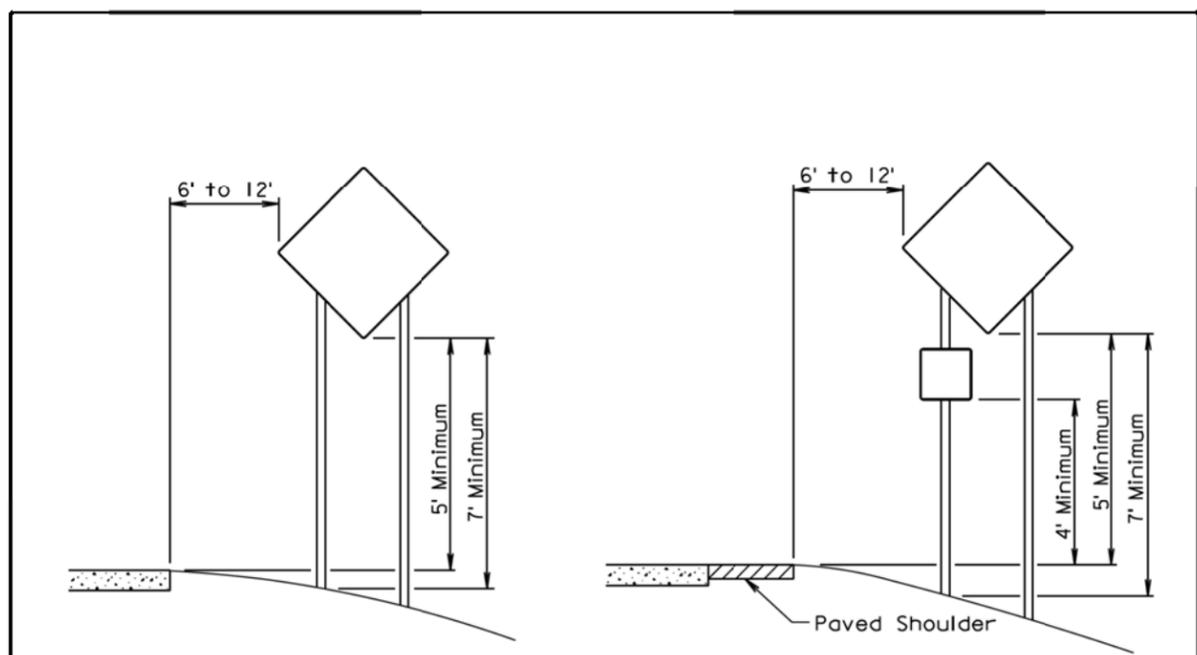
4" white temporary pavement marking tape for right lane closures, 4" yellow temporary pavement marking tape for left lane closures, or temporary road markers at 5' spacing shall be installed when the lane is closed overnight.



April 15, 2015

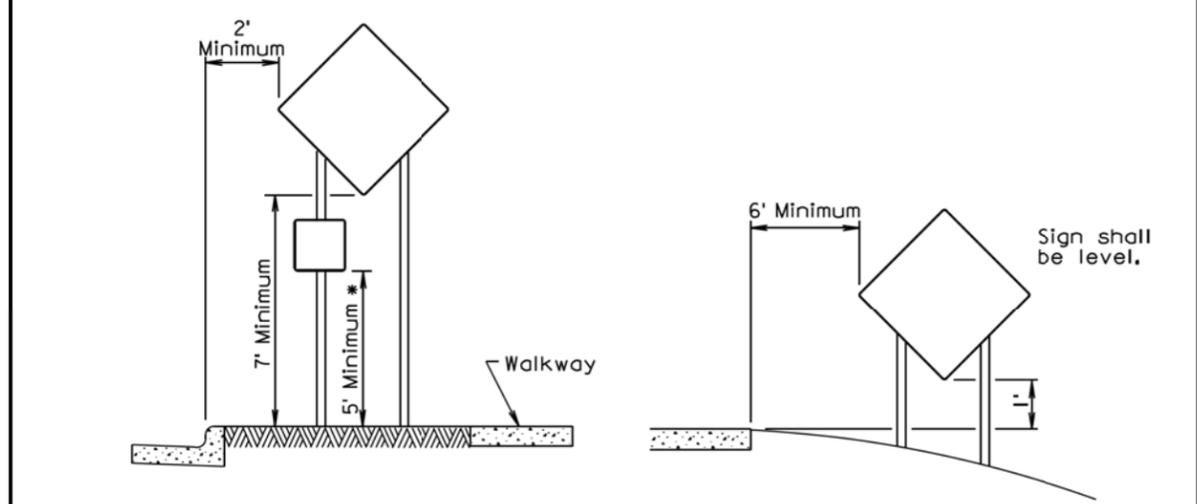
| | | |
|----------------------------------|--|-------------------------------|
| S D D O T | WORK ZONE SPEED REDUCTION FOR INTERSTATE AND HIGH SPEED MULTI-LANE HIGHWAYS | PLATE NUMBER 634.63 |
| | Published Date: 4th Qtr. 2015 | Sheet 2 of 2 |

Plot Scale - 1:200



RURAL DISTRICT

RURAL DISTRICT WITH SUPPLEMENTAL PLATE



URBAN DISTRICT

RURAL DISTRICT 3 DAY MAXIMUM

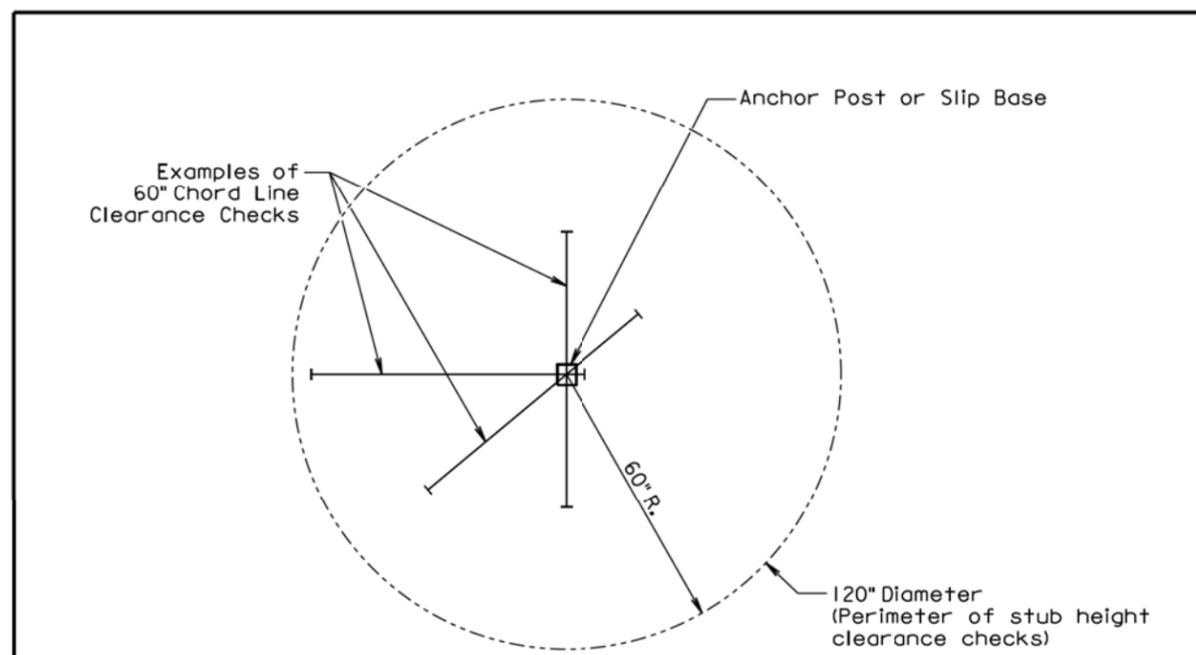
* If the bottom of supplemental plate is mounted lower than 7 feet above a pedestrian walkway, the supplemental plate should not project more than 4" into the pedestrian facility.

(Not applicable to regulatory signs)

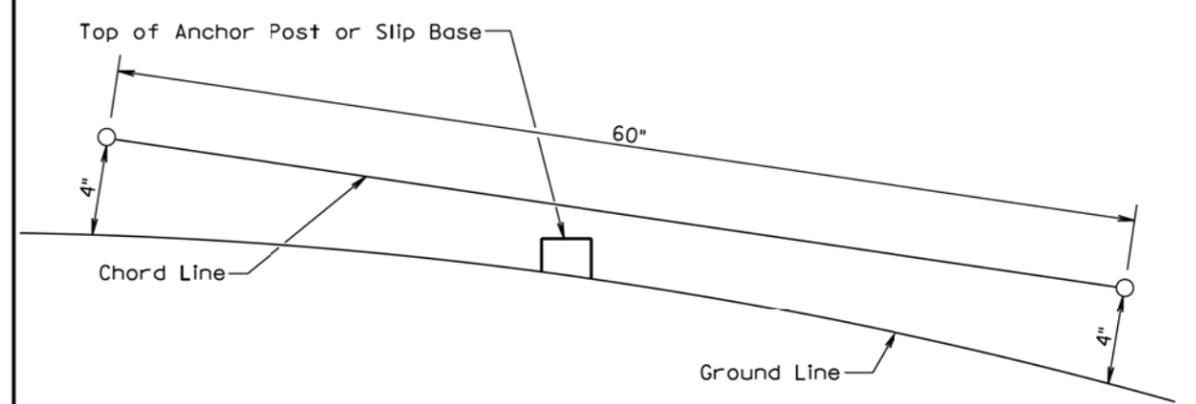
September 22, 2014

| | | |
|----------------------------------|---|-------------------------------|
| S D D O T | CRASHWORTHY SIGN SUPPORTS (Typical Construction Signing) | PLATE NUMBER 634.85 |
| | | Sheet 1 of 1 |

Published Date: 4th Qtr. 2015



PLAN VIEW
(Examples of stub height clearance checks)



ELEVATION VIEW

GENERAL NOTES:

The top of anchor posts and slip bases SHALL NOT extend above a 60° chord line within a 120" diameter circle around the post with ends 4" above the ground.

At locations where there is curb and gutter adjacent to the breakaway sign support, the stub height shall be a maximum of 4" above the ground line at the localized area adjacent to the breakaway support stub.

The 4" stub height clearance is not necessary for U-channel lap splices where the support is designed to yield (bend) at the base.

July 1, 2005

| | | |
|----------------------------------|---|-------------------------------|
| S D D O T | BREAKAWAY SUPPORT STUB CLEARANCE | PLATE NUMBER 634.99 |
| | | Sheet 1 of 1 |

Published Date: 4th Qtr. 2015

- Plotted From - Irrc11640

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