

March 31, 2025

ADDENDUM NO. 1

**RE: Item #9, April 2, 2025 Letting - NH-P 0011(316), PCN 09KJ, Brown, Faulk, Potter, Spink
County - Rout & Seal**

TO WHOM IT MAY CONCERN:

The following addenda to the plans shall be inserted and made a part of your proposal for the referenced project.

SPECIAL PROVISIONS: NO CHANGE

SDEBS BID PROPOSAL: *The electronic bid proposal for this contract has been revised to include the changes associated with this addendum. Bidders must log in to the SDEBS to retrieve and incorporate these changes into their bid.*

Quantities for Bid Items were changed:

Bid Item 350E0010 "Asphalt Concrete Crack Sealing" changed from 177,650 to 108,255 Lb

PLANS: Please destroy sheets 3 & 5 and replace with the enclosed sheets, dated 3/31/25.

Sheet 3: Bid Item 350E0010 "Asphalt Concrete Crack Sealing" changed from 177,650 to 108,255 Lb.

Sheet 5: Quantity of Asphalt Concrete Crack Sealing for SD20 changed from 67,550 to 53,186 Lb and US281 changed from 110,100 to 55,069 Lb. ASPHALT CONCRETE CRACK SEALING note was revised.

Sincerely,

Sam Weisgram
Engineering Supervisor

SW/cj

CC: Mark Peterson, Aberdeen Region Engineer
Lane Goldsmith, Aberdeen Area Engineer

ESTIMATE OF QUANTITIES AND ENVIRONMENTAL COMMITMENTS

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	NH-P 0011(316)	3	11

Revised 3/31/2025 EN

ESTIMATE OF QUANTITIES

BID ITEM NUMBER	ITEM	QUANTITY	UNIT
009E0010	Mobilization	Lump Sum	LS
350E0010	Asphalt Concrete Crack Sealing	108,255	Lb
634E0010	Flagging	150.0	Hour
634E0020	Pilot Car	60.0	Hour
634E0110	Traffic Control Signs	478.6	SqFt
634E0120	Traffic Control, Miscellaneous	Lump Sum	LS
634E0275	Type 3 Barricade	40	Each
634E0420	Type C Advance Warning Arrow Board	2	Each
634E0600	4" Temporary Pavement Marking Tape Type I	6,720	Ft

SPECIFICATIONS

Standard Specifications for Roads and Bridges, 2015 Edition and Required Provisions, Supplemental Specifications, and Special Provisions as included in the Proposal.

ENVIRONMENTAL COMMITMENTS

The SDDOT is committed to protecting the environment and uses Environmental Commitments as a communication tool for the Engineer and Contractor to ensure that attention is given to avoid, minimize, and/or mitigate an environmental impact. Environmental commitments to various agencies and the public have been made to secure approval of this project. An agency with permitting authority can delay a project if identified environmental impacts have not been adequately addressed. Unless otherwise designated, the Contractor's primary contact regarding matters associated with these commitments will be the Project Engineer. During construction, the Project Engineer will verify that the Contractor has met Environmental Commitment requirements. These environmental commitments are not subject to change without prior written approval from the SDDOT Environmental Office.

Additional guidance on SDDOT's Environmental Commitments can be accessed through the Environmental Procedures Manual found at: <https://dot.sd.gov/media/documents/EnvironmentalProceduresManual.pdf> >

For questions regarding change orders in the field that may have an effect on an Environmental Commitment, the Project Engineer will contact the Environmental Engineer at 605-773-3180 or 605-773-4336 to determine whether an environmental analysis and/or resource agency coordination is necessary.

Once construction is complete, the Project Engineer will review all environmental commitments for the project and document their completion.

COMMITMENT B: FEDERALLY THREATENED, ENDANGERED, AND PROTECTED SPECIES

COMMITMENT B2: WHOOPING CRANE

The Whooping Crane is a spring and fall migratory bird in South Dakota that is about 5 feet tall and typically stops on wetlands, rivers, and agricultural lands along their migration route. An adult Whooping Crane is white with a red crown and a long, dark, pointed bill. Immature Whooping Cranes are cinnamon brown. While in flight, their long necks are kept straight and their long dark legs trail behind. Adult Whooping Cranes' black wing tips are visible during flight.

Action Taken/Required:

Harassment or other measures to cause the Whooping Crane to leave the site is a violation of the Endangered Species Act. If a Whooping Crane is sighted roosting in the vicinity of the project, borrow pits, or staging areas associated with the project, cease construction activities in the affected area until the Whooping Crane departs and immediately contact the Project Engineer. The Project Engineer will contact the Environmental Office so that the sighting can be reported to USFWS.

COMMITMENT E: STORM WATER

Construction activities constitute less than 1 acre of disturbance.

Action Taken/Required:

At a minimum and regardless of project size, appropriate erosion and sediment control measures must be installed to control the discharge of pollutants from the construction site.

COMMITMENT H: WASTE DISPOSAL SITE

The Contractor will furnish a site(s) for the disposal of construction and/or demolition debris generated by this project.

Action Taken/Required:

Construction and/or demolition debris may not be disposed of within the Public ROW.

The waste disposal site(s) will be managed and reclaimed in accordance with the following from the General Permit for Construction/Demolition Debris Disposal Under the South Dakota Waste Management Program issued by the Department of Agriculture and Natural Resources.

The waste disposal site(s) will not be located in a wetland, within 200 feet of surface water, or in an area that adversely affects wildlife, recreation, aesthetic value of an area, or any threatened or endangered species, as approved by the Environmental Office and the Project Engineer.

If the waste disposal site(s) is located such that it is within view of any ROW, the following additional requirements will apply:

1. Construction and/or demolition debris consisting of concrete, asphalt concrete, or other similar materials will be buried in a trench separate from wood debris. The final cover over the construction and/or demolition debris will consist of a minimum of 1 foot of soil capable of supporting vegetation. Waste disposal sites provided outside of the Public ROW will be seeded in accordance with Natural Resources Conservation Service recommendations. The seeding recommendations may be obtained through the appropriate County NRCS Office. The Contractor will control the access to waste disposal sites not within the Public ROW with fences, gates, and placement of a sign or signs at the entrance to the site stating, "No Dumping Allowed".
2. Concrete and asphalt concrete debris may be stockpiled within view of the ROW for a period not to exceed the duration of the project. Prior to project completion, the waste will be removed from view of the ROW or buried, and the waste disposal site reclaimed as noted above.

The above requirements will not apply to waste disposal sites that are covered by an individual solid waste permit as specified in SDCL 34A-6-58, SDCL 34A-6-1.13, and ARSD 74:27:10:06.

Failure to comply with the requirements stated above may result in civil penalties in accordance with South Dakota Solid Waste Law, SDCL 34A-6-1.31.

All costs associated with furnishing waste disposal site(s), disposing of waste, maintaining control of access (fence, gates, and signs), and reclamation of the waste disposal site(s) will be incidental to the various contract items.

Revised 3/31/2025 EN

WORK DESCRIPTION

This project involves crack sealing of asphalt concrete surfaces on the route shown in the plans.

GENERAL TRAFFIC CONTROL

Existing guide, route, informational logo, regulatory, and warning signs will be temporarily reset and maintained during construction. Removing, relocating, covering, salvaging, and resetting of existing traffic control devices, including delineation, will be the responsibility of the Contractor. Cost for this work will be incidental to the contract unit prices for the various items unless otherwise specified in the plans. Any delineators and signs damaged or lost will be replaced by the Contractor at no cost to the State.

All temporary traffic control sign locations will be set in the field by the Contractor and verified by the Engineer prior to installation.

Portable sign supports will not be located on sidewalks, bicycle facilities, or other areas designated for pedestrian or bicycle traffic.

All construction operations will be conducted in the general direction of traffic movement.

If there is a discrepancy between the traffic control plans, standard plates, and the MUTCD, whichever is more stringent will be used, as determined by the Engineer.

Unless otherwise stated in these plans, work will not be allowed during hours of darkness.

Fixed location signing placed more than 4 calendar days prior to the start of construction will be covered or laid down until the time of construction. The covers must be approved by the Engineer prior to installation. The cost of materials, labor, and equipment necessary to complete this work will be incidental to other contract items. No separate payment will be made.

All fixed location signs, signposts, and breakaway bases will be removed within 7 calendar days following pavement marking.

Traffic will be maintained on the driving lanes. Use of the shoulder as a driving lane will not be permitted. Any damage to the shoulder due to rerouted traffic or Contractor's equipment will be repaired at no expense to the Department.

A Type 3 Barricade will be installed at the end of a lane closure taper as detailed in these plans. Additional Type 3 Barricades will be installed facing traffic within the closed lane at a spacing of 1/4 mile.

Contractor's lane closure on SD 20 will not exceed 3 miles in length. The maximum time for vehicles to wait at flagger station will not exceed 15 minutes. Contractor will be required to shorten lane closure if traveling public is impeded more than 15 minutes. Contractor's lane closure/s on US 281 will be limited to 5 miles in length. The distance between the closest points of any two-lane closures will be at least 3 miles, excluding tapers.

COORDINATION BETWEEN CONTRACTORS

A separate contract for Project NH 0281(128)193 – PCN 08JN will be awarded to another Contractor for Polymer Chip Seal adjacent to this project. The Polymer Chip Seal for PCN 08JN will take place on the bridge on US 281 over Foote Creek (MRM 193.08).

The Contractor will schedule work so as not to interfere with or hinder the progress of the work performed by the other Contractor on PCN 08JN. Conflicting traffic control devices may need to be temporarily adjusted or removed as directed by the Engineer and at no additional cost to the contract.

TRAFFIC CONTROL SIGNS

Traffic control signs have been included in a table for each route. Payment will only be for those signs used on each route.

SD 20 Route			
BID ITEM NUMBER	ITEM	QUANTITY	UNIT
350E0010	Asphalt Concrete Crack Sealing	53,186	Lb
634E0010	Flagging	150.0	Hour
634E0020	Pilot Car	60.0	Hour
634E0110	Traffic Control Signs	154.6	SqFt

US 281 Route			
BID ITEM NUMBER	ITEM	QUANTITY	UNIT
350E0010	Asphalt Concrete Crack Sealing	55,069	Lb
634E0110	Traffic Control Signs	324.0	SqFt
634E0275	Type 3 Barricade	40	Each
634E0420	Type C Advance Warning Arrow Board	2	Each
634E0600	4" Temporary Pavement Marking Tape Type I	6,720	Ft

WORK ZONE SPEED REDUCTION

The Department is required to obtain a speed reduction resolution prior to the installation of any SPEED LIMIT (R2-1) signs shown on standard plate 634.63. To provide adequate time for the resolution to be enacted, the Contractor will inform the Engineer a minimum of 3 weeks prior to the scheduled installation of any work zone speed reduction signs on the project. The information provided by the Contractor will include the anticipated date of sign installation, the newly reduced speed limit, the location of the work zone, and the anticipated completion date of work requiring the speed reduction.

FLAGGING

Operations will be conducted so that the traveling public will not have to wait longer than 15 minutes at the flagger station.

Advance warning Flagger signs will be required when Flaggers are present and removed when no Flaggers are present. Additional flagger warning signs and flagger hours have been included in the Estimate of Quantities for use on intersecting roads. These flaggers will be used as directed by the Engineer and will be used primarily during daytime hours. Also included in the Estimate of Quantities are WAIT FOLLOW PILOT CAR signs for use on low volume intersecting roads as determined by the Engineer. WAIT FOLLOW PILOT CAR signs will not block the view of the stop sign.



It is required that the flaggers and pilot car operators be able to communicate with one another. If an emergency vehicle needs to pass through the project, the Contractor will be required to expedite traffic movement. All costs associated with this will be incidental to the contract unit price per hour for "Flagging".

CONSTRUCTION REQUIREMENTS

Shoulder bevel slopes greater than 3/8 inch per foot will not be routed and sealed unless directed by the Engineer.

ASPHALT CONCRETE AGGREGATES

SDDOT asphalt mixes are known to contain crushed ledge rock such as granite. The Contractor can expect to encounter various percentages of crushed ledge rock both in the larger aggregates and the fines.

ASPHALT CONCRETE CRACK SEALING

Estimated quantities to complete the project were figured based all the existing transverse and longitudinal cracks. If more longitudinal cracks have developed than estimated, the Contractor will complete the crack sealing at the direction of the Engineer.