

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	NH 0012(152)187, NH 0212(149)208, & P 1806(06)365	C1	C8

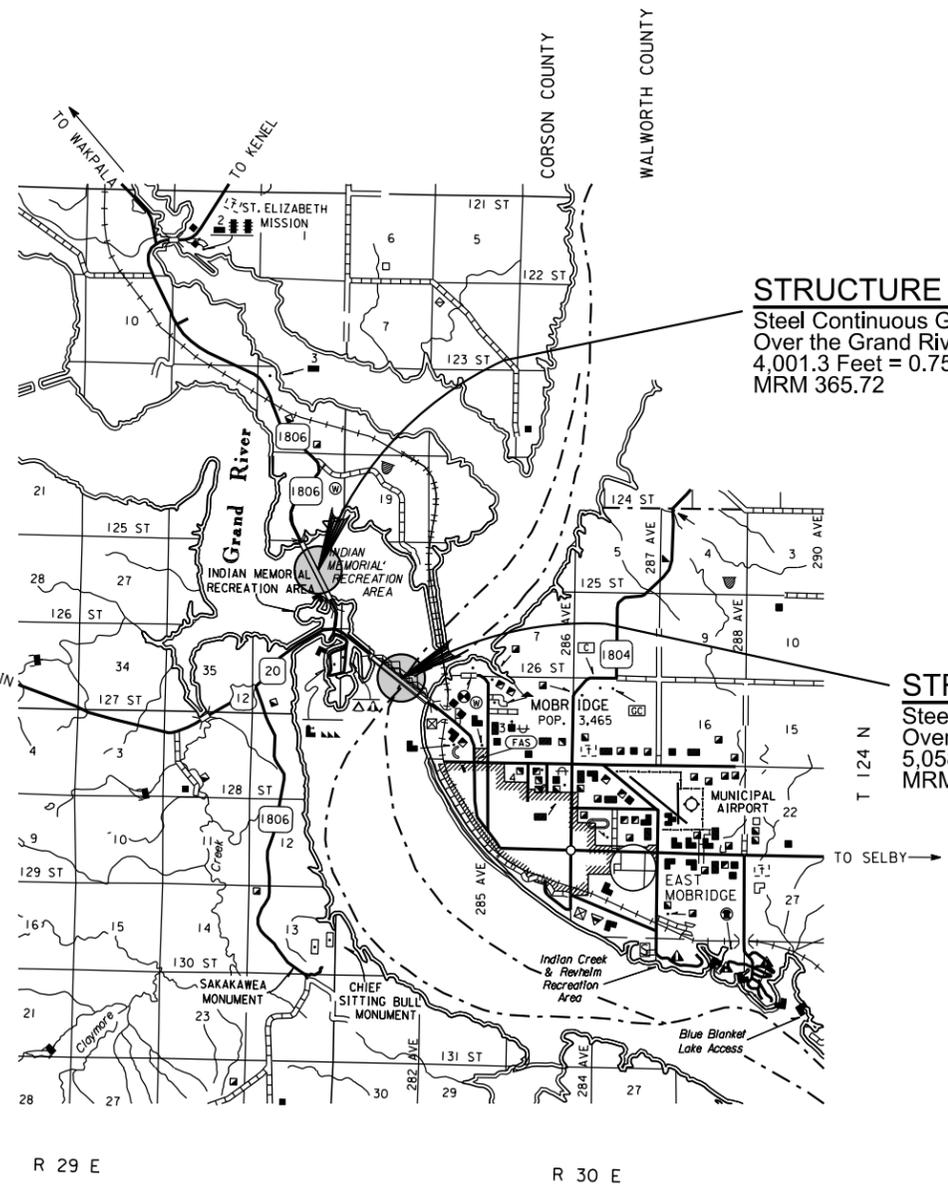
Plotting Date: 02/11/2016

Revised by JJR on 2/11/2016

SECTION C: TRAFFIC CONTROL PLANS

INDEX OF SHEETS

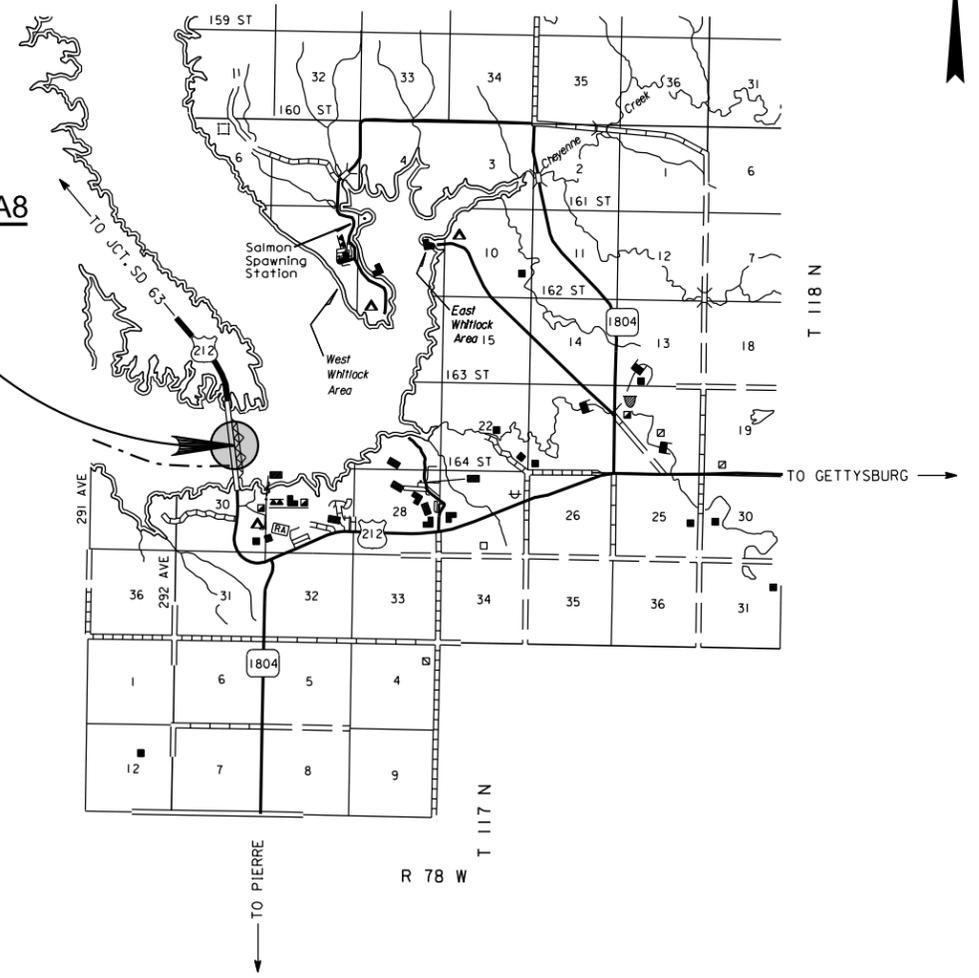
- Sheet C1: Title Sheet
- Sheet C2: Estimate of Quantities and Notes
- Sheet C3: Sign Tabulation
- Sheet C4: Width Restriction Sign Location
- Sheet C5: Width Restriction Sign Design
- Sheets C6-C8: Standard Plates



STRUCTURE # 16-737-253 - PCN 02A7
 Steel Continuous Girder Bridge
 Over the Grand River
 4,001.3 Feet = 0.758 Mile
 MRM 365.72

STRUCTURE # 54-056-158 - PCN 02A8
 Steel Continuous Truss Bridge
 Over the Missouri River
 4,619.3 Feet = 0.875 Mile
 MRM 208.53

STRUCTURE # 65-000-020 - PCN 02A6
 Steel Continuous Truss Bridge
 Over the Missouri River
 5,058.5 Feet = 0.958 Mile
 MRM 187.15



Plot Scale - 1:200

Plotted From - trp25584

File - ...Corso2A6\Title Sheet.dgn

Revised by JJR on 2/16/2016
 Revised by JJR on 3/03/2016
 Revised by JJR on 3/16/2016

STATE OF	PROJECT	SHEET NO.	TOTAL SHEETS
S.D.	NH 0012(152)187, NH 0212(149)208, & P 1806(06)365	C2	C8

ESTIMATE OF QUANTITIES

NH 0012(152)187 – PCN 02A6

BID ITEM NUMBER	ITEM	QUANTITY	UNIT
009E0010	Mobilization	Lump Sum	LS
634E0010	Flagging	360.0	Hour
634E0110	Traffic Control Signs	238	SqFt
634E0120	Traffic Control, Miscellaneous	Lump Sum	LS
634E1002	Detour Signing	456.0	SqFt

P 1806(06)365 – PCN 02A7

BID ITEM NUMBER	ITEM	QUANTITY	UNIT
009E0010	Mobilization	Lump Sum	LS
634E0010	Flagging	180.0	Hour
634E0110	Traffic Control Signs	238	SqFt
634E0120	Traffic Control, Miscellaneous	Lump Sum	LS
634E1002	Detour Signing	171.0	SqFt

NH 0212(149)208 – PCN 02A8

BID ITEM NUMBER	ITEM	QUANTITY	UNIT
009E0010	Mobilization	Lump Sum	LS
634E0010	Flagging	150.0	Hour
634E0110	Traffic Control Signs	238	SqFt
634E0120	Traffic Control, Miscellaneous	Lump Sum	LS
634E1002	Detour Signing	399.0	SqFt

SEQUENCE OF OPERATIONS

The Contractor shall submit a proposed sequence of operations for the Engineer's review and approval at least two weeks prior to the preconstruction meeting.

Traffic shall be maintained through the project at ALL times.

Once work that inconveniences traffic has commenced on a structure site, it shall be pursued in a near continuous, expeditious manner to its completion. Any work that restricts the motorist from driving the posted speed limit, reduces existing roadway width, or causes a potentially unsafe condition due to Contractor operations such as frequent movement of equipment or materials on or through the project, is considered to be an inconvenience to traffic.

The Contractor is allowed to have one flagger setup per bridge unless directed by the Engineer.

The Contractor will not be allowed to have width restriction signing occurring simultaneously for the Missouri River Structures at Mobridge (Structure No. 16-737-253) and Forest City (Structure No. 54-056-158). One of the structures shall be open to accommodate over width traffic at all times throughout the project duration.

The Contractor shall have all lanes of traffic opened prior to leaving for the day.

The Contractor may perform work during daylight hours only, unless additional hours are approved by the Engineer.

If the Contractor elects to have the construction staging area outside the SDDOT ROW the Contractor shall contact Oahe Project Office. Oahe Project Office contact information: Phil Sheffield – (605) 945-3420.

UTILITIES

The Contractor shall contact the involved utility companies through South Dakota One Call (1-800-781-7474) prior to starting work. It shall be the responsibility of the Contractor to coordinate work with the utility owners to avoid damage to existing facilities.

Utilities are not planned to be affected on this project. If utilities are identified near the improvement area through the SD One Call Process as required by South Dakota Codified Law 49 7A and Administrative Rule Article 20:25, the Contractor shall contact the Engineer to determine modifications that will be necessary to avoid utility impacts.

TRAFFIC CONTROL

The Contractor shall designate an employee who will be available 24 hours/day, 7 days/week to be responsible for the maintenance of traffic during periods of repair work. The name and phone number of person(s) shall be provided to the SD Department of Transportation (605-845-3844), SD Highway Patrol (Pierre State Radio (605-842-0810)), Corson County Sheriff Department (605-273-4533), Walworth County Sheriff Department (605-649-7600), Dewey County Sheriff Department (605-865-3330), and Potter County Sheriff Department (605-765-9405).

All traffic control devices shall be in "like new" condition.

GENERAL MAINTENANCE OF TRAFFIC

All traffic control sign locations shall be set in the field by the Contractor and verified by the Engineer prior to installation.

Channelizing devices in a series shall be of the same type. Channelizing drums shall be of a two part construction with breakaway bases. The cost of additional channelizing devices shall be incidental to the contract lump sum price for "Traffic Control, Miscellaneous".

Certified flaggers properly attired and preceded by FLAGGER symbol signs, will be required where work activity and/or equipment present a hazard to the workers, a hazard to through traffic, or encroaches into a driving lane.

Indiscriminate driving and parking of vehicles within the right-of-way will not be permitted. Any damage to the vegetation, surfacing, embankment, delineators and existing signs resulting from such indiscriminate use shall be repaired

and/or restored by the Contractor, at no expense to the State, and to the satisfaction of the Engineer.

All non-fixed location signs may be mounted on portable supports. The portable supports shall be constructed to yield upon impact to minimize hazards to motorists, and shall be of proper height. The bottom of signs on portable or temporary supports shall not be less than seven feet above the pavement in urban areas and one foot above the pavement in rural areas. Portable sign supports may be used as long as the duration is less than 3 days. If the duration is more than 3 days the signs shall meet the minimum mounting heights of 5 foot for rural areas and 7 foot for urban areas.

A shadow vehicle, equipped with flashing amber light and a ROAD MACHINERY AHEAD sign prominently displayed, shall be used in advance of landscaping, clean up, and other mobile work activities. The cost of ROAD MACHINERY AHEAD sign shall be incidental to the contract lump sum price for "Traffic Control, Miscellaneous"

The cost for additional signs shall be paid for at the contract unit price per square foot for "Traffic Control Signs". Additional Flagger hours shall be paid for at the contract unit price per hour for "Flagging".

Traffic Control signs, as shown in the Estimate of Quantities, are estimates. Contractor's operation may require adjustments in quantities, either more or less. Payment will be for those signs actually ordered by the Engineer and used.

Reflectorized Sheeting Requirements for Temporary Traffic Control Devices

Delete the first paragraph of Section 984.1 and replace with the following:

Temporary traffic control devices, including signs, drums, cones, tubular markers, barricades, vertical panels, and direction indicator barricades shall be reflectorized with sheeting applied to a satisfactory backing. For all temporary traffic control warning signs, the reflective sheeting shall meet or exceed the standards of Type VII, Type VIII, Type IX, or Type XI as defined by AASHTO M 268 (ASTM D4956). For all other temporary traffic control signs, the reflective sheeting shall meet or exceed the standards of Type IV, Type V, Type VII, Type VIII, Type IX, or Type XI as defined by AASHTO M 268 (ASTM D4956). For barricades, vertical panels, and direction indicator barricades; the reflective sheeting shall meet or exceed the standards of Type III as defined by AASHTO M 268 (ASTM D4956). Round surfaced temporary traffic control devices including, but not limited to; drums, cones, and tubular markers shall be reflectorized with reflectorized sheeting meeting or exceeding the standards of Type IV as defined by AASHTO M 268 (ASTM D4956). All orange colored material shall be fluorescent.

STATE OF	PROJECT	SHEET NO.	TOTAL SHEETS
S.D.	NH 0012(152)187, NH 0212(149)208, & P 1806(06)365	C3	C8

ITEMIZED LIST FOR TRAFFIC CONTROL SIGNS NH 0012(152)187 – PCN 02A6

SIGN CODE	SIGN DESCRIPTION	CONVENTIONAL ROAD			
		NUMBER	SIGN SIZE	SQFT PER SIGN	SQFT
W16-2P	___ FEET (supplemental distance plaque)	2	30" x 24"	5	10
W20-1	ROAD WORK AHEAD	5	48" x 48"	16	80
W20-4	ONE LANE ROAD AHEAD	2	48" x 48"	16	32
W20-7	FLAGGER (symbol)	2	48" x 48"	16	32
W21-5	SHOULDER WORK	4	48" x 48"	16	64
G20-2	END ROAD WORK	4	36" x 18"	5	20
CONVENTIONAL ROAD TRAFFIC CONTROL SIGNS SQFT					238

ITEMIZED LIST FOR TRAFFIC CONTROL SIGNS PCN: P 1806(06)365 – PCN 02A7

SIGN CODE	SIGN DESCRIPTION	CONVENTIONAL ROAD			
		NUMBER	SIGN SIZE	SQFT PER SIGN	SQFT
W16-2P	___ FEET (supplemental distance plaque)	2	30" x 24"	5	10
W20-1	ROAD WORK AHEAD	5	48" x 48"	16	80
W20-4	ONE LANE ROAD AHEAD	2	48" x 48"	16	32
W20-7	FLAGGER (symbol)	2	48" x 48"	16	32
W21-5	SHOULDER WORK	4	48" x 48"	16	64
G20-2	END ROAD WORK	4	36" x 18"	5	20
CONVENTIONAL ROAD TRAFFIC CONTROL SIGNS SQFT					238

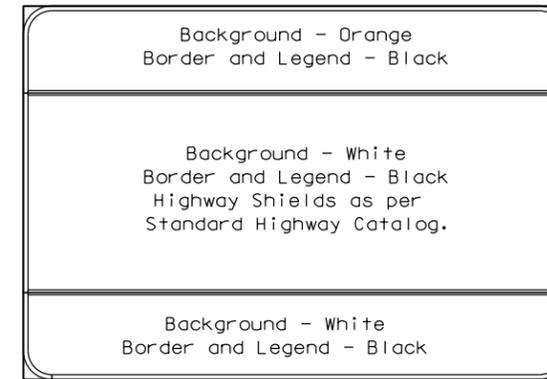
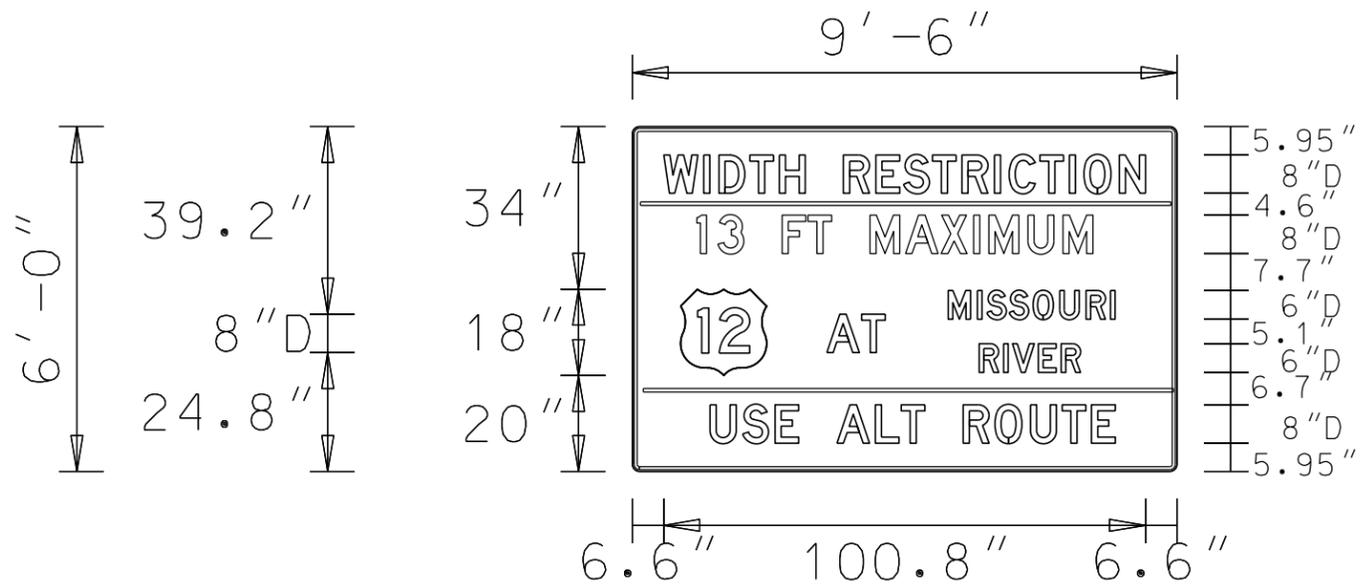
ITEMIZED LIST FOR TRAFFIC CONTROL SIGNS NH 0212(149)208 – PCN 02A8

SIGN CODE	SIGN DESCRIPTION	CONVENTIONAL ROAD			
		NUMBER	SIGN SIZE	SQFT PER SIGN	SQFT
W16-2P	___ FEET (supplemental distance plaque)	2	30" x 24"	5	10
W20-1	ROAD WORK AHEAD	5	48" x 48"	16	80
W20-4	ONE LANE ROAD AHEAD	2	48" x 48"	16	32
W20-7	FLAGGER (symbol)	2	48" x 48"	16	32
W21-5	SHOULDER WORK	4	48" x 48"	16	64
G20-2	END ROAD WORK	4	36" x 18"	5	20
CONVENTIONAL ROAD TRAFFIC CONTROL SIGNS SQFT					238

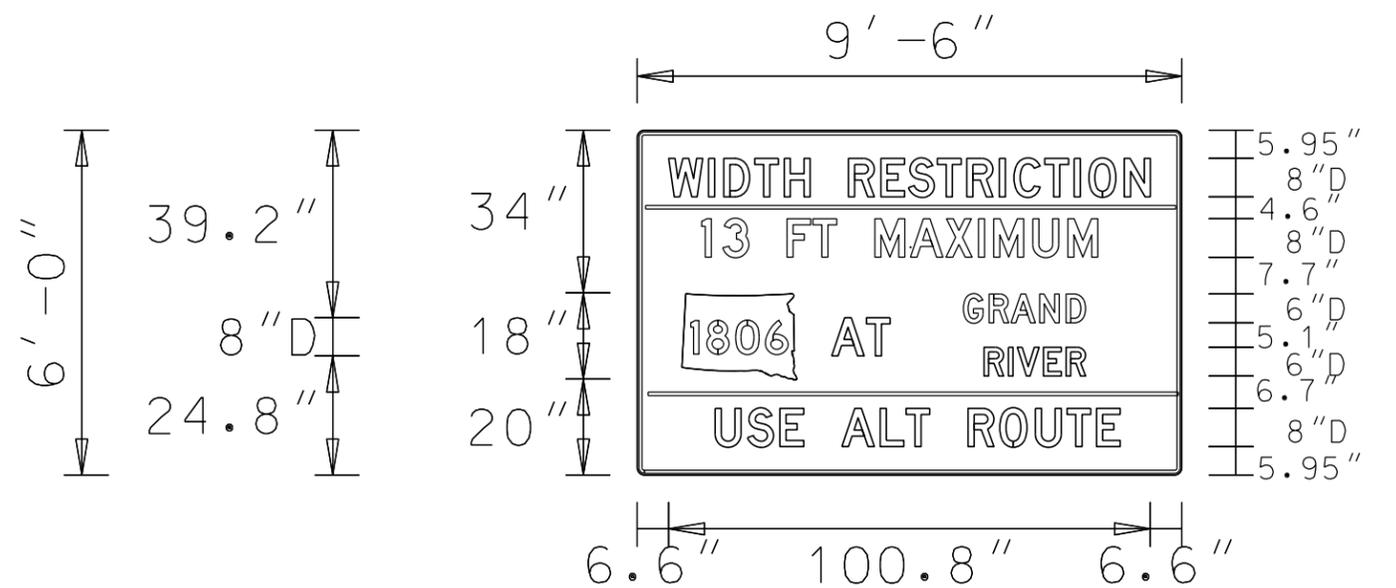
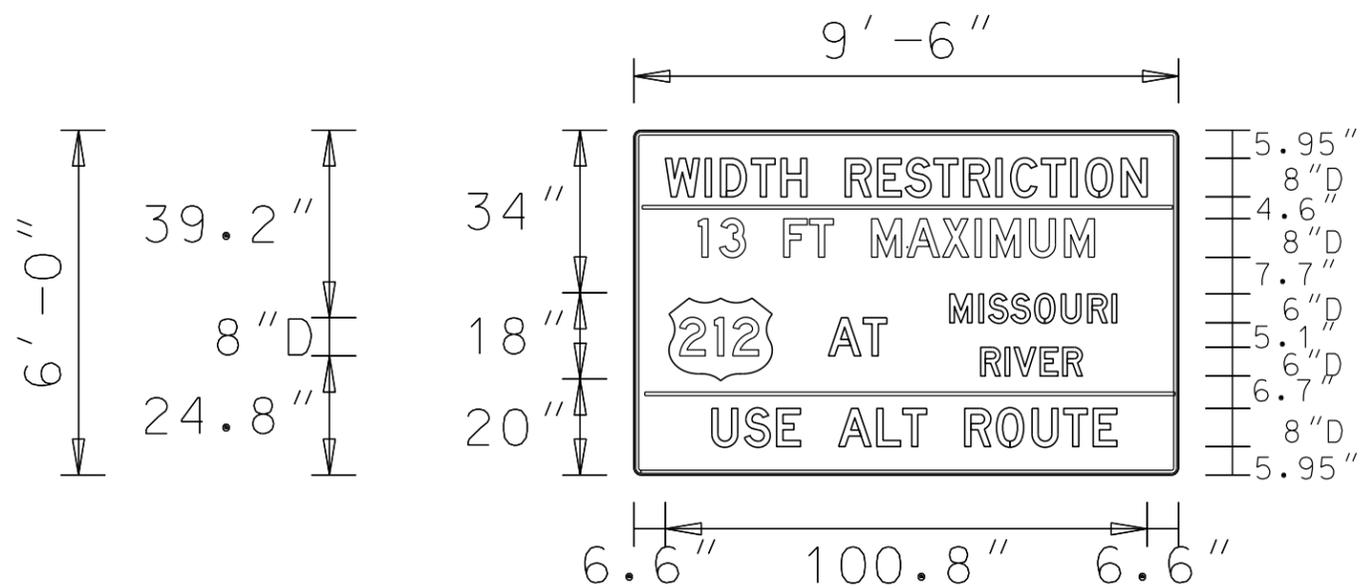
WIDTH RESTRICTION SIGN DESIGN

STATE OF SOUTH DAKOTA	PROJECT	SHEET NO.	TOTAL SHEETS
	NH 0012(152)187, NH 0212(149)208 & P 1806(06)365	C5	C8

Plotting Date: 02/05/2016

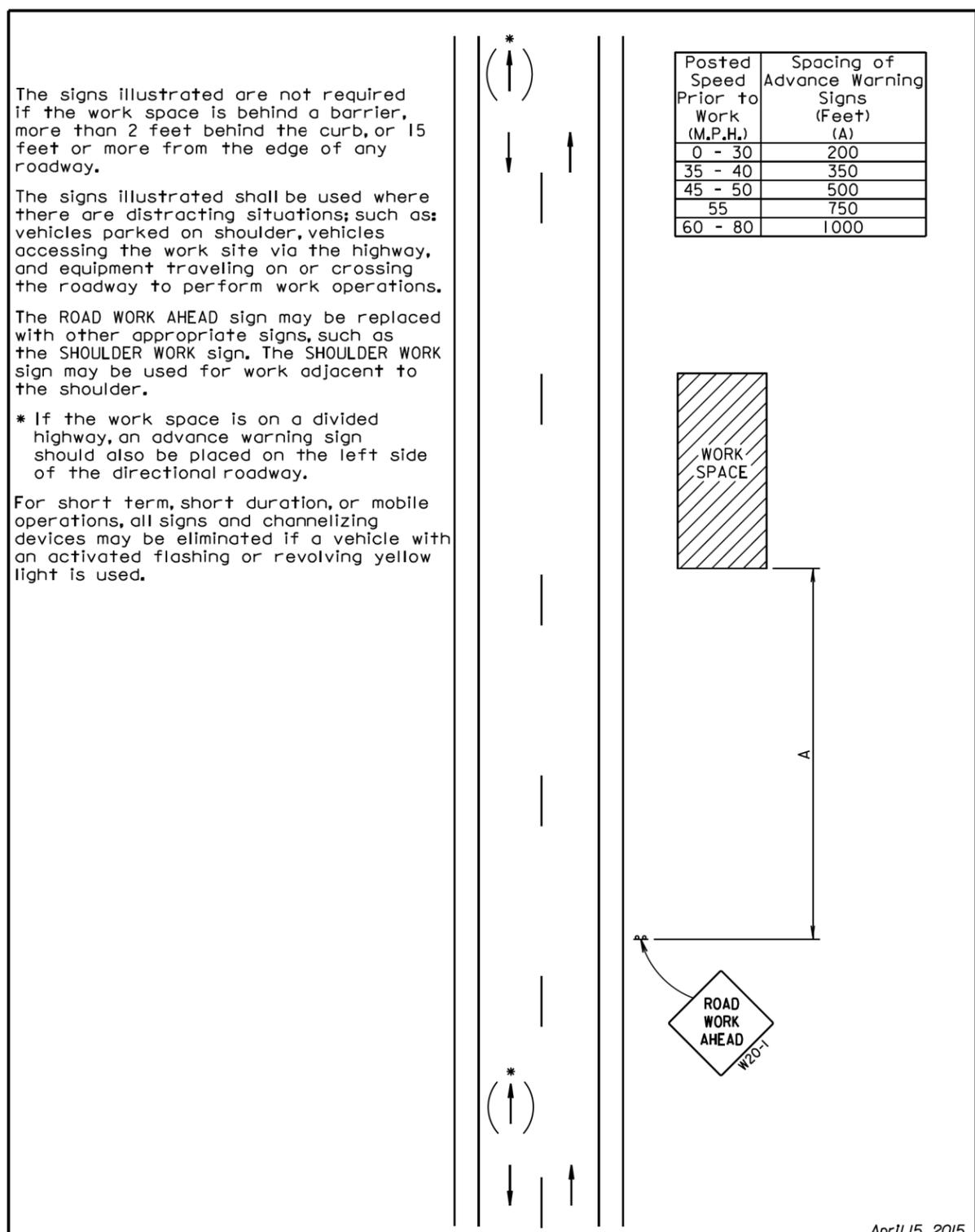


Typical Sign Layout for Overwidth Warning Signs



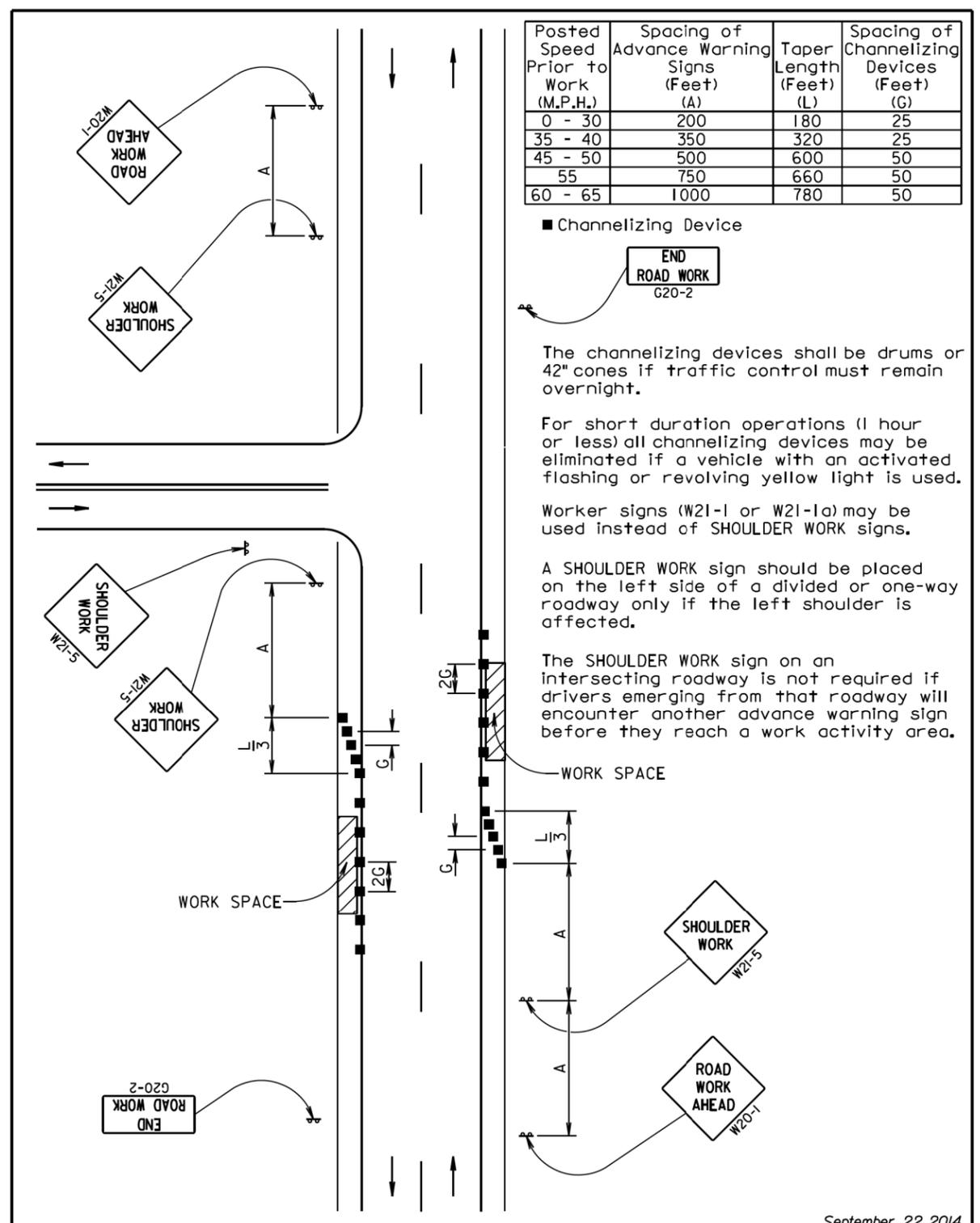
Plotting Date: 02/05/2016

PLOT SCALE - 1:200



April 15, 2015

S D D O T	GUIDES FOR TRAFFIC CONTROL DEVICES WORK BEYOND THE SHOULDER	PLATE NUMBER 634.01
	<i>Published Date: 1st Qtr. 2016</i>	Sheet 1 of 1



September 22, 2014

S D D O T	GUIDES FOR TRAFFIC CONTROL DEVICES WORK ON SHOULDERS	PLATE NUMBER 634.03
	<i>Published Date: 1st Qtr. 2016</i>	Sheet 1 of 1

PLOT NAME - 1
FILE - ... \CORS02A6\STANDARD PLATES.DGN

Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)	Spacing of Channelizing Devices (Feet) (G)
0 - 30	200	25
35 - 40	350	25
45 - 50	500	50
55	750	50
60 - 65	1000	50

- Flagger
- Channelizing Device

For low-volume traffic situations with short work zones on straight roadways where the flagger is visible to road users approaching from both directions, a single flagger may be used.

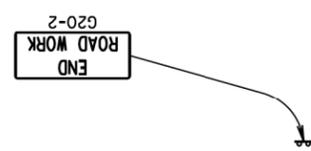
The ROAD WORK AHEAD and the END ROAD WORK signs may be omitted for short duration operations (1 hour or less).

For tack and/or flush seal operations, when flaggers are not being used, the FRESH OIL sign (W21-2) shall be displayed in advance of the liquid asphalt areas.

Flashing warning lights and/or flags may be used to call attention to the advance warning signs.

The channelizing devices shall be drums or 42" cones.

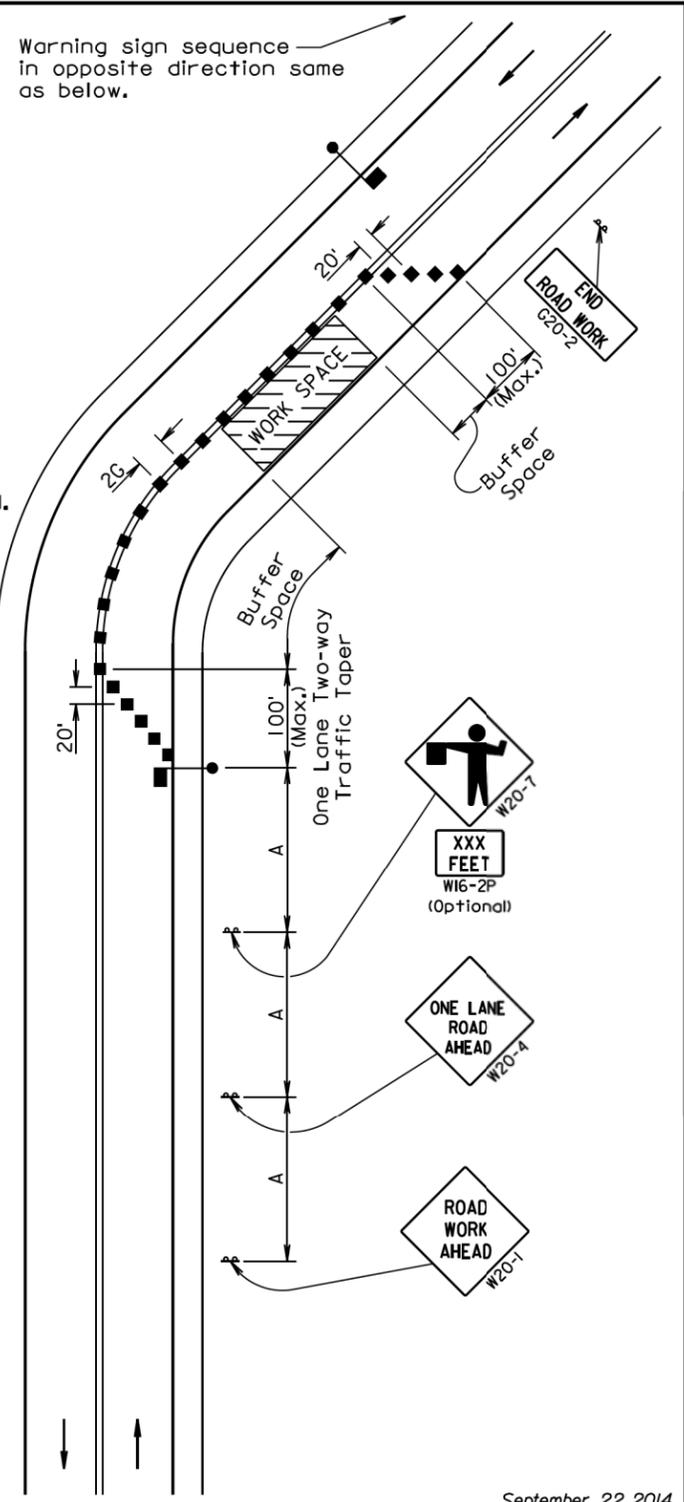
Channelizing devices are not required along the centerline adjacent to work area when pilot cars are utilized for escorting traffic through the work area.



Channelizing devices and flaggers shall be used at intersecting roads to control intersecting road traffic as required.

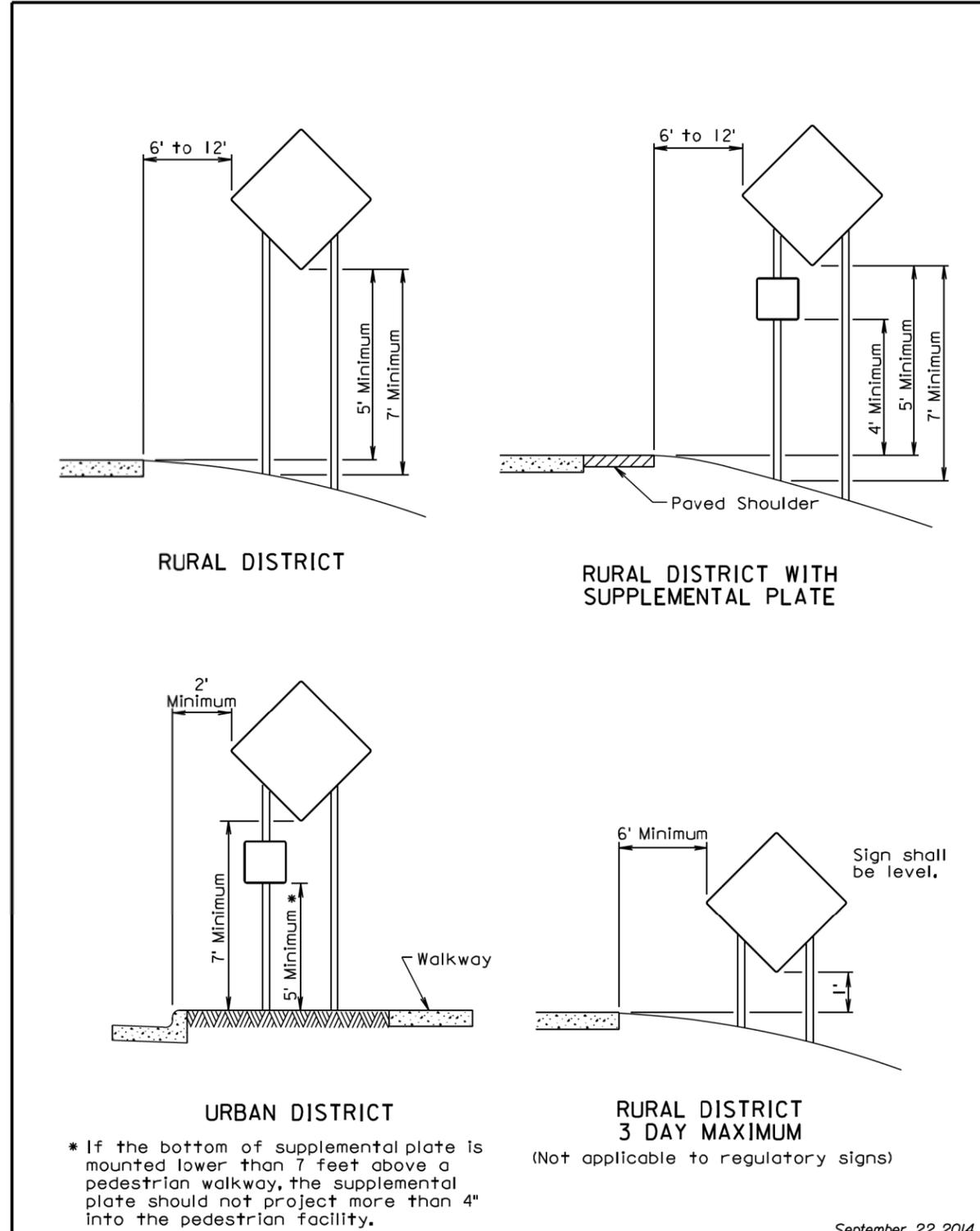
The buffer space should be extended so that the two-way traffic taper is placed before a horizontal or vertical curve to provide adequate sight distance for the flagger and queue of stopped vehicles.

The length of A may be adjusted to fit field conditions.



September 22, 2014

S D D O T	GUIDES FOR TRAFFIC CONTROL DEVICES LANE CLOSURE WITH FLAGGER PROVIDED	PLATE NUMBER 634.23
	<i>Published Date: 1st Qtr. 2016</i>	Sheet 1 of 1

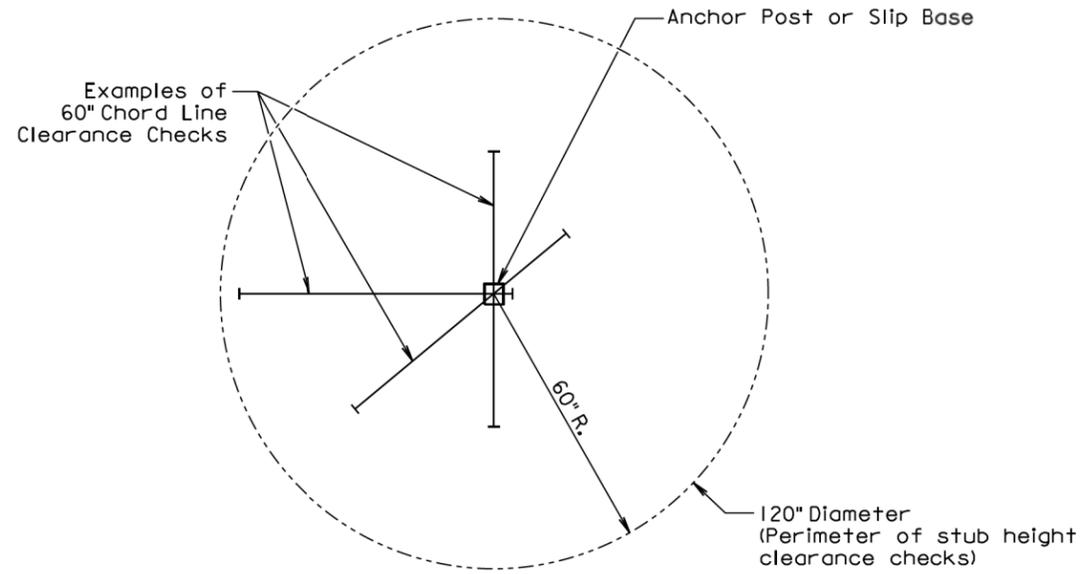


* If the bottom of supplemental plate is mounted lower than 7 feet above a pedestrian walkway, the supplemental plate should not project more than 4" into the pedestrian facility.

September 22, 2014

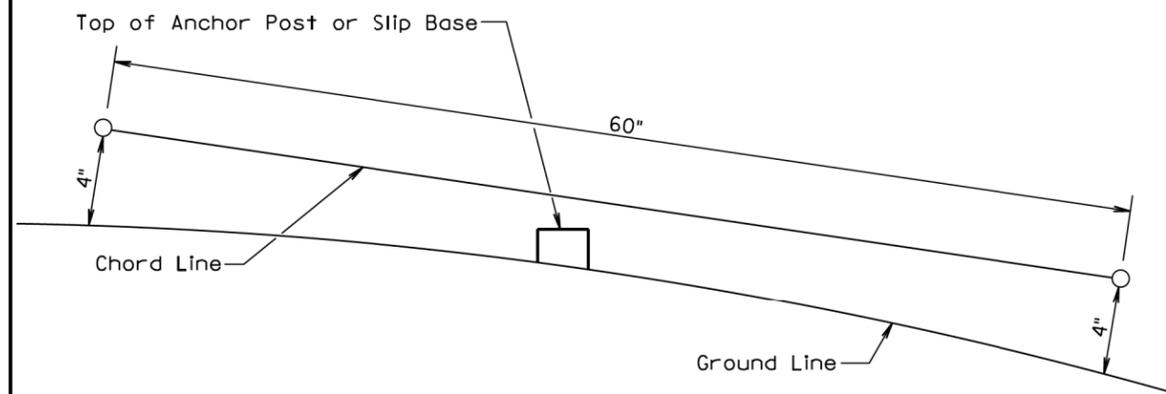
S D D O T	CRASHWORTHY SIGN SUPPORTS (Typical Construction Signing)	PLATE NUMBER 634.85
	<i>Published Date: 1st Qtr. 2016</i>	Sheet 1 of 1

Plotting Date: 02/05/2016



PLAN VIEW

(Examples of stub height clearance checks)



ELEVATION VIEW

GENERAL NOTES:

The top of anchor posts and slip bases SHALL NOT extend above a 60" chord line within a 120" diameter circle around the post with ends 4" above the ground.

At locations where there is curb and gutter adjacent to the breakaway sign support, the stub height shall be a maximum of 4" above the ground line at the localized area adjacent to the breakaway support stub.

The 4" stub height clearance is not necessary for U-channel lap splices where the support is designed to yield (bend) at the base.

July 1, 2005

Published Date: 1st Qtr. 2016

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BREAKAWAY SUPPORT STUB CLEARANCE

PLATE NUMBER
634.99

Sheet 1 of 1