

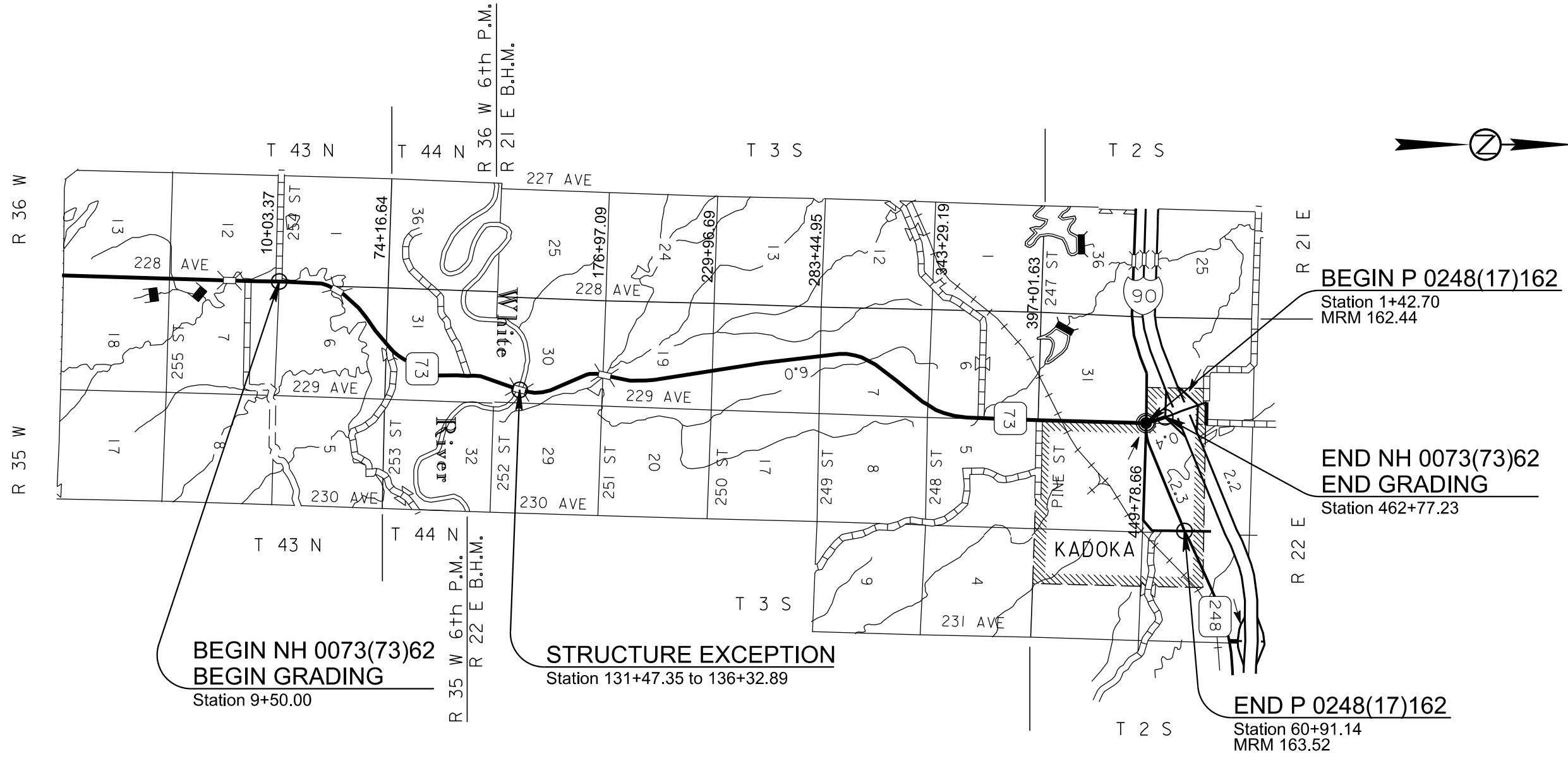
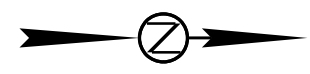
# SECTION C: TRAFFIC CONTROL PLANS

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	NH 0073(73)62 P 0248(17)162	C1	C13

Plotting Date: 08/04/2023

## INDEX OF SHEETS

- C1 General Layout with Index
- C2-C4 Estimate with General Notes and Tables
- C5-C7 Sign Tab and Layout Details
- C8-C13 Standard Plates



Plot Scale - 1:200

Plotted From - TRW11NT26

File - ...WorkingTitleC.dgn

## SECTION C – ESTIMATE OF QUANTITIES

BID ITEM NUMBER	ITEM	QUANTITY	UNIT
260E3500	Temporary Gravel Surfacing	12,740.0	Ton
634E0010	Flagging	6,000.0	Hour
634E0020	Pilot Car	2,750.0	Hour
634E0110	Traffic Control Signs	1,772.0	SqFt
634E0120	Traffic Control, Miscellaneous	Lump Sum	LS
634E0275	Type 3 Barricade	25	Each
634E0630	Temporary Pavement Marking	25.8	Mile
634E1002	Detour and Restriction Signing	288.3	SqFt
634E1215	Contractor Furnished Portable Changeable Message Sign	2	Each

## SEQUENCE OF OPERATIONS

Contractor requests to deviate from the sequence of operations will be submitted in writing to the Engineer for review. Approval of an alternate sequence of operations will only be allowed when the proposed changes meet with the Department's intent for traffic control and sequencing of the work. An alternate sequence will be submitted for review a minimum of one week prior to potential implementation.

- Install fixed support traffic control
- Install preliminary erosion and sediment control
- Install temporary fencing
- Mill and salvage existing surfacing
- Perform grading work
  - Strip topsoil
  - Rough grading
  - Install culverts/box culverts
  - Finish grading
- Place base course
- Install prime and blotter
- Install permanent pavement markings
- Install rip rap
- Install permanent erosion and sediment control
- Install permanent fence
- Remove traffic control

## GENERAL TRAFFIC CONTROL

Existing guide, route, informational logo, regulatory, and warning signs will be temporarily reset and maintained during construction. Removing, relocating, covering, salvaging, and resetting of existing traffic control devices, including delineation, will be the responsibility of the Contractor. Cost for this work will be incidental to the contract unit prices for the various items unless otherwise specified in the plans. Any delineators and signs damaged or lost will be replaced by the Contractor at no cost to the State.

All temporary traffic control sign locations will be set in the field by the Contractor and verified by the Engineer prior to installation.

Portable sign supports will not be located on sidewalks, bicycle facilities, or other areas designated for pedestrian or bicycle traffic.

All construction operations will be conducted in the general direction of traffic movement.

Traffic will be maintained through the project by flagger & pilot car during the day. At night, a 28' width will be maintained to carry two-way traffic through the project.

If there is a discrepancy between the traffic control plans, standard plates, and the MUTCD, whichever is more stringent will be used, as determined by the Engineer.

Unless otherwise stated in these plans, work will not be allowed during hours of darkness.

Fixed location signing placed more than 4 calendar days prior to the start of construction will be covered or laid down until the time of construction. The covers must be approved by the Engineer prior to installation. The cost of materials, labor, and equipment necessary to complete this work will be incidental to other contract items. No separate payment will be made.

All fixed location signs, sign posts, and breakaway bases will be removed within 7 calendar days following pavement marking.

All haul trucks will be equipped with an additional flashing amber light that is visible from the backside of the haul truck. The costs for the flashing amber lights will be incidental to the various related contract items.

At no time will a vertical drop-off of greater than 3 inches be left overnight adjacent to the traveled way. The Contractor will utilize embankment material to ensure a 3-inch vertical drop-off is not exceeded. The slope of the embankment material will not be steeper than a 4:1 within 30 feet of the traveled way.

The Contractor will furnish, install, maintain, and remove TRUCK CROSSING (W8-6) signs daily. The TRUCK CROSSING signs will be displayed always when haul vehicles are hauling material. When hauling conditions no longer exist, the signs will be covered or removed from view. The exact number and location will be determined during construction. Payment for additional signs will be based on the contract unit price per square foot for "Traffic Control Signs".

## PIPE REPLACEMENT

Pipe replacement will be phased half at a time to maintain an open lane of traffic. In the event that the lane closure needs to stay up overnight traffic will be set up as a lane closure using stop signs, standard plate 634.25.

## DIVERSIONS FOR BOX CULVERT REPLACEMENT

Diversions will use drums or 42" cones spaced at 25' as shown on Standard Plate No. 634.28. The 4"x4" White Delineator Back-to-Back delineators spaced at 50' on remaining sections of the detour on both sides and will be incidental to the contract lump sum price for Contract Traffic Control Miscellaneous bid item.

## FLAGGING

Operations will be conducted so that the traveling public will not have to wait longer than 15 minutes at the flagger station.

Additional flagger warning signs and flagger hours have been included in the Estimate of Quantities for use on intersecting roads. These flaggers will be used as directed by the Engineer and will be used primarily during daytime hours. Also included in the Estimate of Quantities are WAIT FOLLOW PILOT CAR signs for use on low volume intersecting roads as determined by the Engineer. WAIT FOLLOW PILOT CAR signs will not block the view of the stop sign.



It is required that the flaggers and pilot car operators be able to communicate with one another. If an emergency vehicle needs to pass through the project, the Contractor will be required to expedite traffic movement. All costs associated with this will be incidental to the contract unit price per hour for "Flagging".

## TEMPORARY PAVEMENT MARKING

The total length of no passing zone on this project is estimated to be 4.13 miles.

It is estimated that 17 DO NOT PASS (R4-1) and 18 PASS WITH CARE (R4-2) signs will be required to mark the no passing zones, should the Contractor elect to use these signs.

Temporary Pavement Marking will be the responsibility of the Contractor. The cost of materials, labor and equipment necessary to complete this work will be incidental to the contract unit price per mile of Temporary Pavement Marking. Temporary Pavement Markings will be applied when needed, as determined by the Engineer. Plans quantity for Temporary Pavement Marking was calculated to be three times the length of the project, one application for the prime, one application for the surface treatment and one application for the fog seal. Temporary Pavement Marking paint may be used for the primed surface, however Temporary Flexible Vertical Markers must be used for the surface treatment and the fog seal surfaces.

Prior to nightfall, tabs will be required to mark centerline on segment of roadway where existing centerline markings have been removed and new markings have not been installed.

**TEMPORARY PAVEMENT MARKING (Continued)**

The temporary pavement markings for the prime and blotter will be applied upon completion of the respective cure periods. The cure period will be determined by the Engineer on the project but will not exceed 72 hours. During the cure period, "Fresh Oil" (W21-2) and "No Center Line" (W8-12) signs will be installed at the beginning of the application area and will be repeated at two-mile intervals in accordance with the MUTCD. After the blotter is applied and cured, all three lane lines must be temporary striped.

Covers on the tabs will be sufficiently secured to prevent traffic from dislodging the cover and when removed, the covers will be properly disposed of. The Contractor will remove and properly dispose of the tabs after permanent pavement marking is applied. Method of removal will be nondestructive to the road surface and will be accomplished within one week of completion of the permanent pavement marking.

Full reflectivity of all temporary flexible vertical markers (tabs) is required at all times. The Contractor will be required to replace any missing or non-reflective tabs after each installation as detailed below at no additional cost to the State.

In the absence of a signed lane closure or pilot car operation, FLAGGER (W20-7) symbol signs and flaggers, or a shadow vehicle with rotating yellow lights or strobe lights will be positioned on the shoulder in advance of workers for both directions of traffic during the installation and removal of the temporary flexible vertical markers (tabs). The traffic control device used will be moved intermittently to provide proper warning of the work operation. A ROAD WORK AHEAD (W20-1) sign, a WORKER (W21-1) symbol sign or a BE PREPARED TO STOP (W3-4) sign will be mounted on the rear of the shadow vehicle. The method of traffic control used by the Contractor for this work must be approved by the Engineer.

**TEMPORARY GRAVEL SURFACING (TRAVEL GRAVEL)**

An estimated 12,740 tons of Temporary Gravel Surfacing is provided for this project and may be used as determined necessary and at locations approved by the Engineer. The Temporary Gravel Surfacing is provided to protect completed subgrade work from damage by traffic or equipment during wet weather. Temporary Gravel Surfacing may be used on the finished subgrade prior to Base Course, Salvaged installation and/or to protect exposed subgrade prior to the completion of the finished subgrade surface. The Engineer will direct the Contractor to place Temporary Gravel Surfacing if adverse weather is imminent and/or the usage of the material will help prevent road closures.

The Temporary Gravel Surfacing will be obtained from the stockpile site(s) provided by the Contractor from the salvaged granular material produced on this project and may be used without further gradation testing.

Compaction to a specified density is not required.

The Contractor will be required to salvage as much of the Temporary Gravel Surfacing as possible prior to resuming construction of the subgrade. The salvaged Temporary Gravel Surfacing may be used again on the project.

Temporary gravel surfacing will be paid for at the contract unit price per ton for Temporary Gravel Surfacing. Measurement of the Temporary Gravel Surfacing will be by use of scale, loader scale, or as approved by the Engineer. The contract unit price will include all costs associated with hauling, placing, compacting, maintaining, salvaging and stockpiling the material. For Temporary Gravel Surfacing salvaged after use on the subgrade and used again on the project, the amount used again will be measured and paid for again at the contract unit price per ton.

The temporary gravel surfacing remaining at the Contractor's stockpile site(s) that is not used on this project will be hauled to the Kadoka SDDOT Maintenance Yard and paid for at the contract unit price per ton for Haul and Stockpile Granular Material.

**CONTACTOR FURNISHED PORTABLE CHANGEABLE MESSAGE SIGN**

One week prior to starting work affecting the traveling public, portable changeable message signs (PCMS) will be installed at locations detailed in the plans to notify drivers of the upcoming construction. The Contractor will program the portable changeable message signs with the following message:

**ROAD WORK  
STARTS (Date)**

When work begins that will affect traffic patterns, the Contractor will re-program the PCMS with the messages as detailed in the plans.

The Contractor will set up and utilize one portable changeable message sign on SD 73 for northbound traffic at the SD 73 & SD 44 Junction and one portable changeable message sign on SD 73 for southbound traffic at the SD 73 & I-90 Junction. These will be used to inform traffic during periods when the project is closed due to wet weather or inform traffic of fresh oil ahead. The portable message signs will display the following messages, in sequence, when used during periods of wet weather closures:

Due To	SD 73
Muddy	N/S
Condition	is Closed

The portable message signs will display the following messages, in sequence, when fresh oil is on the driving surface ahead:

Fresh Oil	Consider
Ahead	Alternate
XX Miles	Route

When the portable changeable message signs are not being utilized for wet weather closures, they will be used as directed by the Engineer.

The portable changeable message signs will be paid for at the contract unit price per each for Contractor Furnished Portable Changeable Message Sign. This payment will be full compensation for furnishing, mobilizing, operating, and maintaining the signs for the duration of this project.

The portable changeable message signs will be programmed to use standard abbreviations and working as described in the MUTCD or as directed by the Engineer.

**ROAD CLOSURE DUE TO INCLEMENT WEATHER**

In the event a road closure is required due to inclement weather and impassible conditions, the Contractor will set up and maintain all traffic control devices required to close the road and inform the traveling public, as laid out in these plans. There are enough traffic control devices in these plans to set up (2) flagger stations with lighting and 2 full closure stations. There will be flaggers on duty 24 hours a day while the road is closed. It is also expected that the Contractor will have equipment and operators on standby to escort emergency vehicles through the closure if needed.

**LIGHTING FOR NIGHTTIME ROAD CLOSURE FLAGGING**

When the road is closed due to weather, flagger stations will be lighted between sunset and sunrise. Non-glare light sources will be provided.

Light levels are as defined in Section 2.9.2 of NCHRP 476.

Light in conformance with Level III is to be provided where labor intensive work is being completed such as during hand work, pavement sawing, project inspection, materials testing, and flagging.

Acceptable light sources will be Contractor furnished stand-alone lights or vehicle/equipment mounted lights. Stand-alone units will be marked with a minimum of two reflectorized drums on an approaching traffic side.

Cost for this lighting will be included in the contract lump sum price for "Traffic Control, Miscellaneous".

**TRAFFIC CONTROL FOR ASPHALT SURFACE TREATMENT**

The Contractor will furnish, install, and maintain LOOSE GRAVEL (W8-7) signs with 40 MPH (W13-1P) advisory speed plaques upon start of surface treatment operations at each end of the segment and on either side of intersecting asphalt roads and major intersections as determined by the Engineer. In addition, LOOSE GRAVEL signs with 40 MPH advisory speed plaques will be installed at no more than 4 mile intervals throughout each segment. The 40 MPH advisory speed plaque should not be installed with LOOSE GRAVEL signs in areas where the posted speed limit is less than 40 MPH. LOOSE GRAVEL signs and 40 MPH advisory speed plaques will be covered or removed from view when they are not applicable.

ROAD WORK NEXT XX MILES (G20-1), LOOSE GRAVEL (W8-7), and END ROAD WORK (G20-2) signs are the only signs that need to be mounted on fixed location breakaway sign supports, as shown on the plan layout. ROAD WORK AHEAD (W20-1), FLAGGER (W20-7), ONE LANE ROAD AHEAD (W20-4), and TRUCK CROSSING (W8-6) signs may be mounted on portable supports. Signs mounted on portable supports will be moved as necessary to keep current with the work activities.

Until the end of each day's chip seal operations, at the discretion of the Contractor, additional flaggers and FLAGGER (W20-7) symbol signs will be provided to alert the traveling public entering completed portions of the project to the potential of airborne chips.

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**TRAFFIC CONTROL FOR ASPHALT SURFACE TREATMENT (Continued)**

The flaggers will provide each motorist with a printed notice on the Contractor's letterhead similar to the one shown below. Cost of the notice will be incidental to other contract items.

"CONTRACTOR'S LETTERHEAD"

THIS HIGHWAY IS BEING RESURFACED WITH A ROCK CHIP SEAL COAT.

THIS TYPE OF CONSTRUCTION HAS THE POTENTIAL OF CAUSING VEHICLE DAMAGE SUCH AS CHIPPED WINDSHIELDS AND BROKEN HEADLIGHTS DUE TO ROCKS BEING THROWN BY HIGH SPEED ONCOMING OR PASSING TRAFFIC.

YOU MAY WISH TO CONSIDER TAKING AN ALTERNATE ROUTE. IF YOU PROCEED, KEEP TO THE RIGHT AND DRIVE 40 MPH OR LESS. ANOTHER FLAGGER AND A PILOT CAR WILL BE ESCORTING YOU AROUND THE OIL SEAL COAT APPLICATION AREA.

THANK YOU.

**PRESS RELEASE ANNOUNCEMENTS**

The SDDOT will prepare a press release to be released 5 days prior to any phase change or any other major change that affects traffic flow. The SDDOT will be responsible to keep law enforcement, emergency services, and the traveling public notified of changes in project access. The Contractor will provide the Engineer with pertinent information 7 days prior to any phase change or any other major change that affects traffic flow.

**INCIDENTS**

An incident is an emergency road user occurrence, a natural disaster, or other unplanned event that affects or impedes the normal flow of traffic such as a crash, hazardous materials spill, or other event.

The Contractor will set up a meeting prior to start of work to plan and coordinate responses to an incident. The Contractor will invite the Department of Transportation, the South Dakota Highway Patrol, the Jackson County Sheriff and local emergency response entities to the meeting.

The Contractor will assist to maintain traffic as required by these plan notes and as agreed to at that meeting.

Emergency vehicle access through the project will be considered and discussed at the meeting.

The Contractor may be required to modify messages on portable changeable message signs or relocate portable changeable message signs, and to provide flaggers to direct or detour traffic. The Contractor should be prepared to relocate advance warning signs if determined to be necessary for a major traffic incident lasting more than two hours. Fixed location ground mounted signs may be covered, and additional portable signs provided.

No additional payment will be made for the modification of portable changeable message sign messages or the relocation of portable changeable message signs. Cost for the relocation of an advance warning sign due to an incident will be 50% of the designated sign rate. Flaggers will be paid for at the contract unit price per hour for "Flagging".



### ITEMIZED LIST FOR TRAFFIC CONTROL SIGNS

SIGN CODE	SIGN DESCRIPTION	CONVENTIONAL ROAD			
		NUMBER	SIGN SIZE	SQFT PER SIGN	SQFT
R1-1	STOP	2	30"	5.2	10.4
R4-7	KEEP RIGHT (symbol)	2	24" x 30"	5.0	10.0
R11-2	ROAD CLOSED	2	48" x 30"	10.0	20.0
R11-4	ROAD CLOSED TO THRU TRAFFIC	2	60" x 30"	12.5	25.0
W1-4	REVERSE CURVE (L or R)	3	48" x 48"	16.0	48.0
W1-6	LARGE ARROW (one direction)	3	48" x 24"	8.0	24.0
W3-1	STOP AHEAD (symbol)	2	48" x 48"	16.0	32.0
W3-4	BE PREPARED TO STOP	4	48" x 48"	16.0	64.0
W8-1	BUMP	12	48" x 48"	16.0	192.0
W8-6	TRUCK CROSSING	4	48" x 48"	16.0	64.0
W8-7	LOOSE GRAVEL	10	48" x 48"	16.0	160.0
SPECIAL	WINDROW	2	48" x 48"	16.0	32.0
W8-11	UNEVEN LANES	6	48" x 48"	16.0	96.0
W8-12	NO CENTER LINE	8	48" x 48"	16.0	128.0
W8-17	SHOULDER DROP-OFF (symbol)	6	48" x 48"	16.0	96.0
W13-1P	ADVISORY SPEED (plaque)	13	30" x 30"	6.3	81.9
W16-2P	___ FEET (supplemental distance plaque)	6	30" x 24"	5.0	30.0
W20-1	ROAD WORK AHEAD	14	48" x 48"	16.0	224.0
W20-4	ONE LANE ROAD AHEAD	4	48" x 48"	16.0	64.0
W20-7	FLAGGER (symbol)	6	48" x 48"	16.0	96.0
W21-1	WORKERS (symbol)	2	48" x 48"	16.0	32.0
W21-2	FRESH OIL	8	48" x 48"	16.0	128.0
W21-3	ROAD MACHINERY AHEAD	2	48" x 48"	16.0	32.0
W21-5	SHOULDER WORK	2	48" x 48"	16.0	32.0
SPECIAL	WAIT FOLLOW PILOT CAR	4	30" x 18"	3.8	15.2
G20-1	ROAD WORK NEXT ___ MILES	2	36" x 18"	4.5	9.0
G20-2	END ROAD WORK	5	36" x 18"	4.5	22.5
-	TYPE 2 OBJECT MARKER BACK TO BACK	4	6" x 12"	1.0	4.0
		<b>CONVENTIONAL ROAD TRAFFIC CONTROL SIGNS SQFT 1772.0</b>			

Plot Scale - 1:200

Plotted From - TRW11N126

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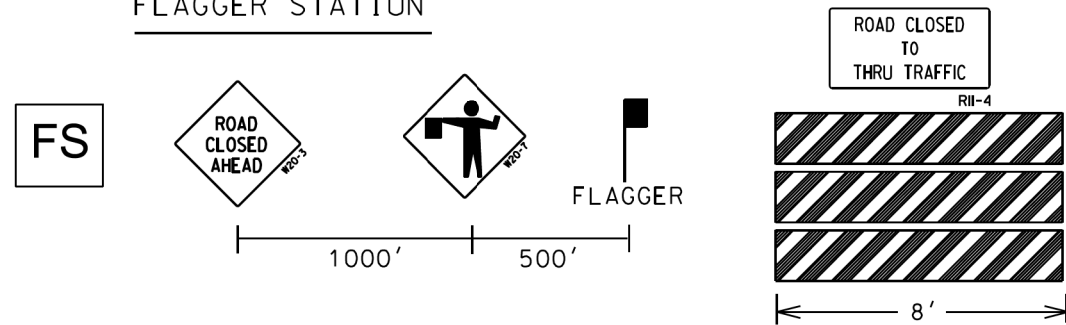
# ROAD CLOSURE LAYOUT

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	NH 0073(73)62 P 0248(17)162	C6	C13

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- MS1 = Portable Changeable Message Sign to be located south of the SD 73 and I-90 junction.
- MS2 = Portable Changeable Message Sign to be located north of the SD 73 - SD 44 Junction.

## FLAGGER STATION

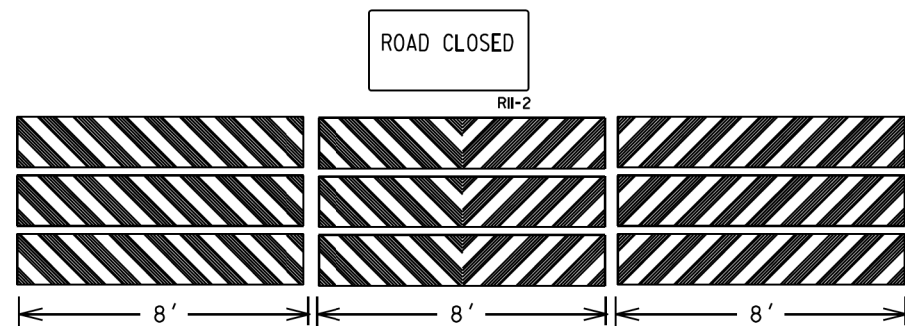


## NOTES:

All flagger locations are to be illuminated during night time hours.

Flagger Stations will be located at the stations as shown or as approved by the Engineer.

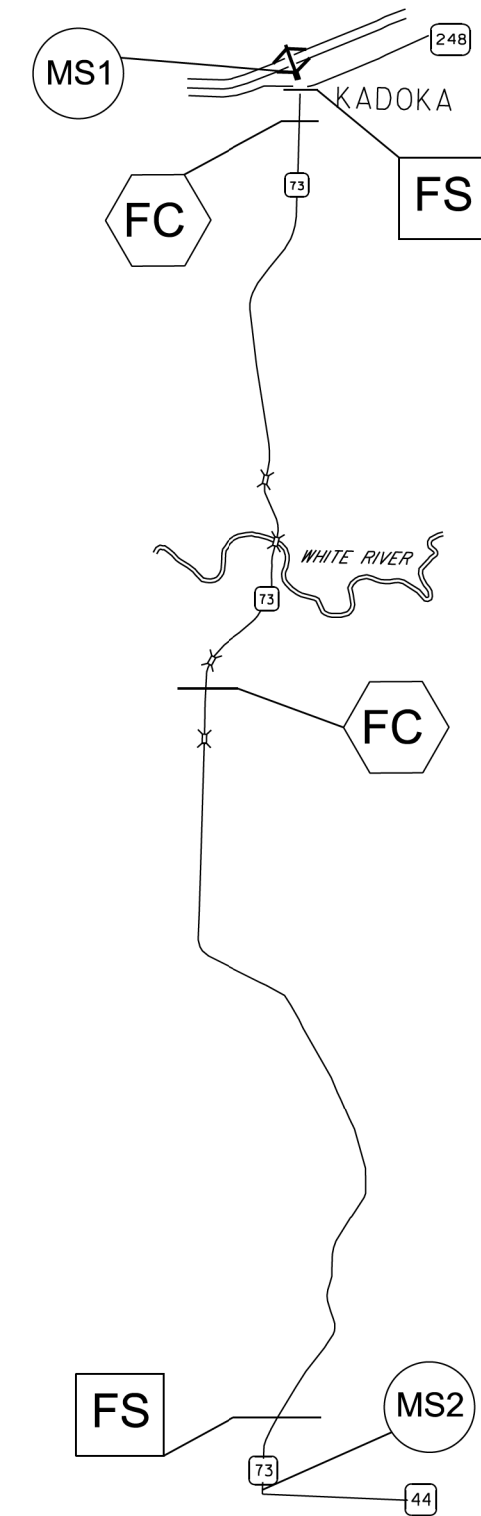
## FC FULL ROAD CLOSURE



## NOTES:

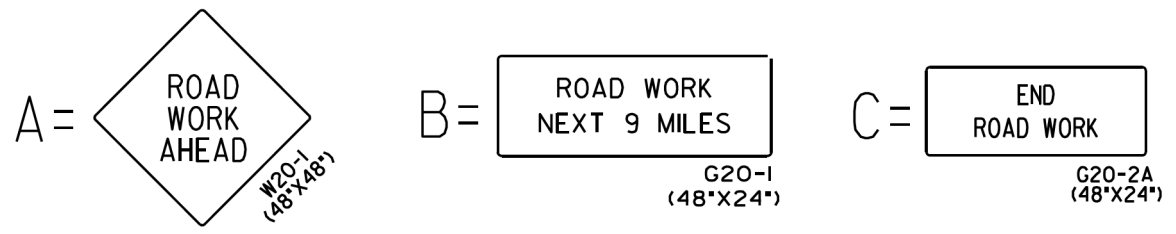
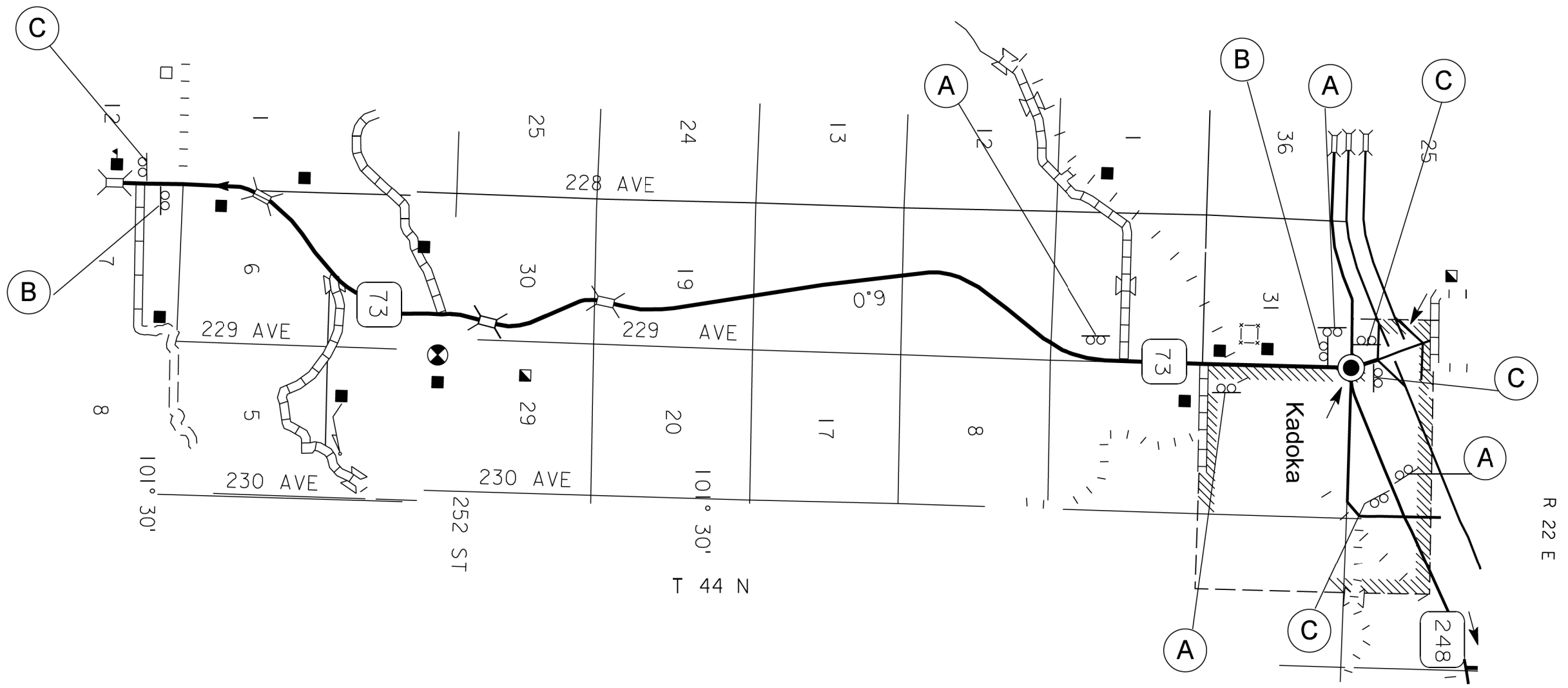
Three Type 3 Barricades will be required at each Full Road Closure Location.

Full Road Closure locations will be placed as close as reasonable to the impassable area to minimize travel restrictions to local residents.



NOT TO SCALE

# FIXED LOCATION SIGN LAYOUT



PLOT SCALE - 1\"/>

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PLOT NAME - 1

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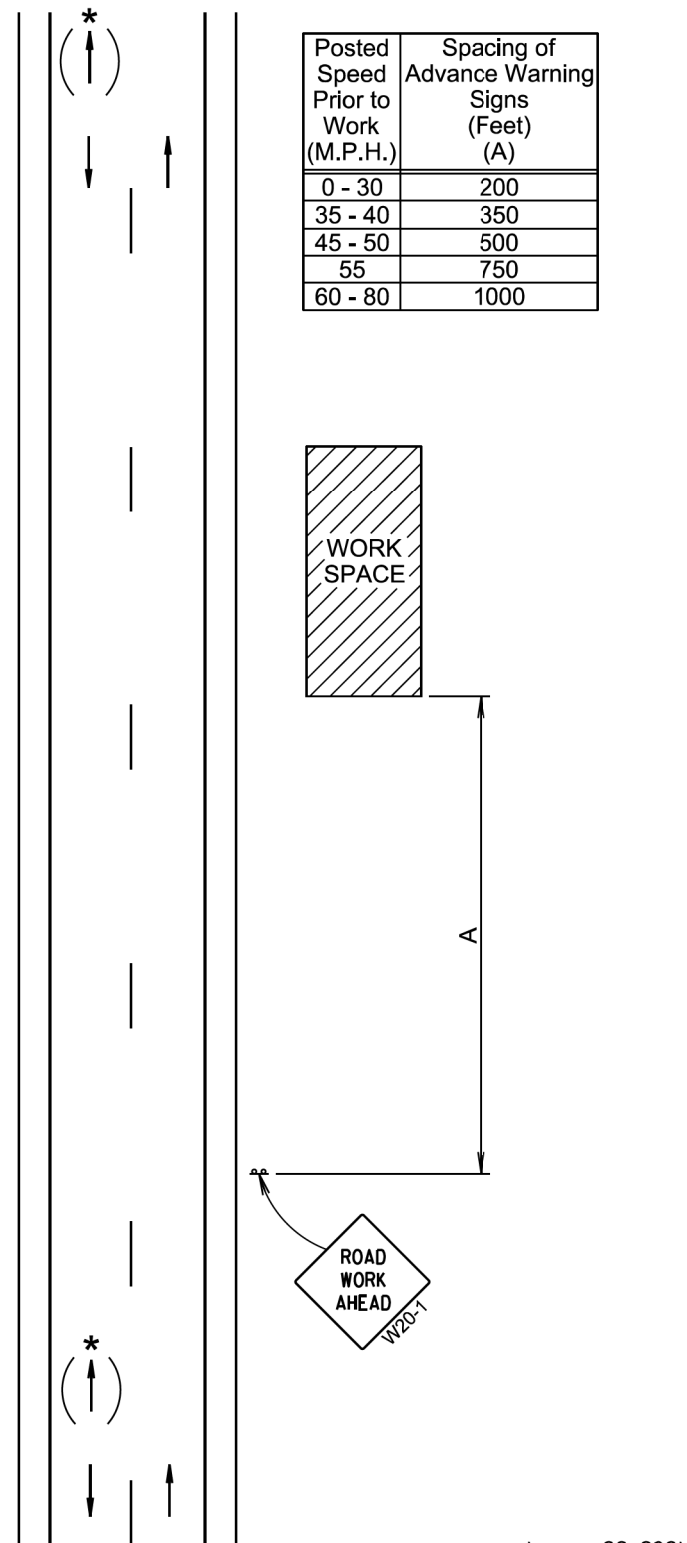
The signs illustrated are not required if the work space is behind a barrier, more than 2 feet behind the curb, or 15 feet or more from the edge of any roadway.

The signs illustrated will be used where there are distracting situations; such as: vehicles parked on shoulder, vehicles accessing the work site via the highway, and equipment traveling on or crossing the roadway to perform work operations.

The ROAD WORK AHEAD sign may be replaced with other appropriate signs, such as the SHOULDER WORK sign. The SHOULDER WORK sign may be used for work adjacent to the shoulder.

\* If the work space is on a divided highway, an advance warning sign should also be placed on the left side of the directional roadway.

For short term, short duration, or mobile operations, all signs and channelizing devices may be eliminated if a vehicle with an activated flashing or revolving yellow light is used.

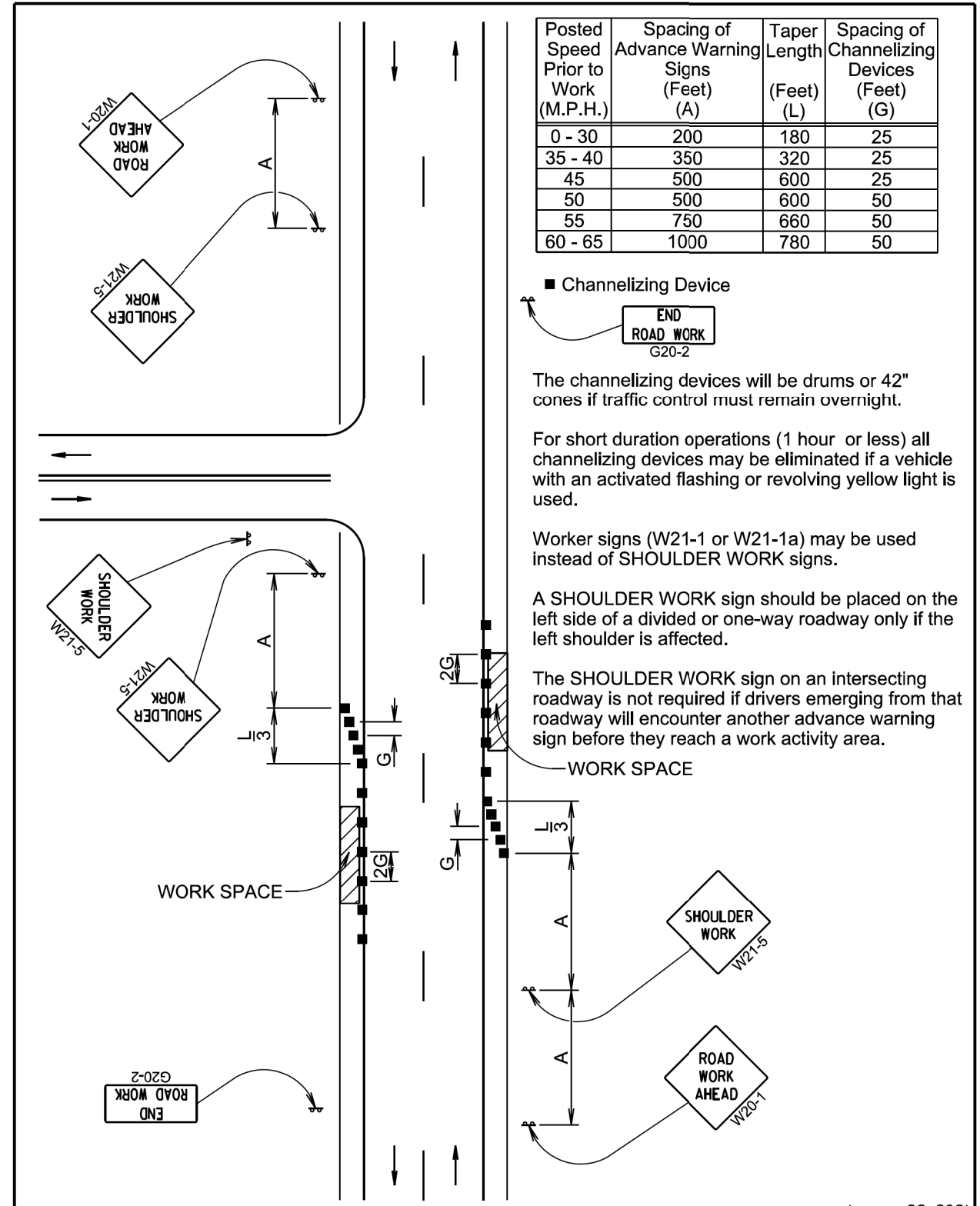


Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)
0 - 30	200
35 - 40	350
45 - 50	500
55	750
60 - 80	1000



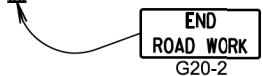
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<b>S D D O T</b>	<b>WORK BEYOND THE SHOULDER</b>	PLATE NUMBER <b>634.01</b>
	Published Date: 2025	Sheet 1 of 1



Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)	Taper Length (Feet) (L)	Spacing of Channelizing Devices (Feet) (G)
0 - 30	200	180	25
35 - 40	350	320	25
45	500	600	25
50	500	600	50
55	750	660	50
60 - 65	1000	780	50

■ Channelizing Device



The channelizing devices will be drums or 42" cones if traffic control must remain overnight.

For short duration operations (1 hour or less) all channelizing devices may be eliminated if a vehicle with an activated flashing or revolving yellow light is used.

Worker signs (W21-1 or W21-1a) may be used instead of SHOULDER WORK signs.

A SHOULDER WORK sign should be placed on the left side of a divided or one-way roadway only if the left shoulder is affected.

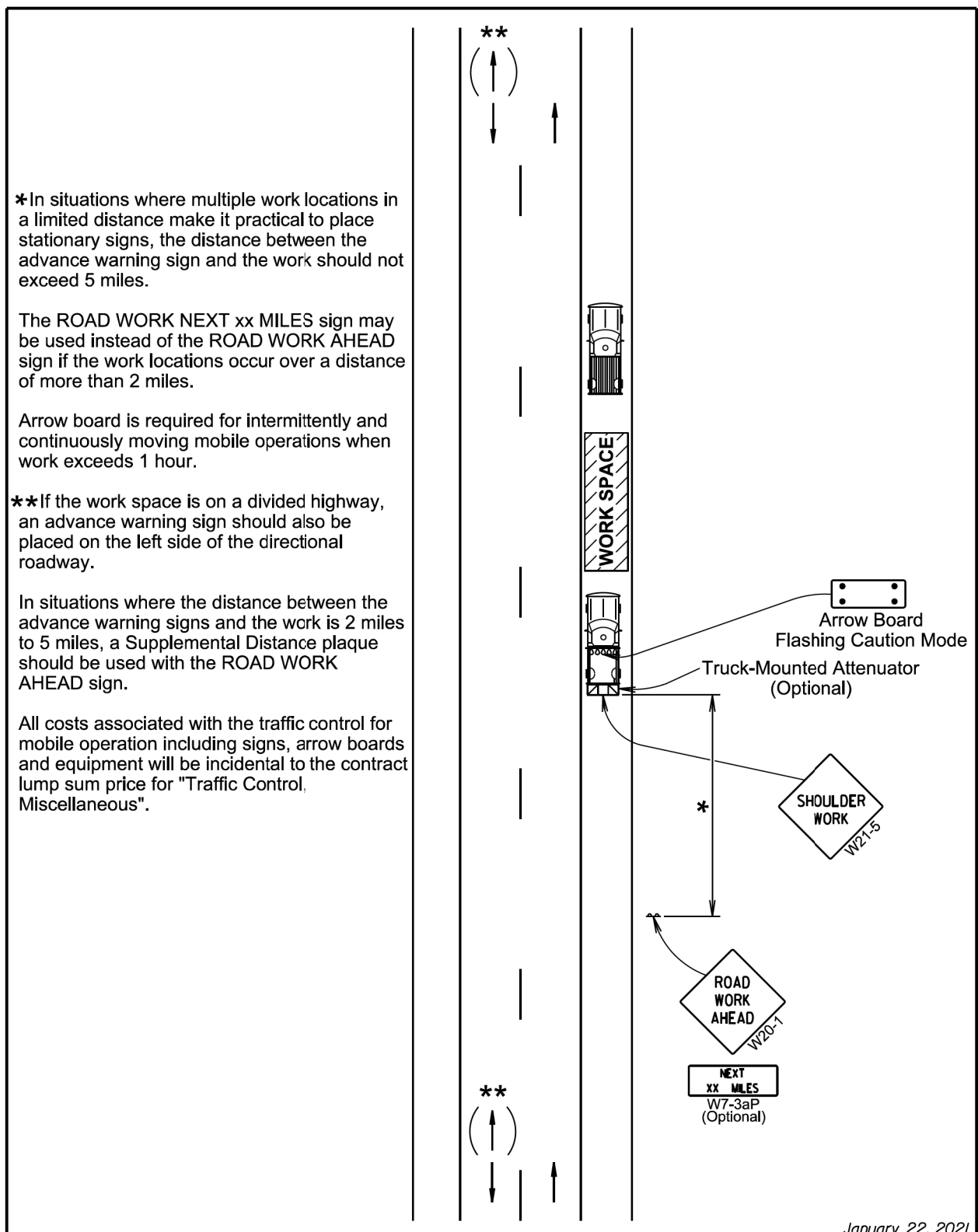
The SHOULDER WORK sign on an intersecting roadway is not required if drivers emerging from that roadway will encounter another advance warning sign before they reach a work activity area.

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<b>S D D O T</b>	<b>WORK ON SHOULDERS</b>	PLATE NUMBER <b>634.03</b>
	Published Date: 2025	Sheet 1 of 1



Plot Scale - 1:200



\*In situations where multiple work locations in a limited distance make it practical to place stationary signs, the distance between the advance warning sign and the work should not exceed 5 miles.

The ROAD WORK NEXT xx MILES sign may be used instead of the ROAD WORK AHEAD sign if the work locations occur over a distance of more than 2 miles.

Arrow board is required for intermittently and continuously moving mobile operations when work exceeds 1 hour.

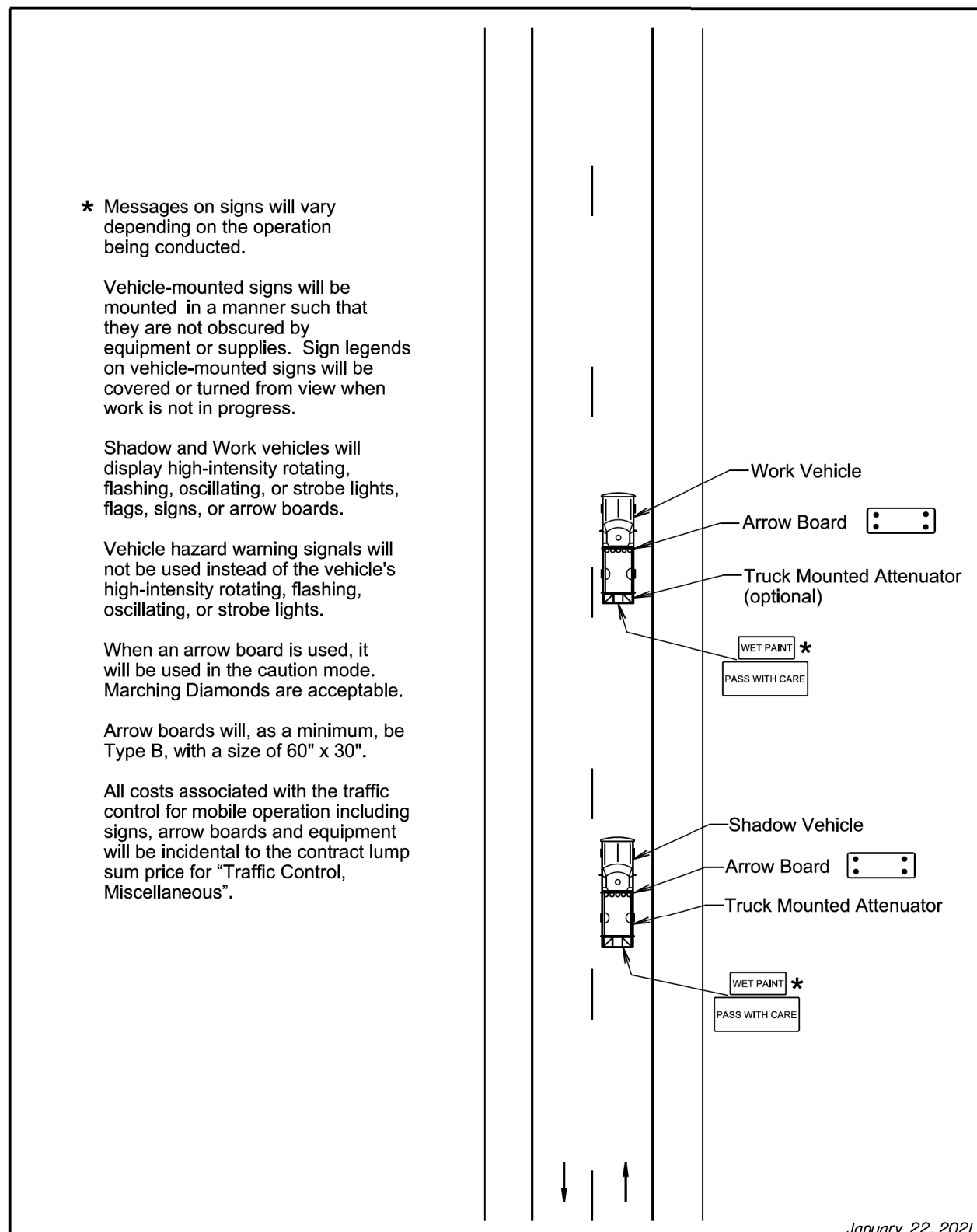
\*\*If the work space is on a divided highway, an advance warning sign should also be placed on the left side of the directional roadway.

In situations where the distance between the advance warning signs and the work is 2 miles to 5 miles, a Supplemental Distance plaque should be used with the ROAD WORK AHEAD sign.

All costs associated with the traffic control for mobile operation including signs, arrow boards and equipment will be incidental to the contract lump sum price for "Traffic Control, Miscellaneous".

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<i>Published Date: 2025</i>	S D D O T	MOBILE OPERATIONS ON SHOULDERS	PLATE NUMBER 634.04
			Sheet 1 of 1



\* Messages on signs will vary depending on the operation being conducted.

Vehicle-mounted signs will be mounted in a manner such that they are not obscured by equipment or supplies. Sign legends on vehicle-mounted signs will be covered or turned from view when work is not in progress.

Shadow and Work vehicles will display high-intensity rotating, flashing, oscillating, or strobe lights, flags, signs, or arrow boards.

Vehicle hazard warning signals will not be used instead of the vehicle's high-intensity rotating, flashing, oscillating, or strobe lights.

When an arrow board is used, it will be used in the caution mode. Marching Diamonds are acceptable.

Arrow boards will, as a minimum, be Type B, with a size of 60" x 30".

All costs associated with the traffic control for mobile operation including signs, arrow boards and equipment will be incidental to the contract lump sum price for "Traffic Control, Miscellaneous".

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<i>Published Date: 2025</i>	S D D O T	MOBILE OPERATIONS ON 2-LANE ROAD	PLATE NUMBER 634.06
			Sheet 1 of 1

Plotted From - TRW11NT26

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Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)	Spacing of Channelizing Devices (Feet) (G)
0 - 30	200	25
35 - 40	350	25
45	500	25
50	500	50
55	750	50
60 - 65	1000	50

- Flagger
- Channelizing Device

For low-volume traffic situations with short work zones on straight roadways where the flagger is visible to road users approaching from both directions, a single flagger may be used.

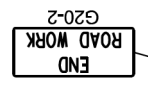
The ROAD WORK AHEAD and the END ROAD WORK signs may be omitted for short duration operations (1 hour or less).

For tack and/or flush seal operations, when flaggers are not being used, the FRESH OIL sign (W21-2) will be displayed in advance of the liquid asphalt areas.

Flashing warning lights and/or flags may be used to call attention to the advance warning signs.

The channelizing devices will be drums or 42" cones.

Channelizing devices are not required along the centerline adjacent to work area when pilot cars are utilized for escorting traffic through the work area.

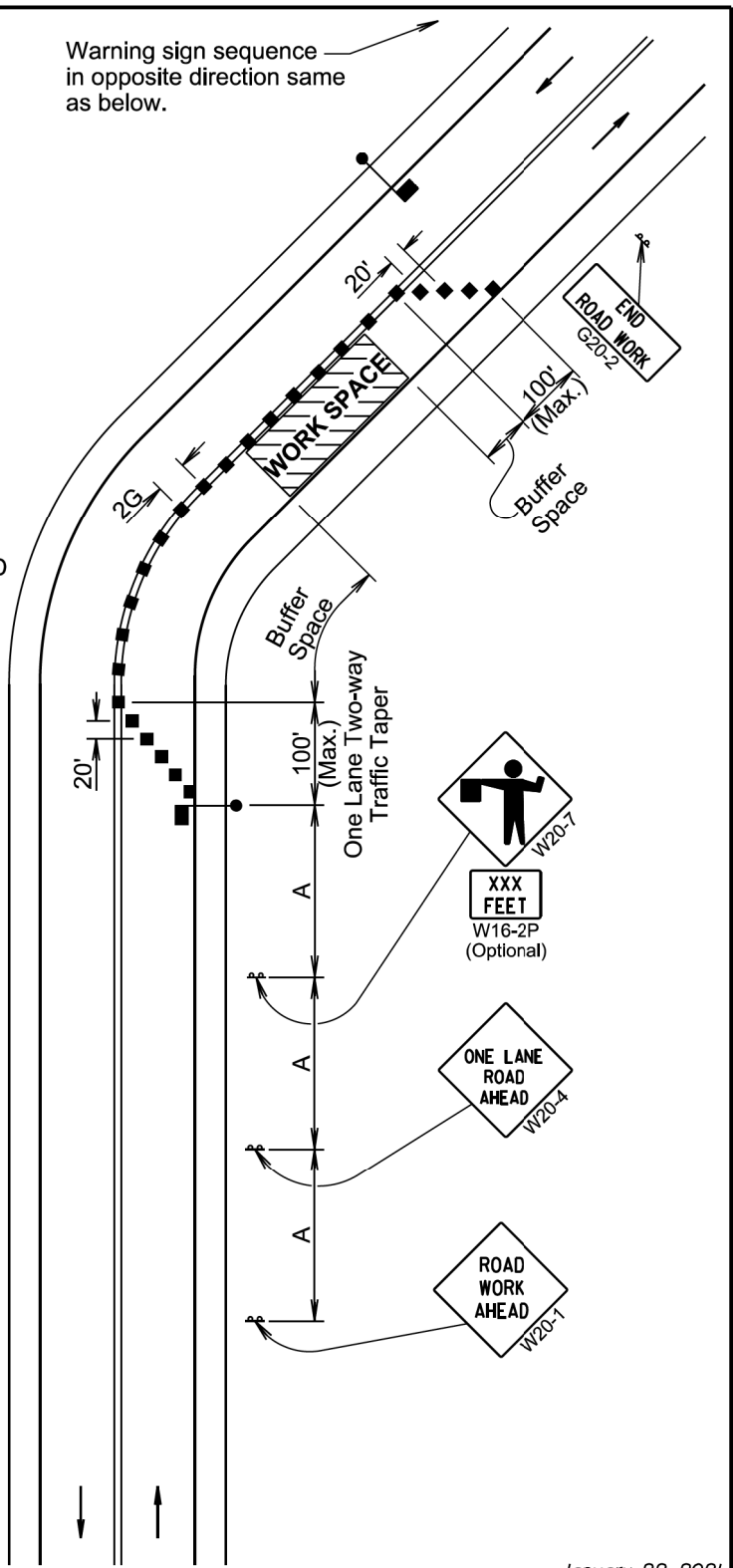


Channelizing devices and flaggers will be used at intersecting roads to control intersecting road traffic as required.

The buffer space should be extended so that the two-way traffic taper is placed before a horizontal or vertical curve to provide adequate sight distance for the flagger and queue of stopped vehicles.

The length of A may be adjusted to fit field conditions.

Warning sign sequence in opposite direction same as below.

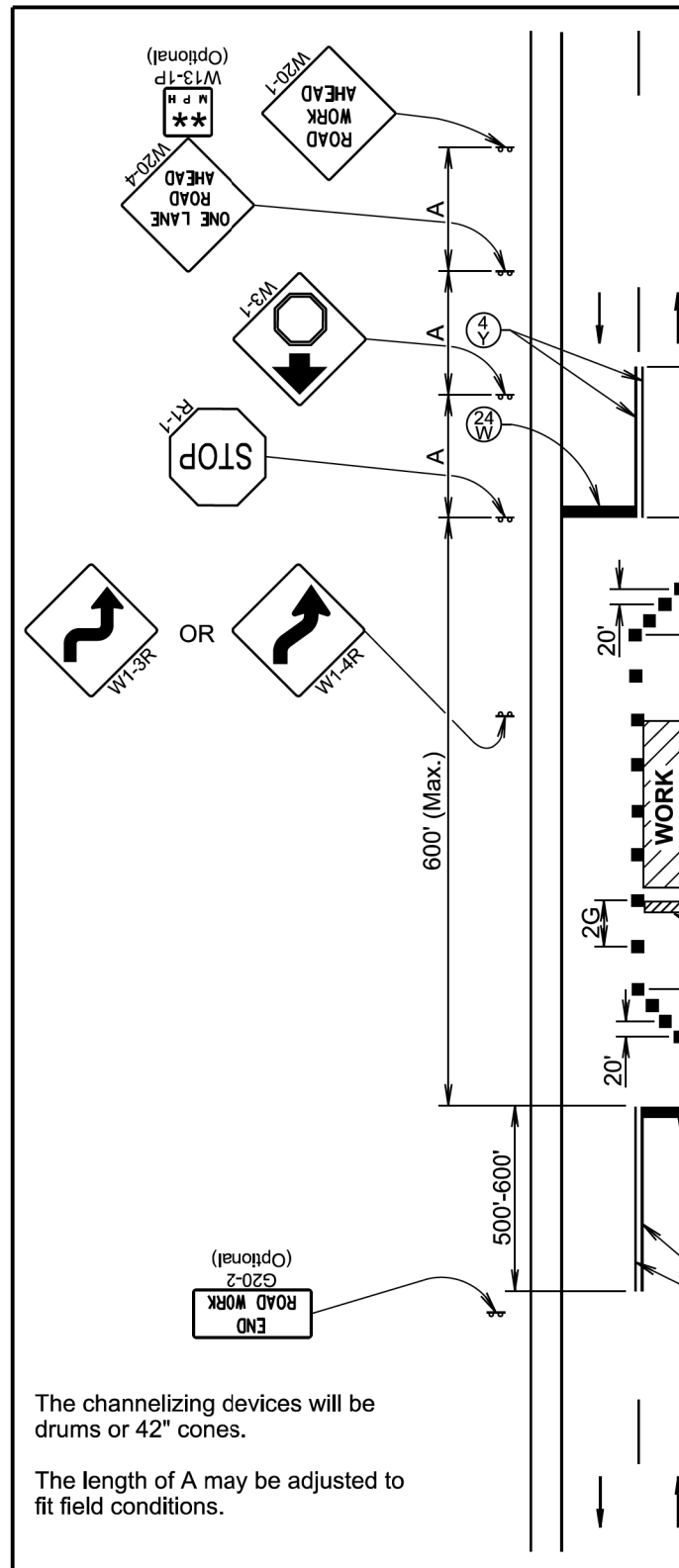


January 22, 2021

<b>SD DOT</b>	<b>LANE CLOSURE WITH FLAGGER PROVIDED</b>	PLATE NUMBER <b>634.23</b>
	Published Date: 2025	Sheet 1 of 1

Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)	Taper Length (Feet) (L)	Spacing of Channelizing Devices (Feet) (G)
0 - 30	200	180	25
35 - 40	350	320	25
45	500	600	25
50	500	600	50
55	750	660	50
60 - 65	1000	780	50

- Ⓜ 24" White Temporary Pavement Marking
- Ⓨ 4" Yellow Temporary Pavement Marking
- Channelizing Device
- \*\* Need and safe speed to be determined at the site by the Engineer.

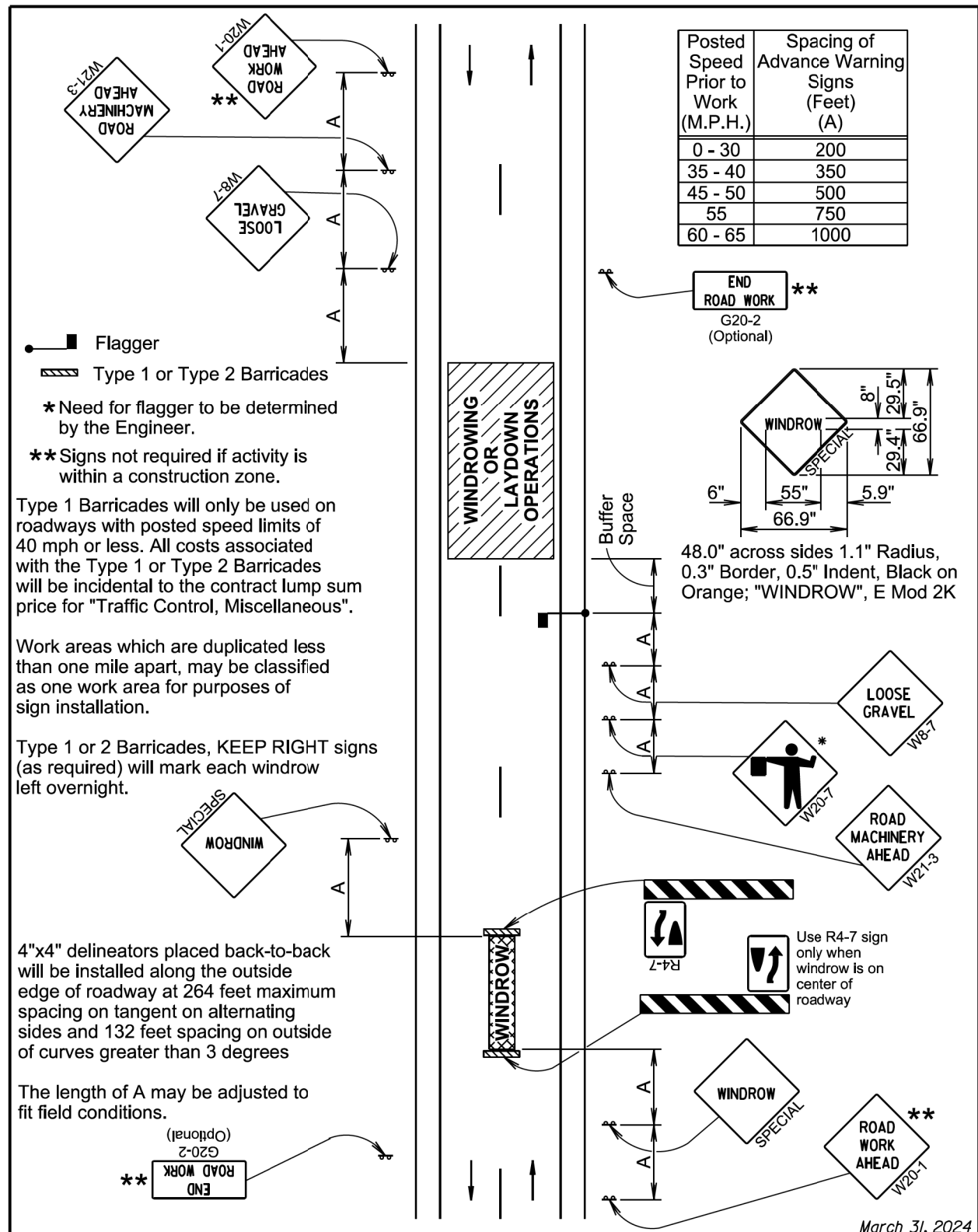


The channelizing devices will be drums or 42" cones.

The length of A may be adjusted to fit field conditions.

January 22, 2021

<b>SD DOT</b>	<b>LANE CLOSURE USING STOP SIGNS</b>	PLATE NUMBER <b>634.25</b>
	Published Date: 2025	Sheet 1 of 1



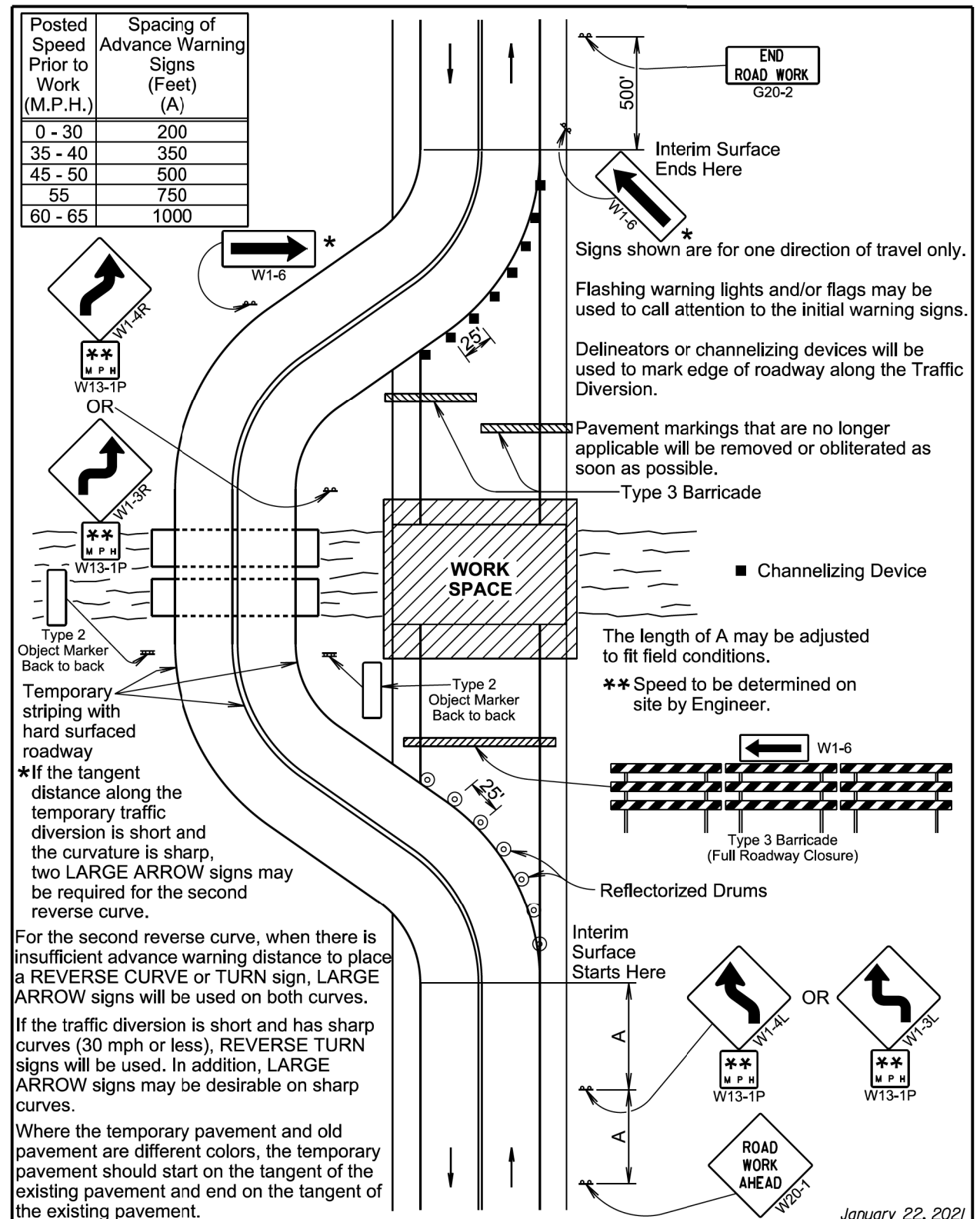
Published Date: 2025

**S**  
**D**  
**D**  
**O**  
**T**

**WINDROWING OR LAYDOWN OPERATION**

PLATE NUMBER  
634.27

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Published Date: 2025

**S**  
**D**  
**D**  
**O**  
**T**

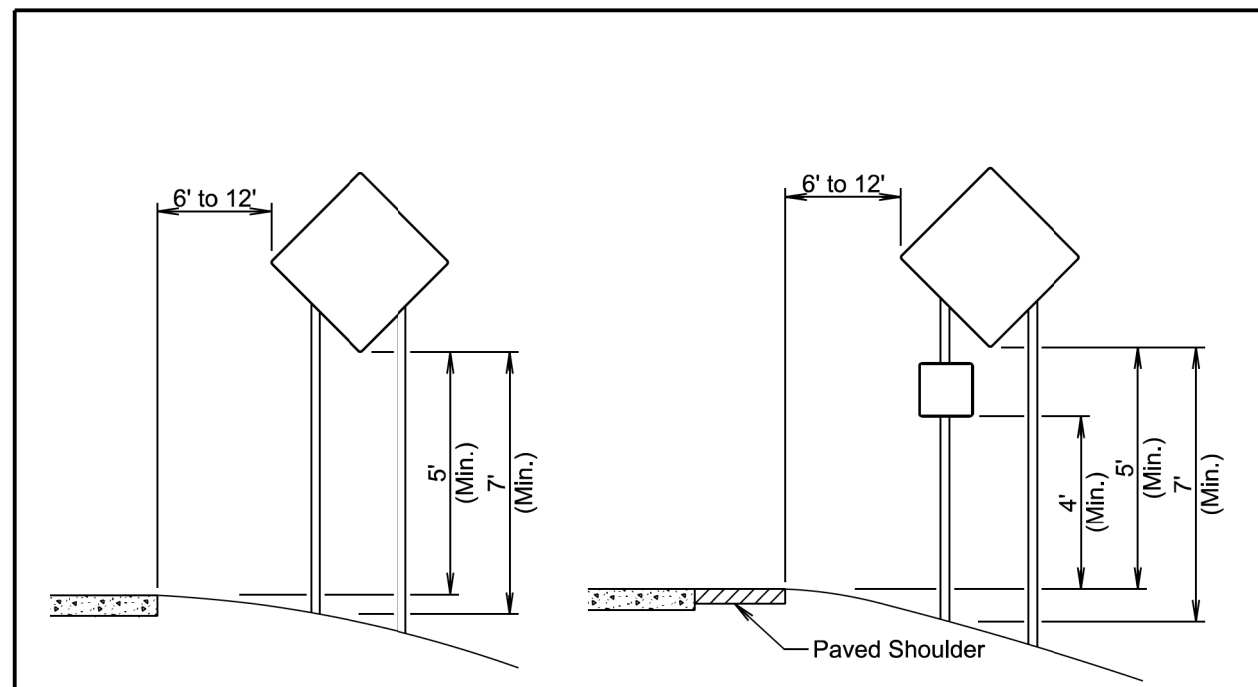
**ROAD CLOSED WITH TRAFFIC DIVERTED**

PLATE NUMBER  
634.28

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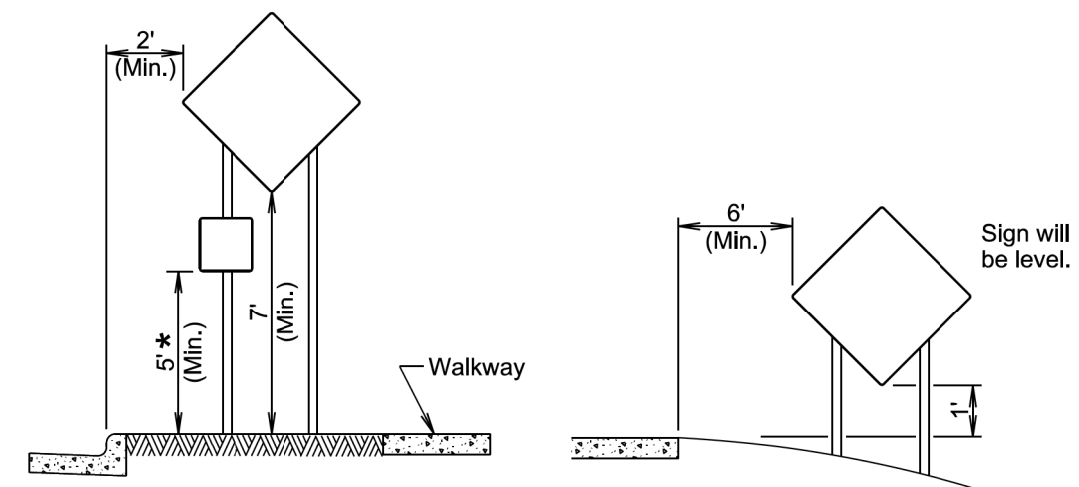
1:200 Plot Scale - TRW11NT26 - Plotted From - ...WorkingNote Templates.dgn

Plot Scale - 1:200



**RURAL DISTRICT**

**RURAL DISTRICT WITH SUPPLEMENTAL PLATE**



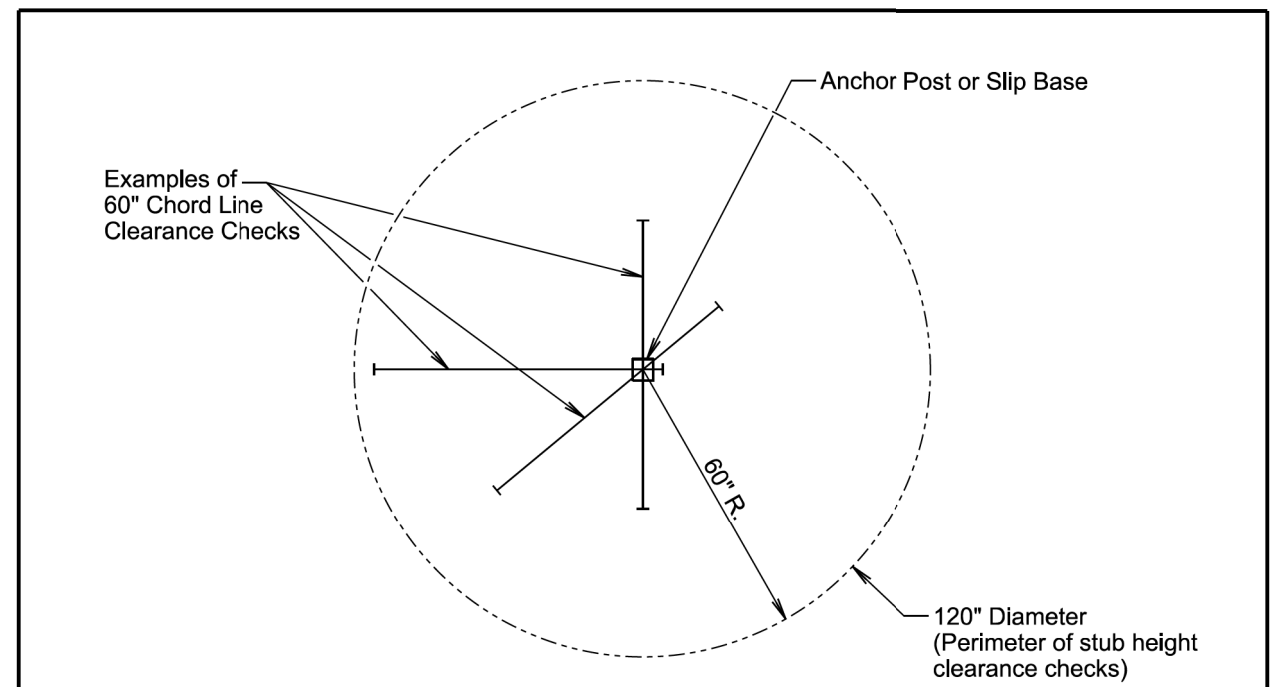
**URBAN DISTRICT**

**RURAL DISTRICT 3 DAY MAXIMUM**  
(Not applicable to regulatory signs)

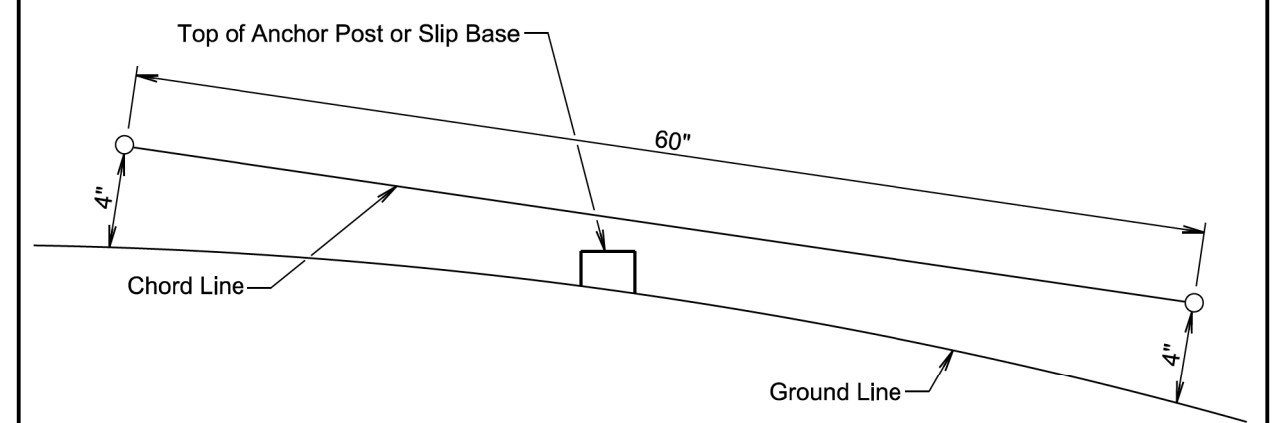
\* If the bottom of supplemental plate is mounted lower than 7 feet above a pedestrian walkway, the supplemental plate should not project more than 4" into the pedestrian facility.

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<b>Published Date: 2025</b>	<b>S D D O T</b>	<b>CRASHWORTHY SIGN SUPPORTS</b> <i>(Typical Construction Signing)</i>	PLATE NUMBER <b>634.85</b>
			Sheet 1 of 1



**PLAN VIEW**  
(Examples of stub height clearance checks)



**ELEVATION VIEW**

**GENERAL NOTES:**

- The top of anchor posts and slip bases WILL NOT extend above a 60" chord line within a 120" diameter circle around the post with ends 4" above the ground.
- At locations where there is curb and gutter adjacent to the breakaway sign support, the stub height will be a maximum of 4" above the ground line at the localized area adjacent to the breakaway support stub.
- The 4" stub height clearance is not necessary for U-channel lap splices where the support is designed to yield (bend) at the base.

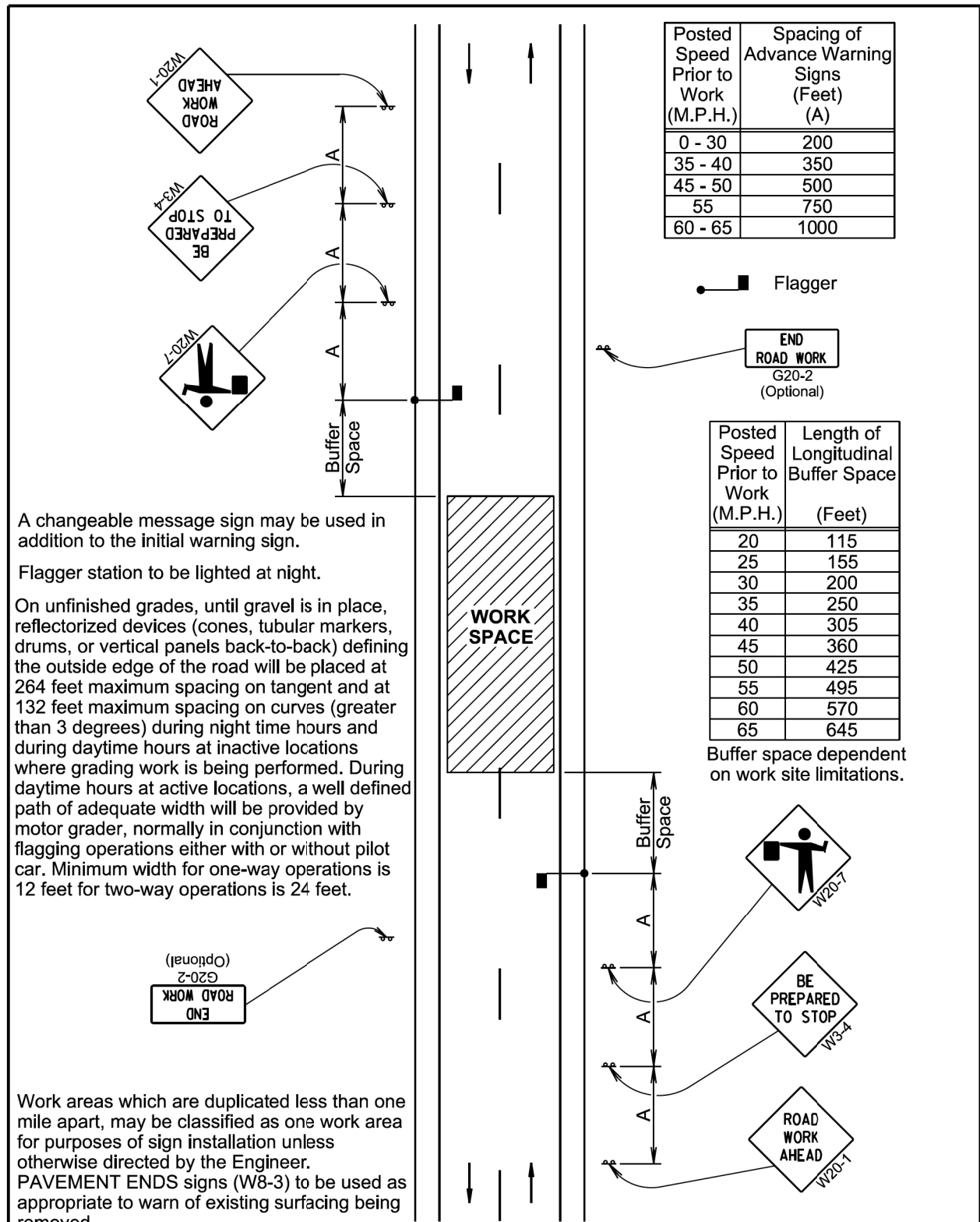
January 22, 2021

<b>Published Date: 2025</b>	<b>S D D O T</b>	<b>BREAKAWAY SUPPORT STUB CLEARANCE</b>	PLATE NUMBER <b>634.99</b>
			Sheet 1 of 1

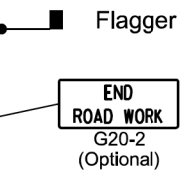
- Plotted From - TRW11NT26

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Plot Scale - 1:200



Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)
0 - 30	200
35 - 40	350
45 - 50	500
55	750
60 - 65	1000



Posted Speed Prior to Work (M.P.H.)	Length of Longitudinal Buffer Space (Feet)
20	115
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645

Buffer space dependent on work site limitations.

A changeable message sign may be used in addition to the initial warning sign.

Flagger station to be lighted at night.

On unfinished grades, until gravel is in place, reflectorized devices (cones, tubular markers, drums, or vertical panels back-to-back) defining the outside edge of the road will be placed at 264 feet maximum spacing on tangent and at 132 feet maximum spacing on curves (greater than 3 degrees) during night time hours and during daytime hours at inactive locations where grading work is being performed. During daytime hours at active locations, a well defined path of adequate width will be provided by motor grader, normally in conjunction with flagging operations either with or without pilot car. Minimum width for one-way operations is 12 feet for two-way operations is 24 feet.

Work areas which are duplicated less than one mile apart, may be classified as one work area for purposes of sign installation unless otherwise directed by the Engineer.

PAVEMENT ENDS signs (W8-3) to be used as appropriate to warn of existing surfacing being removed.

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<b>S D D O T</b>	<b>LONG TERM ROAD WORK</b>	PLATE NUMBER <b>634.31</b>
	Published Date: 2025	Sheet 1 of 1

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