

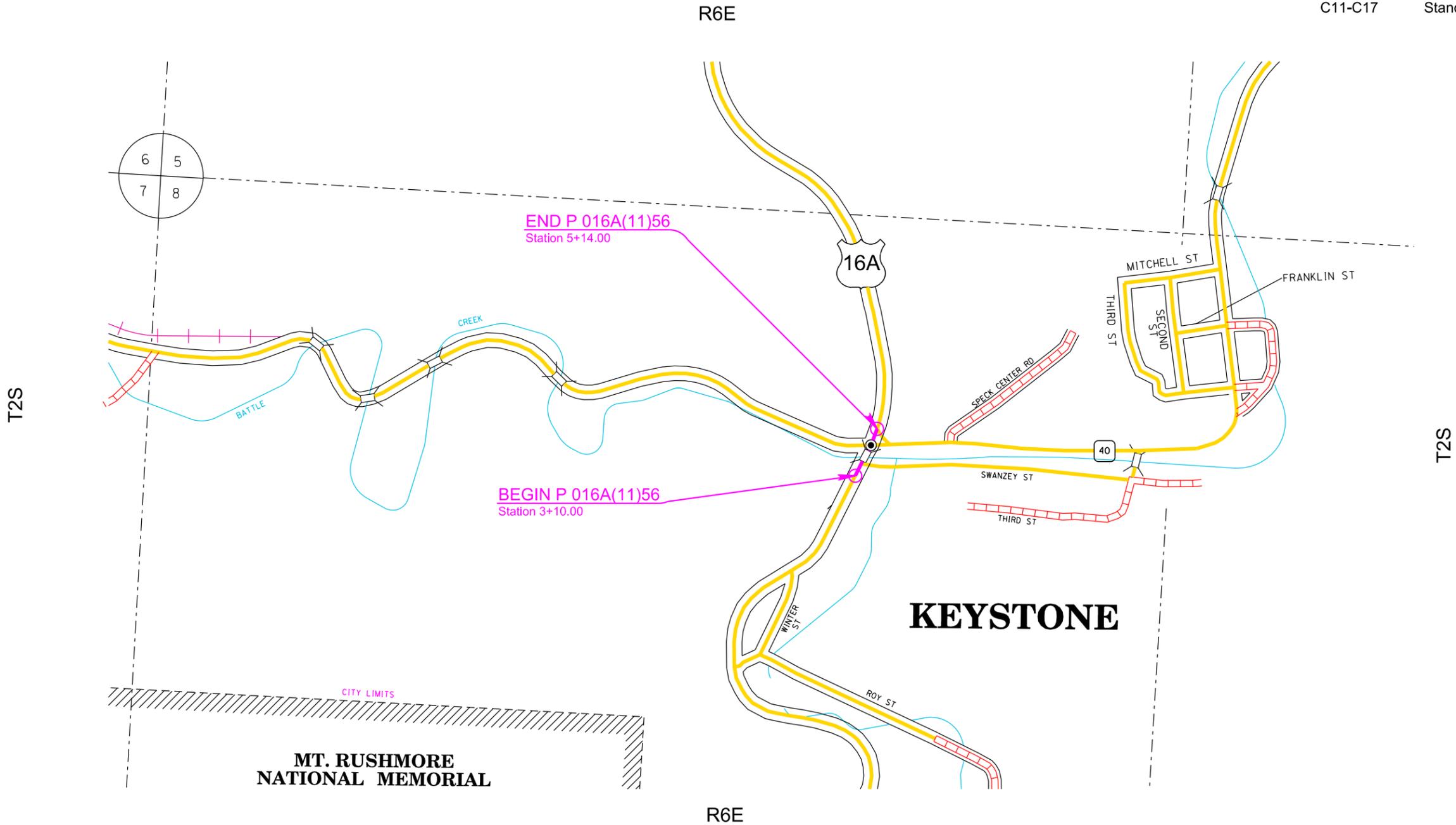
SECTION C: TRAFFIC CONTROL PLANS

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	P 016A(11)56	C1	C17

Plotting Date: 02/20/2026

INDEX OF SHEETS

- C1 General Layout with Index
- C2-C5 Estimate with General Notes & Tables
- C6 Temporary Traffic Control Plan
- C7 Restriction Signing
- C8-C19 Detour Signing
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- C11-C17 Standard Plates



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Plotted From: TRCU10208

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SECTION C ESTIMATE OF QUANTITIES

BID ITEM NUMBER	ITEM	QUANTITY	UNIT
110E7150	Remove Sign for Reset	1	Each
632E3500	Reset Sign	1	Each
634E0010	Flagging	500.0	Hour
634E0020	Pilot Car	100.0	Hour
634E0110	Traffic Control Signs	811.5	SqFt
634E0120	Traffic Control, Miscellaneous	Lump Sum	LS
634E0275	Type 3 Barricade	20	Each
634E0310	Temporary Flexible Vertical Markers (Tabs)	500	Ft
634E0420	Type C Advance Warning Arrow Board	1	Each
634E0525	Linear Delineation System Panel, Barrier Mounted	27	Each
634E0560	Remove Pavement Marking, 4" or Equivalent	1,360	Ft
634E0565	Remove Pavement Marking, Arrow	8	Each
634E0600	4" Temporary Pavement Marking Tape Type I	1,308	Ft
634E0700	Traffic Control Movable Concrete Barrier	26	Each
634E0750	Temporary Concrete Barrier End Protection	1	Each
634E0755	Remove and Reset Temporary Concrete Barrier End Protection	1	Each
634E0760	Temporary Concrete Barrier End Protection Module Set or Repair Kit	1	Each
634E0900	Portable Temporary Traffic Control Signal	3	Unit
634E1002	Detour and Restriction Signing	972.0	SqFt
634E2025	Longitudinal Pedestrian Barrier	60	Ft
634E2050	Temporary Sidewalk	200	SqFt

SEQUENCE OF OPERATIONS

1. Set up traffic control detour route signing.
2. Install initial erosion and sediment control as shown in Section D plans.
3. Remove traffic signals and Eastbound US Hwy 16A merge lane island entirely.
4. Install temporary surfacing at merge lane island removal area.
5. Install one lane road condition with movable traffic control barriers and temporary traffic signals.
6. Asphalt Removals in areas needed to maintain traffic will not begin until Spring 2026, just prior to asphalt paving as approved by the Project Engineer.
7. Complete structure work.
8. Structure work will be sequenced and constructed as detailed in Section E plans.
9. Install surfacing on Hwy 16A as shown in Section F plans.
10. Install final stabilization measures.
11. Remove all traffic control and restore traffic to US Hwy 16A.
12. New signal installation can occur as traffic allows and as approved by the Project Engineer.

Approval of an alternate sequence of operations will only be allowed when the proposed changes meet with the Department's intent for traffic control and sequencing of the work. An alternate sequence will be submitted for review a minimum of one week prior to potential implementation.

GENERAL TRAFFIC CONTROL

Existing guide, route, informational logo, regulatory, and warning signs will be temporarily reset and maintained during construction. Removing, relocating, covering, salvaging, and resetting of existing traffic control devices, including delineation, will be the responsibility of the Contractor. Cost for this work will be incidental to the contract unit prices for the various items unless otherwise specified in the plans. Any delineators and signs damaged or lost will be replaced by the Contractor at no cost to the State.

All temporary traffic control sign locations will be set in the field by the Contractor and verified by the Engineer prior to installation.

Portable sign supports will not be located on sidewalks, bicycle facilities, or other areas designated for pedestrian or bicycle traffic.

If there is a discrepancy between the traffic control plans, standard plates, and the MUTCD, whichever is more stringent will be used, as determined by the Engineer.

Unless otherwise stated in these plans, work will not be allowed during hours of darkness.

Fixed location signing placed more than 4 calendar days prior to the start of construction will be covered or laid down until the time of construction. The covers must be approved by the Engineer prior to installation. The cost of materials, labor, and equipment necessary to complete this work will be incidental to other contract items. No separate payment will be made.

All fixed location signs, sign posts, and breakaway bases will be removed within 7 calendar days following pavement marking.

All haul trucks will be equipped with an additional flashing amber light that is visible from the backside of the haul truck. The costs for the flashing amber lights will be incidental to the various related contract items.

The Contractor will furnish, install, maintain, and remove TRUCK CROSSING (W8-6) signs daily. The TRUCK CROSSING signs will be displayed always when haul vehicles are hauling material. When hauling conditions no longer exist, the signs will be covered or removed from view. The exact number and location will be determined during construction. Payment for additional signs will be based on the contract unit price per square foot for "Traffic Control Signs".

GROOVED PAVEMENT (W8-15) signs with MOTORCYCLE (W8-15P) plaques are required in advance of areas that have been cold milled and are not resurfaced the same day. The GROOVED PAVEMENT sign assemblies will be installed a minimum of 1000 feet in advance of cold milled sections and remain in place until the sections have been resurfaced.

The Contractor will notify businesses/homeowners a minimum of two weeks prior to construction to inform them of upcoming construction and again a minimum of 48 hours prior to any blocked access to make appropriate arrangements.

A mobile work operation will be allowed provided the rumble strip or rumble stripe grooving, flush sealing, and pavement marking can be completed satisfactorily by a continuously moving work operation. A mobile work operation will require approval by the Engineer.

If inappropriate or conflicting pavement markings exist, the markings will be removed and replaced with applicable temporary pavement markings when the work duration is more than 3 days. When the work duration is less than 3 days, the channelizing devices in the area where the pavement markings conflict will be placed at one-half of the normal channelizing device spacing. Pavement marking removals will be incidental to the contract unit price per foot for "Remove Pavement Marking, 4" or equivalent". Temporary pavement marking will be paid for at the contract unit price per mile/foot for "Temporary Pavement Marking". The additional channelizing devices will be incidental to the contract lump sum price for "Traffic Control, Miscellaneous".

A Type 3 Barricade will be installed at the end of a lane closure taper as detailed in these plans. Additional Type 3 Barricades will be installed facing traffic within the closed lane at a spacing of 1/4 mile.

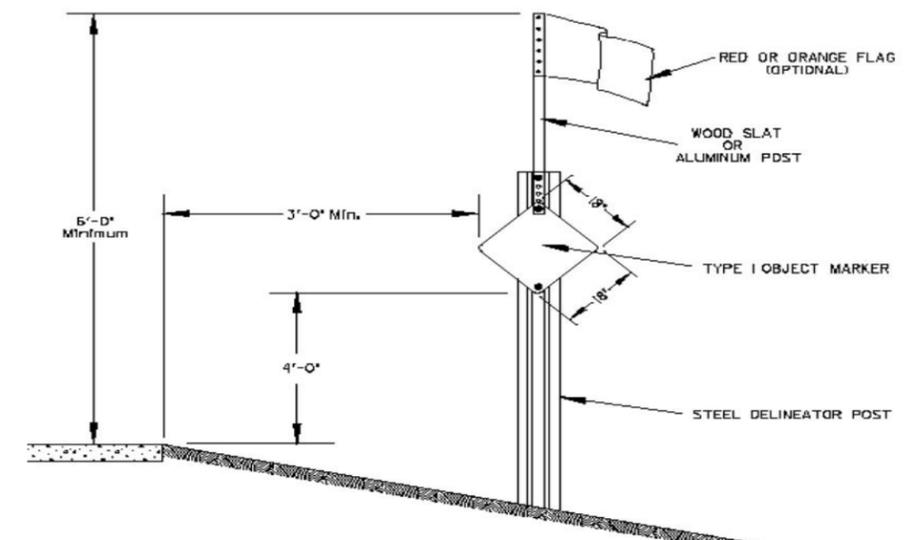
Lane closures will be limited to 5 miles in length. The distance between the closest points of any two-lane closures will be at least 3 miles, excluding tapers.

BUMP MARKERS

Orange bump markers (Orange Type 1 Object Marker back-to-back) will be placed adjacent to the bump location. The bump marker details are shown in the following drawing. The steel delineator post will be a 1.12 lb/ft flanged channel steel post for ground mounted installation. If the duration is less than 3 days, the Type 1 Object Marker can be installed on temporary supports.

BUMP (W8-1) signs with appropriate ADVISORY SPEED (W13-1P) plaques will be placed 500 feet in advance of the bump or as approved by the Engineer for adequate sight distance.

All costs for bump markers, bump signs, and advisory speed plaques will be incidental to the contract unit price per square foot for "Traffic Control Signs".



OVERWIDTH/ OVERLENGTH RESTRICTION AND DETOUR SIGNING

The Contractor will furnish and install the overwidth/ overlength restriction and detour signs as shown in these plans. Prior to installing the signs, the Contractor will mark the sign locations and review them with the Engineer. overwidth/ overlength restriction and detour signs will be installed on fixed location, ground mounted, breakaway supports. It will be the responsibility of the Contractor to maintain and reinstall these signs during the project as required by the construction progress. Upon completion of the project, the Contractor will remove the overwidth/ overlength restriction and detour signs.

All costs for furnishing the signs, posts, and mounting hardware, and for installing, maintaining, covering, and removing the overwidth/ overlength restriction and detour signs will be incidental to the contract unit price per square foot for "Detour and Restriction Signing".

PORTABLE TEMPORARY TRAFFIC CONTROL SIGNAL

The Contractor will furnish, install, operate, and maintain a portable temporary traffic control signal during construction phases as determined by the Engineer. There will be one controller and one slave unit per location and a 3 signal scenario.

Locations are as follows:

- US HWY 16A / SD HWY 40 Intersection.
 - 3 signals for the one lane, three-legged intersection.

The portable temporary traffic control signal will be set up to dwell in red. Detection will be video, microwave, or radar. The green time may be adjusted as needed. The Engineer will contact the Region Traffic Engineer one week prior to activation to obtain the appropriate signal timings.

All vehicle signal heads will have backplates with retroreflective border. The vehicle signal head backplates will have a factory applied 3-inch wide yellow retroreflective border. Sheeting for the border will be Type IX or Type XI in conformance with ASTM D4956.

Signal backplates will be polycarbonate, aluminum, or aluminum-composite. Minimum material thicknesses are:

- Polycarbonate, 0.10-inch
- Aluminum, 0.06-inch
- Aluminum-Composite, 0.08-inch

Signal backplates will extend not less than 5 inches from the edge of the signal head at the top, bottom, and sides.

All traffic signal equipment and materials will meet the requirements of Sections 635 and 985 of the Specifications except the controller requirements.

All costs involved with constructing the portable temporary traffic control signal as specified above and on the plans, will be included in the contract unit price per unit for "Portable Temporary Traffic Control Signal".

ITEMIZED LIST FOR TRAFFIC CONTROL SIGNS

SIGN CODE	SIGN DESCRIPTION	CONVENTIONAL ROAD			
		NUMBER	SIGN SIZE	SQFT PER SIGN	SQFT
R4-7	KEEP RIGHT (symbol)	1	24" x 30"	5.0	5.0
R9-9	SIDEWALK CLOSED	2	24" x 12"	2.0	4.0
R9-11	SIDEWALK CLOSED AHEAD (ARROW L or R) CROSS HERE	1	24" x 18"	3.0	3.0
R10-6	STOP HERE ON RED	3	24" x 36"	6.0	18.0
R11-2	ROAD CLOSED	2	48" x 30"	10.0	20.0
W1-4	REVERSE CURVE (L or R)	2	48" x 48"	16.0	32.0
W3-3	SIGNAL AHEAD (symbol)	3	48" x 48"	16.0	48.0
W3-4	BE PREPARED TO STOP	5	48" x 48"	16.0	80.0
W4-2	LEFT or RIGHT LANE ENDS (symbol)	1	48" x 48"	16.0	16.0
W8-1	BUMP	6	48" x 48"	16.0	96.0
W8-6	TRUCK CROSSING	4	48" x 48"	16.0	64.0
W8-7	LOOSE GRAVEL	3	48" x 48"	16.0	48.0
W8-15P	MOTORCYCLE (plaque)	3	24" x 18"	3.0	9.0
W13-1P	ADVISORY SPEED (plaque)	3	30" x 30"	6.3	18.9
W20-1	ROAD WORK AHEAD	5	48" x 48"	16.0	80.0
W20-4	ONE LANE ROAD AHEAD	6	48" x 48"	16.0	96.0
W20-5	LEFT or RIGHT LANE CLOSED AHEAD	1	48" x 48"	16.0	16.0
W20-7	FLAGGER (symbol)	5	48" x 48"	16.0	80.0
W21-5	SHOULDER WORK	2	48" x 48"	16.0	32.0
G20-2	END ROAD WORK	4	36" x 18"	4.5	18.0
-	TYPE 1 ORANGE OBJECT MARKER BACK TO BACK	6	18" x 18"	4.6	27.6
CONVENTIONAL ROAD TRAFFIC CONTROL SIGNS SQFT					811.5

ITEMIZED LIST FOR DETOUR AND RESTRICTION SIGNING

SIGN CODE	SIGN DESCRIPTION	CONVENTIONAL ROAD			
		NUMBER	SIGN SIZE	SQFT PER SIGN	SQFT
R1-1	STOP	4	30"	5.2	20.8
R1-3P	ALL WAY (plaque)	4	18" x 6"	0.8	3.2
R6-1L	ONE WAY LEFT ARROW	2	36" x 12"	3.0	6.0
SPECIAL	NO VEHICLES OVER 12 FT WIDE OVER 50 FT LONG	6	120" x 48"	40.0	240.0
SPECIAL	RESTRICTION WIDTH 12' MAX LENGTH 50' MAX AT KEYSTONE	4	126" x 96"	84.0	336.0
W3-1	STOP AHEAD (symbol)	2	48" x 48"	16.0	32.0
W20-2	DETOUR AHEAD	4	48" x 48"	16.0	64.0
SPECIAL	BROWN DESTINATION (MT RUSHMORE W/ ARROW LT)	2	30" x 24"	5.0	10.0
SPECIAL	BROWN DESTINATION (MT RUSHMORE W/ ARROW RT)	1	30" x 24"	5.0	5.0
SPECIAL	GREEN DESTINATION (OLD HILL CITY RD W/ ARROW LT)	1	24" x 30"	5.0	5.0
M1-4	US ROUTE MARKER (16A)	22	30" x 24"	5.0	110.0
M1-5	SD ROUTE MARKER (40)	1	24" x 24"	4.0	4.0
M3-2	DIRECTION MARKER - EAST	8	24" x 12"	2.0	16.0
M3-2	DIRECTION MARKER - EAST (SD)	1	24" x 12"	2.0	2.0
M3-4	DIRECTION MARKER - WEST	13	24" x 12"	2.0	26.0
M4-5	TO (SD)	1	24" x 12"	2.0	2.0
M4-8	DETOUR	20	24" x 12"	2.0	40.0
M4-8a	END DETOUR	2	24" x 18"	3.0	6.0
M5-1	ADVANCE TURN ARROW 90° (L or R)	7	21" x 15"	2.2	15.4
M6-1	DIRECTION ARROW - Horizontal Single Head (L or R)	10	21" x 15"	2.2	22.0
M6-3	DIRECTION ARROW - Vertical Single Head	3	21" x 15"	2.2	6.6
CONVENTIONAL ROAD DETOUR AND RESTRICTION SIGNING SQFT					972.0

FLAGGING

Operations will be conducted so that the traveling public will not have to wait longer than 15 minutes at the flagger station.

It is required that the flaggers be able to communicate with one another. If an emergency vehicle needs to pass through the project, the Contractor will be required to expedite traffic movement. All costs associated with this will be incidental to the contract unit price per hour for "Flagging".

TEMPORARY SURFACING

It is anticipated that existing surfacing will need to be removed in areas that are being used to accommodate traffic.

An estimated quantity of 30 Tons of Asphalt Concrete Composite and 60 tons Base Course have been added to the Estimate of Quantities. This quantity may be adjusted according to field conditions and Contractor operations.

Temporary Surfacing (Asphalt Concrete Composite) will be used as shown in the Temporary Traffic Control Plans and as directed by the Engineer. Temporary Surfacing (Asphalt Concrete Composite) will consist of 6" of Base Course and 3" of Asphalt Concrete Composite.

The existing area inlet on Old Hill City Road, referenced on the Temporary Traffic Control Plan Sheet will need to be modified to accommodate temporary surfacing. This may include the removal of frame & grate and providing a cover capable of carrying traffic loads, subject to approval of the Engineer. The area inlet will be returned to its original condition when temporary surfacing is removed. All costs for this area inlet modification will be incidental to various bid items associated with the removal of existing surfacing and installation of temporary surfacing described in this section.

Temporary Surfacing (Base Course) may be used in pavement removal areas that are being used to accommodate traffic, maintain access and at the direction of the Engineer.

Temporary Base Course and Asphalt Concrete Composite have been included in Section F Estimate of Quantities (refer to Section F – Table of Surfacing for Traffic Control). All costs associated with excavation, maintenance and removal of the Temporary Surfacing will be incidental to various bid items associated with the removal of existing surfacing and installation of temporary surfacing described in this section.

TRAFFIC CONTROL FOR ASPHALT CONCRETE RESURFACING

The Contractor will need to install LOOSE GRAVEL (W8-7) signs with motorcycle plaques (W8-15P) in areas where loose sand is present during the flush seal operation. LOOSE GRAVEL signs have been included in these plans for this.

TEMPORARY PAVEMENT MARKING

Temporary flexible vertical markers (tabs) must be used on the final lift of asphalt surfacing.

The Contractor will remove and properly dispose of the tabs after permanent pavement marking is applied. Method of removal will be nondestructive to the road surface and will be accomplished within one week of completion of the permanent pavement marking.

Full reflectivity of all temporary flexible vertical markers (tabs) is required at all times. The Contractor will be required to replace any missing or non-reflective tabs after each installation as detailed below at no additional cost to the State.

Quantities of Temporary Pavement Markings (Tabs) consist of:

One pass on top of the final lift of asphalt concrete.

In the absence of a signed lane closure or pilot car operation, FLAGGER (W20-7) symbol signs and flaggers, or a shadow vehicle with rotating yellow lights or strobe lights will be positioned on the shoulder in advance of workers for both directions of traffic during the installation and removal of the temporary flexible vertical markers (tabs). The traffic control device used will be moved intermittently to provide proper warning of the work operation. A ROAD WORK AHEAD (W20-1) sign, a WORKER (W21-1) symbol sign or a BE PREPARED TO STOP (W3-4) sign will be mounted on the rear of the shadow vehicle. The method of traffic control used by the Contractor for this work must be approved by the Engineer.

TEMPORARY PAVEMENT MARKING TAPE, TYPE I

Temporary Pavement Marking Tape, Type 1 will be used for Stop bars, Edgelines as required in the standard plates and as shown on the Temporary Traffic Control Plan sheet.

Temporary pavement marking for stop lines will consist of 4" Temporary Pavement Marking Tape Type I. Placement of each 24" white stop line will be accomplished by placing six pieces of 4" x 12' tape adjacent to one another. The workspace requires three stop lines which is an equivalent of approximately 216' of 4" tape plus one additional workspace for re-install due to work location changing to a different lane (3+1=4 workspaces at 144' each = 288').

An additional 920' of 4" White has been included in the quantity for the lane closure tapers and edgelines as shown on the Temporary Traffic Control Plan sheet. Temporary tape will be removed upon completion of the project.

PAVEMENT MARKING REMOVAL (4" or Equivalent)			
Route	Location	Description	Estimated Quantity (Ft.)
US 16A	Lane Lines	As Needed	90
US 16A	WB Left Turn/ Thru Lane	Existing Stop Bar	90
US 16A	WB Right Turn/ Thru Lane	Existing Stop Bar	90
SD 40	WB Right Turn/ Thru Lane	Existing Stop Bar	90
SD 40	Lane Closure/One Lane Rd. Transition	Double Yellow Ceterline	500
Old Hill City Rd.	Lane Closure/One Lane Rd. Transition	Double Yellow Ceterline	500
TOTAL=			1360

PAVEMENT MARKING REMOVAL (Arrows)			
Route	Location	Description	Estimated Quantity (Each)
US 16A	WB Left Turn/ Thru Lane	Arrows	4
US 16A	WB Right Turn/ Thru Lane	Arrows	4
TOTAL=			8

* All arrows to be removed are combination (thru/turn) arrows. Combination arrows will be considered one arrow each in the table above.

TEMPORARY PAVEMENT MARKING TAPE, TYPE 1	
DESCRIPTION	ESTIMATED QUANTITY (Ft)
	WHITE
STOP BARS (3+1)	288
US 16A/ SD 40/ OHCR INTERSECTION (ONE WAY EDGELINES)	600
TRAFFIC CONTROL TAPERS	420
TOTAL=	1,308

REMOVE/INSTALL SIGN			
DESCRIPTION	LOCATION	ESTIMATED QUANTITY (EACH)	
		REMOVE	INSTALL
STOP SIGN (REMOVE SIGN FROM POST)	NB 3RD STREET (BRIDGE)	1	
STOP SIGN (REINSTALL SIGN ON POST)	NB 3RD STREET (BRIDGE)		1
TOTALS=		1	1

STATE FURNISHED PORTABLE CHANGEABLE MESSAGE SIGN

One week prior to starting work affecting the traveling public, portable changeable message signs (PCMS) will be installed at locations determined by the Engineer to notify drivers of the upcoming construction. The Engineer will program the portable changeable message signs with the following message:

ROAD WORK
STARTS
(Date)

The PCMS will be removed by the State at the Engineer's discretion.

INCIDENTS

An incident is an emergency road user occurrence, a natural disaster, or other unplanned event that affects or impedes the normal flow of traffic such as a crash, hazardous materials spill, or other event.

The Contractor will set up a meeting prior to start of work to plan and coordinate responses to an incident. The Contractor will invite the Department of Transportation, the South Dakota Highway Patrol, the Pennington County Sheriff and local emergency response entities to the meeting.

The Contractor will assist to maintain traffic as required by these plan notes and as agreed to at that meeting.

Emergency vehicle access through the project will be considered and discussed at the meeting.

The Contractor may be required to modify messages on portable changeable message signs or relocate portable changeable message signs, and to provide flaggers to direct or detour traffic. The Contractor should be prepared to relocate advance warning signs if determined to be necessary for a major traffic incident lasting more than two hours. Fixed location ground mounted signs may be covered and additional portable signs provided.

No additional payment will be made for the modification of portable changeable message sign messages or the relocation of portable changeable message signs. Cost for the relocation of an advance warning sign due to an incident will be 50% of the designated sign rate. Flaggers will be paid for at the contract unit price per hour for "Flagging".

PRESS RELEASE ANNOUNCEMENTS

The SDDOT will prepare a press release to be released 5 days prior to any phase change or any other major change that affects traffic flow. The SDDOT will be responsible to keep law enforcement, emergency services, and the traveling public notified of changes in project access. The Contractor will provide the Engineer with pertinent information 7 days prior to any phase change or any other major change that affects traffic flow.

TEMPORARY PEDESTRIAN SIDEWALK

Temporary pedestrian sidewalk will consist of 2" of asphalt concrete composite and 4" of base course. The traveled surface will be a smooth, continuous, non-slip, hard surface. There should be no curbs or abrupt changes in grade or terrain that could cause tripping or be a barrier to wheelchair use.

There will be no testing required for the asphalt concrete composite or base course used for temporary pedestrian sidewalk.

Temporary pedestrian sidewalk will have a minimum width of 48 inches, with 60 inches recommended. The Contractor will try to provide boulevard sidewalk, whenever possible, for temporary pedestrian sidewalk that is 48 inches wide. Temporary pedestrian sidewalk less than 60 inches wide will provide for a 60-inch x 60-inch passing space at intervals not to exceed 200 feet. Temporary pedestrian sidewalk will have a maximum cross slope of 2%. The maximum grade will be 5% where the temporary pedestrian sidewalk does not follow the grade of the road.

All costs associated with installing, maintaining, removing of temporary pedestrian sidewalk, including all materials, base course, asphalt concrete composite, labor, disposal and incidental work, will be included in the contract unit price per square foot for "Temporary Sidewalk".

LONGITUDINAL PEDESTRIAN BARRICADE

Longitudinal pedestrian barricades should not be used to provide positive protection for pedestrians.

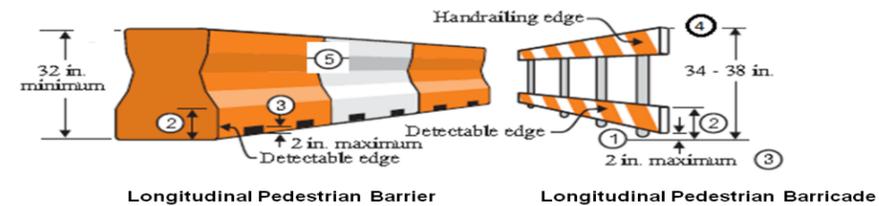
To prevent any tripping hazard to pedestrians, ballast will be located behind or internal to the device.

When longitudinal pedestrian barricades are combined in a series, the maximum gap between devices that do not interlock will be one inch. Joints between devices that do interlock will be closed and flush to prevent canes or small wheels from being trapped and to facilitate safe hand trailing. When used as a sidewalk closure mechanism, longitudinal pedestrian barricade must run the entire width of the sidewalk. Longitudinal pedestrian barricade should provide a color contrasting pattern. Black should not be used to color any base on a device. The devices should comply with the general color and stripe pattern requirements of Section 6F.68 of the MUTCD.

Longitudinal pedestrian barricade will have continuous bottom and top surfaces. The top surface will be smooth to allow safe hand trailing. Both upper and lower surfaces will share a common vertical plane.

All costs will be incidental to the contract unit price per foot for "Longitudinal Pedestrian Barricade".

PEDESTRIAN CHANNELIZING DEVICE DETAILS



1. Barricade rail supports may not extend into the pedestrian walkway more than 4 inches from the face of the barricade.
2. The top edge of the bottom portion will be a minimum of 8 inches above the walkway.
3. Devices will not block water drainage from the walkway. A gap height or opening from the walkway surface up to a maximum of 2 inches in height is allowed for drainage purposes.
4. The top edge of the longitudinal pedestrian barricade is to be used as a guiderail to provide visual and tactile guidance to pedestrians along a designated route. The top surface should have a minimum width of 0.5 inches to allow the hand to feel the surface. The surface should be smooth and free of any sharp or abrasive elements to allow safe hand trailing.
5. Longitudinal pedestrian barrier used to provide positive protection from traffic to pedestrians should be crashworthy.

TRAFFIC CONTROL MOVABLE CONCRETE BARRIERS

Concrete barriers will be provided by the State and are available for pickup from the SDDOT Maintenance Yard located adjacent to Hwy 79 approximately two miles south of Rapid City and returned to the same location when they are no longer needed on the project.

Barriers to be adjusted or moved will be disconnected from adjacent barriers to minimize damage to connecting pins. Pins damaged by the Contractor will be replaced at no cost to the Department.

Concrete barrier sections will be placed as depicted in the plans to comply with clear zone requirements and as required by the Engineer. The barriers will be pinned and bolted together as directed by the Engineer.

All costs associated with picking the barriers up from the SDDOT Maintenance Yard, transporting, setting, connecting, and hauling them back to the SDDOT Maintenance Yard will be incidental to the contract unit price per each for Traffic Control Movable Concrete Barrier. After the initial placement, the concrete barriers may need to be adjusted. Adjustment of the barriers, where they do not need to be loaded on a truck for transport, will be incidental to the contract unit price per each for Traffic Control Movable Concrete Barrier. All costs associated with removing, loading, unloading, and resetting of the barriers at a new site, will be incidental to the contract unit price per each for Remove and Reset Traffic Control Movable Concrete Barrier. No additional payment will be made for barriers that are not immediately reset at a new location on the project

and stored on-site until they are either reset on the project or returned to the SDDOT as indicated in these plans.

TEMPORARY CONCRETE BARRIER END PROTECTION

Crash attenuators meeting the requirements of NCHRP 350 or MASH TL-3 will be furnished and installed by the Contractor. Attachment of the attenuators to the concrete barriers will be by approved methods.

All costs associated with furnishing, transporting, initial setup, connecting, maintaining, and removing the crash attenuators will be incidental to the contract unit price per each for Temporary Concrete Barrier End Protection.

All costs associated with moving and resetting crash attenuators to accommodate traffic flows after initial set-up will be paid for at the contract unit price per each for Remove & Reset Temporary Concrete Barrier End Protection. All costs associated with removing from initial placement and resetting at a new location will be incidental to the contract unit price per each. No additional payment will be made for crash attenuators that are not immediately reset at a new location on the project and stored on-site until they are either reset or removed from the project as determined by the Engineer. No additional payment will be made for minor adjustments.

The Contractor will have replacement hardware available so that in the event the crash attenuator is hit and made unusable, the crash attenuator can be made functional within 24 hours. The cost of replacement will be incidental to the contract unit price per each for Temporary Concrete Barrier Module Set or Repair Kit. No payment will be made for the Temporary Concrete Barrier Module Set or Repair Kit if no repairs are necessary. Upon completion of the project, crash attenuators will remain the property of the Contractor.

BARRIER MOUNTED LINEAR DELINEATION SYSTEM PANELS

A linear delineation system (LDS) panel will be attached to each barrier section. The color will be red. The LDS will be 34 inches long and 6 inches in height and be constructed of aluminum formed into a shape to provide retroreflective properties across a wide range of angles. It will be sheeted with sheeting meeting the requirements of ASTM D4956 Type XI. The panels will be evenly spaced, with the top of the panel 4 inches below the top of the barrier. Installation will be as per the manufacturer's recommendations. This will allow for easy removal for replacement of damaged panels or to replace with an alternate color. The Contractor will furnish and install one panel on the traffic side of the barrier. Replacement of damaged linear delineation system panels will be furnished and replaced by the Contractor. All costs associated with furnishing, installing, and replacing, if needed, will be incidental to the contract unit price per each for Linear Delineation System Panel, Barrier Mounted.

All LDS panels will remain attached to the barrier sections and will become the property of the State of South Dakota upon completion of the project.

The Contractor will verify the number of LDS panels that will need to be installed or replaced on the Traffic Control Movable Concrete Barriers. The contract amount of LDS panels is an estimate and the full contract amount may not be needed.

Maintaining the linear delineation system will be incidental to the contract lump sum price for Traffic Control, Miscellaneous.

TEMPORARY TRAFFIC CONTROL PLAN

STATE OF SOUTH DAKOTA	PROJECT P 016A(11)56	SHEET C6	TOTAL SHEETS C17
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Plotting Date: 02/07/2026

Plot Scale: 1:200

Plotted From: TRCU10208

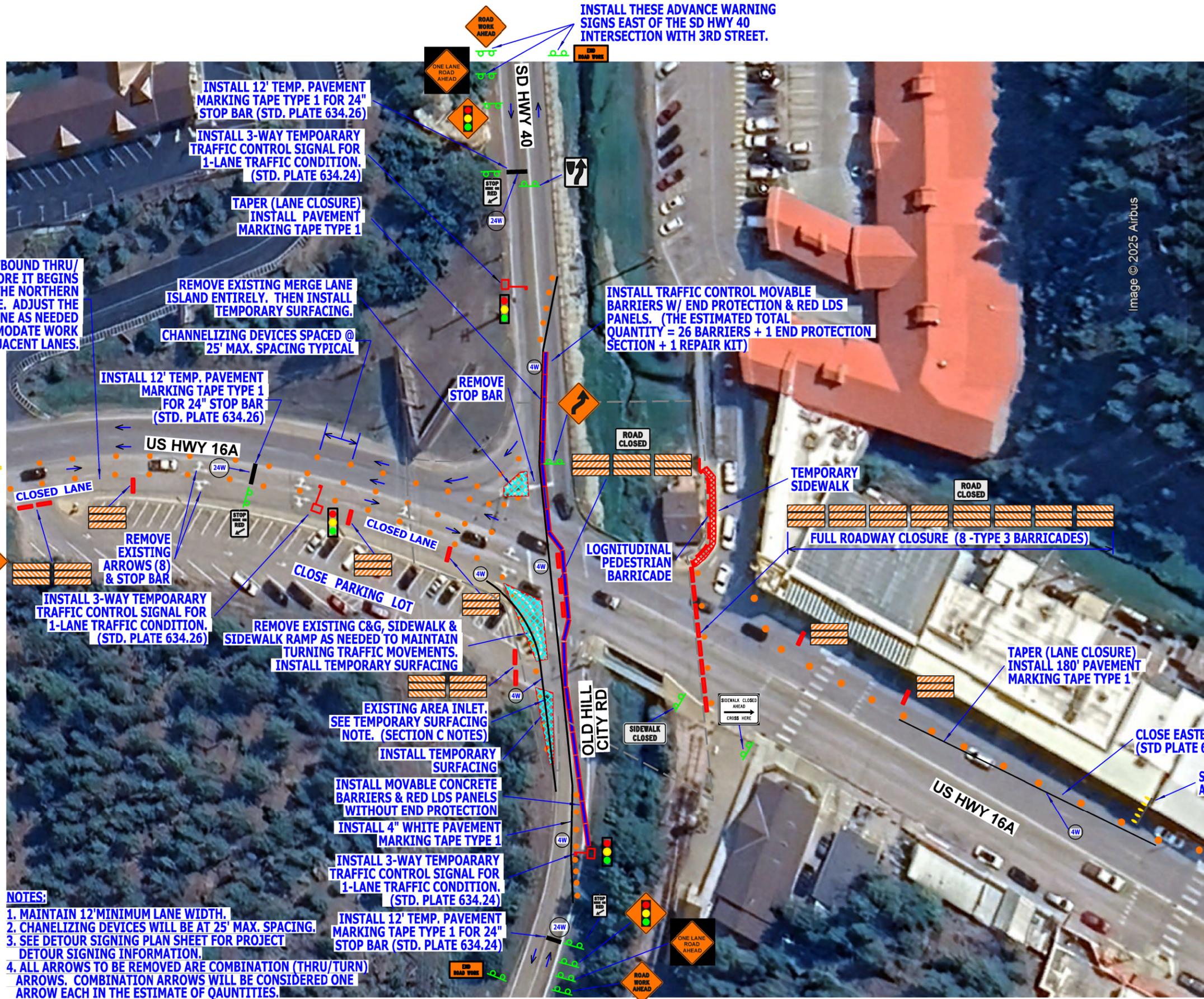


Image © 2025 Airbus

CLOSE THE EXISTING WESTBOUND THRU/ RIGHT TURN LANE BEFORE IT BEGINS IMMEDIATELY NORTH OF THE NORTHERN PARKING LOT ENTRANCE. ADJUST THE LOCATION OF THE CLOSED LANE AS NEEDED (LANE SHIFT) TO ACCOMMODATE WORK IN ADJACENT LANES.

REMOVE EXISTING MERGE LANE ISLAND ENTIRELY. THEN INSTALL TEMPORARY SURFACING.

INSTALL TRAFFIC CONTROL MOVABLE BARRIERS W/ END PROTECTION & RED LDS PANELS. (THE ESTIMATED TOTAL QUANTITY = 26 BARRIERS + 1 END PROTECTION SECTION + 1 REPAIR KIT)

INSTALL 12' TEMP. PAVEMENT MARKING TAPE TYPE 1 FOR 24\"/>

REMOVE STOP BAR

TEMPORARY SIDEWALK

FULL ROADWAY CLOSURE (8 -TYPE 3 BARRICADES)

INSTALL 3-WAY TEMPORARY TRAFFIC CONTROL SIGNAL FOR 1-LANE TRAFFIC CONDITION. (STD. PLATE 634.26)

REMOVE EXISTING C&G, SIDEWALK & SIDEWALK RAMP AS NEEDED TO MAINTAIN TURNING TRAFFIC MOVEMENTS. INSTALL TEMPORARY SURFACING

LONGITUDINAL PEDESTRIAN BARRICADE

TAPER (LANE CLOSURE) INSTALL 180' PAVEMENT MARKING TAPE TYPE 1

EXISTING AREA INLET. SEE TEMPORARY SURFACING NOTE. (SECTION C NOTES)

INSTALL TEMPORARY SURFACING

INSTALL MOVABLE CONCRETE BARRIERS & RED LDS PANELS WITHOUT END PROTECTION

INSTALL 4\"/>

INSTALL 3-WAY TEMPORARY TRAFFIC CONTROL SIGNAL FOR 1-LANE TRAFFIC CONDITION. (STD. PLATE 634.24)

INSTALL 12' TEMP. PAVEMENT MARKING TAPE TYPE 1 FOR 24\"/>

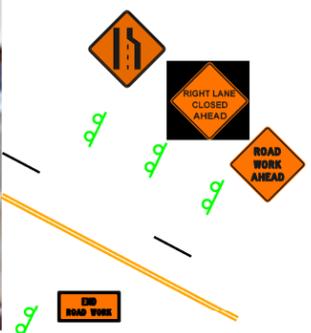
CLOSE EASTBOUND RIGHT LANE (STD PLATE 634.47)

SEQUENTIAL ARROW BOARD

NOTES:

1. MAINTAIN 12' MINIMUM LANE WIDTH.
2. CHANNELIZING DEVICES WILL BE AT 25' MAX. SPACING.
3. SEE DETOUR SIGNING PLAN SHEET FOR PROJECT DETOUR SIGNING INFORMATION.
4. ALL ARROWS TO BE REMOVED ARE COMBINATION (THRU/TURN) ARROWS. COMBINATION ARROWS WILL BE CONSIDERED ONE ARROW EACH IN THE ESTIMATE OF QUANTITIES.

- TEMPORARY SIDEWALK
- TEMPORARY SURFACING
- WORK ZONE

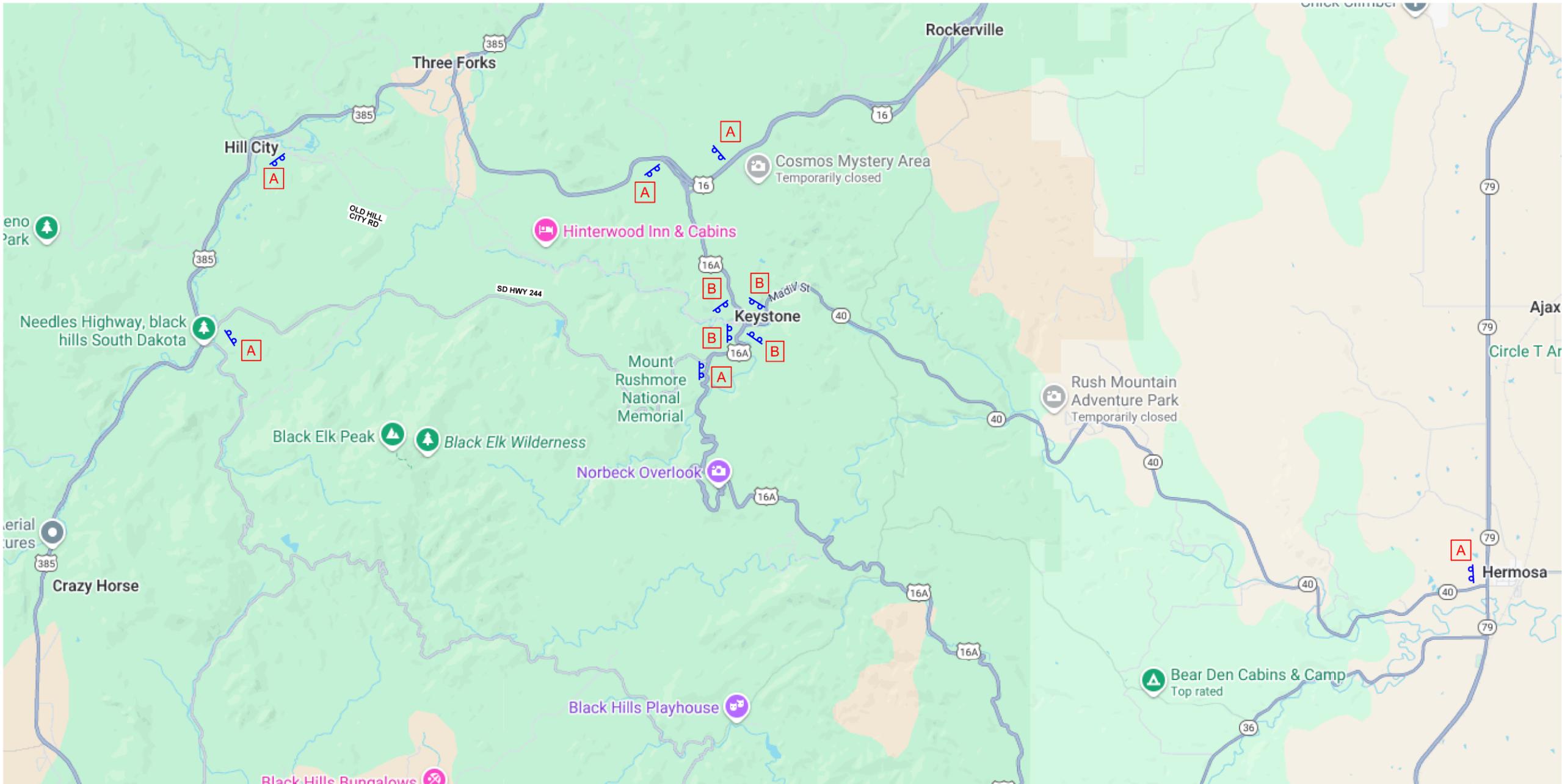


File: ...IPenn0518\Section C1TitleC.dgn

RESTRICTION SIGNING

STATE OF SOUTH DAKOTA	PROJECT P 016A(11)56	SHEET C7	TOTAL SHEETS C17
-----------------------	-------------------------	-------------	---------------------

Plotting Date: 02/07/2026



RESTRICTION

WIDTH 12 FT MAX
LENGTH 50 FT MAX

16A **40**

AT KEYSTONE

USE ALT ROUTE

B = NO VEHICLES
OVER 12 FT WIDE
OVER 50 FT LONG



Plot Scale - 1:200

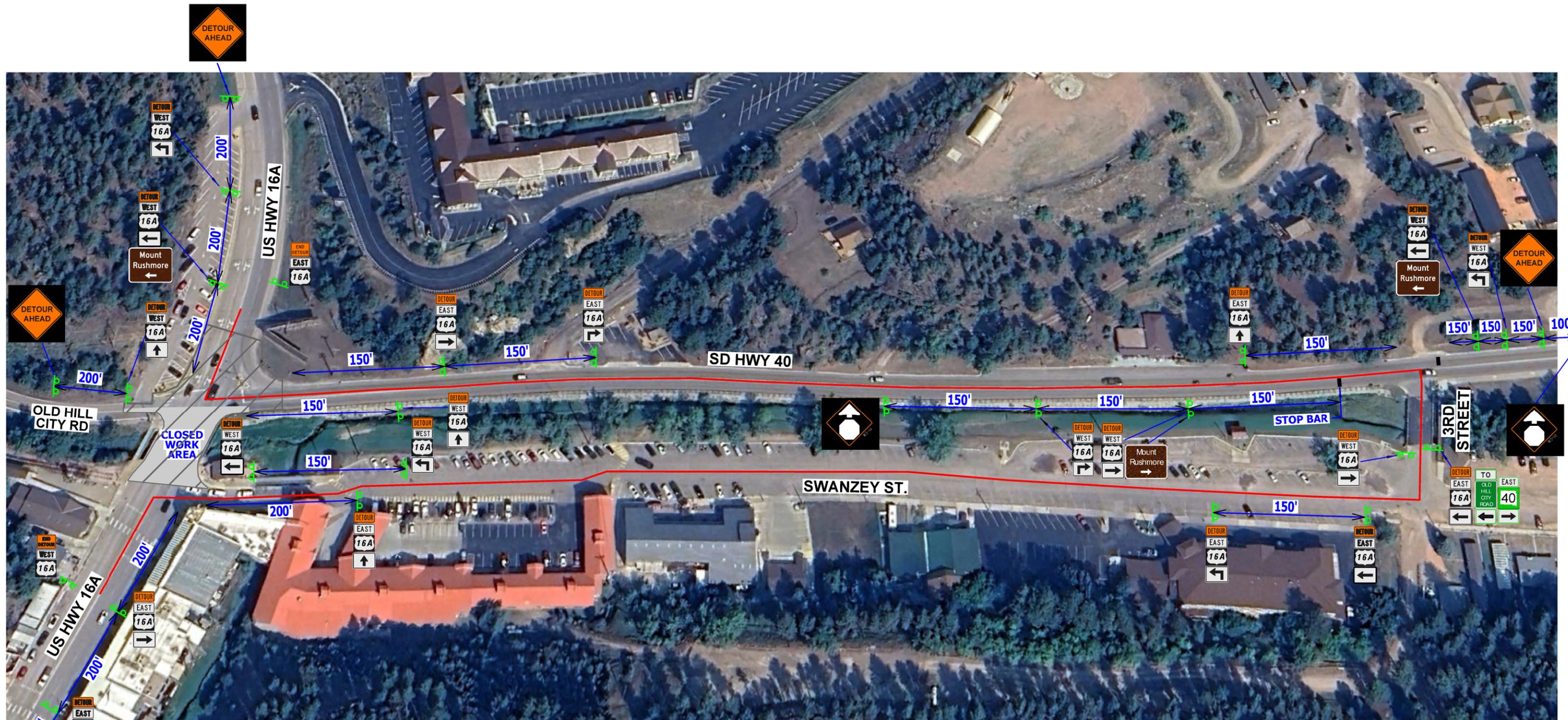
Plotted From - TRCU10208

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DETOUR SIGNING

STATE OF SOUTH DAKOTA	PROJECT P 016A(11)56	SHEET C8	TOTAL SHEETS C17
-----------------------	-------------------------	-------------	---------------------

Plotting Date: 02/07/2026



DETOUR AHEAD (W20-2)

DETOUR (M4-8)

EAST (M3-2) / **WEST** (M3-4)

16A (M1-4) / **16A** (M1-4)

DETOUR AHEAD (W20-2)

END DETOUR (M4-8a)

TO (M4-5 SD) / **EAST** (M3-2 SD)

OLD HILL CITY ROAD (M1-5 SD40) / **40** (M1-5 SD40)

← (M6-1SD LT) / **→** (M6-1SD RT)

↑ (M6-3) / **←** (M6-1) / **↶** (M5-1)

NOTE:
SEE DETOUR SIGNING (3RD STREET/ SD HWY 40 INTERSECTION)
PLAN SHEET FOR "ALL WAY STOP" CONFIGURATION.



Plot Scale: 1:200

Plotted From: TRCU10208

File: ...I:\Penn0518\Section_C1TitleC.dgn

DETOUR SIGNING (3RD STREET/ SD HWY 40 INTERSECTION)

STATE OF SOUTH DAKOTA	PROJECT P 016A(11)56	SHEET C9	TOTAL SHEETS C17
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Plotting Date: 02/07/2026



Plot Scale - 1:200



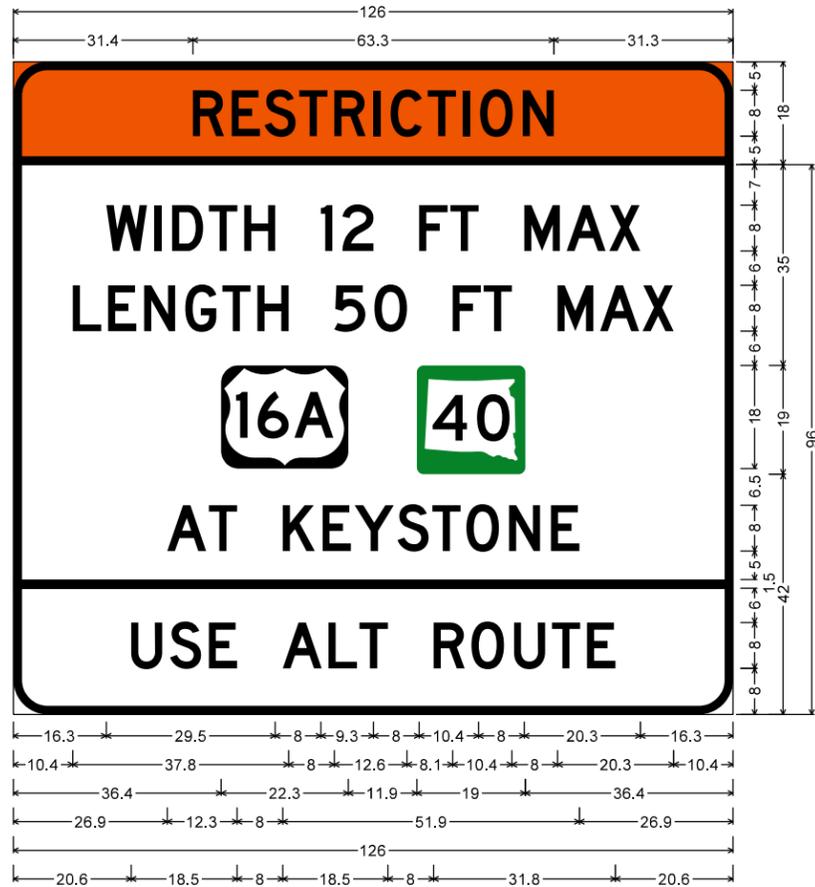
— DETOUR ROUTE

Plotted From - TRCU10208

File - ...IPenn0518\Section C1TitleC.dgn

Image © 2026 Airbus

TRAFFIC CONTROL SPECIAL SIGN DESIGNS

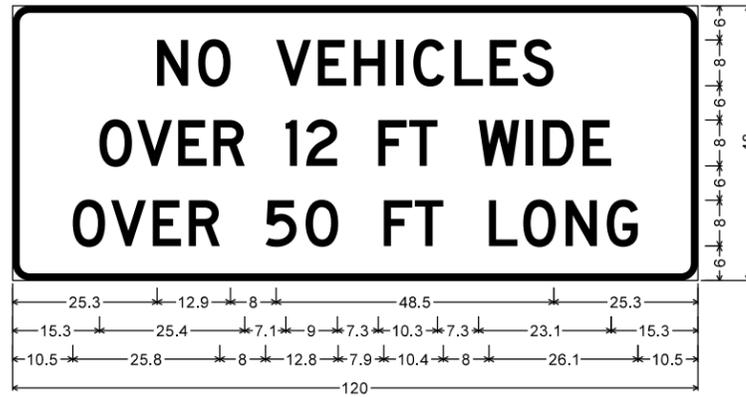


6.0" Radius, 1.5" Border, Black on Orange;
"RESTRICTION", D 2K;

6.0" Radius, 1.5" Border, Black on White;
"WIDTH 12 FT MAX", D 2K; "LENGTH 50 FT MAX", D 2K;
Rounded Rectangle 3.0" Radius;
State Highway 40 M1-5 8.0" D 2K; "AT KEYSTONE", D 2K; "USE ALT ROUTE", D 2K;

Table of letter and object lefts

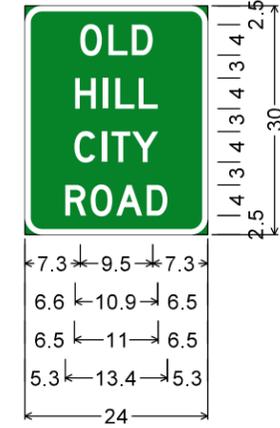
R	E	S	T	R	I	C	T	I	O	N		
31.4	38.3	44.0	50.0	56.3	63.0	66.0	72.5	78.6	81.8	89.1		
W	I	D	T	H	I	Z	F	T	M	A	X	
16.3	24.5	27.8	34.3	40.4	53.9	57.6	71.0	76.5	89.5	96.9	104.4	
L	E	N	G	T	H	S	O	F	T	M	A	X
10.4	16.5	22.9	30.1	36.5	42.8	56.1	63.3	76.9	82.4	95.3	102.8	110.1
■	40											
36.4	70.6											
A	T	K	E	Y	S	T	O	N	E			
26.9	34.3	47.1	54.1	59.8	67.3	73.4	79.4	86.8	94.1			
-	0											
-0.0												
U	S	E	A	L	T	R	O	U	T	E		
20.6	27.4	34.3	47.1	55.1	60.6	73.5	80.3	87.6	94.3	100.5		



3.0" Radius, 1.3" Border, Black on White;
"NO VEHICLES", D 2K; "OVER 12 FT WIDE", D 2K 90% spacing;
"OVER 50 FT LONG", D 2K;

Table of letter and object lefts

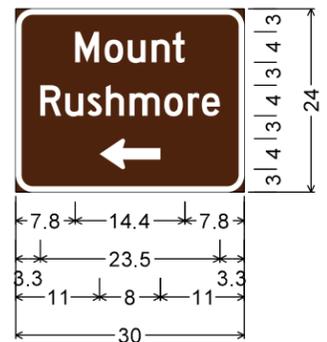
N	O	V	E	H	I	C	L	E	S		
25.3	32.5	46.1	53.5	59.8	67.1	70.1	77.4	83.5	89.3		
O	V	E	R	I	Z	F	T	W	I	D	E
15.3	21.9	29.0	35.1	47.9	51.4	64.0	69.4	81.6	89.8	92.8	99.9
O	V	E	R	5	O	F	T	L	O	N	G
10.5	17.3	24.5	30.9	44.3	51.4	65.0	70.5	83.4	89.4	96.9	104.0



2.3" Radius, 0.8" Border, White on Green;
"OLD", D 2K;
"HILL", D 2K;
"CITY", D 2K;
"ROAD", D 2K;

Table of letter and object lefts

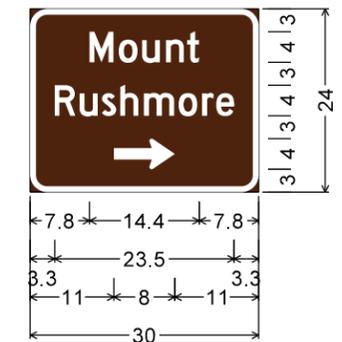
O	L	D	
7.3	11.0	14.0	
H	I	L	L
6.6	10.3	11.9	15.0
C	I	T	Y
6.5	10.1	11.4	14.0
R	O	A	D
5.3	8.6	12.0	16.0



2.3" Radius, 0.8" Border, White on Brown;
"Mount", D 2K;
"Rushmore", D 2K;
Standard Arrow Custom 8.0" X 4.0" 180°;

Table of letter and object lefts

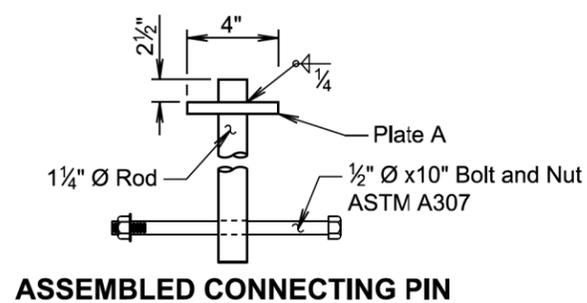
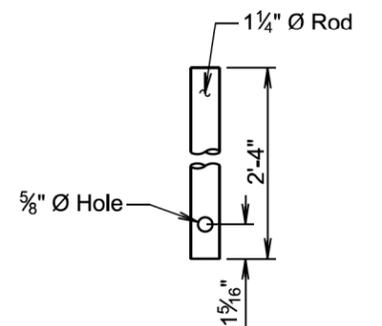
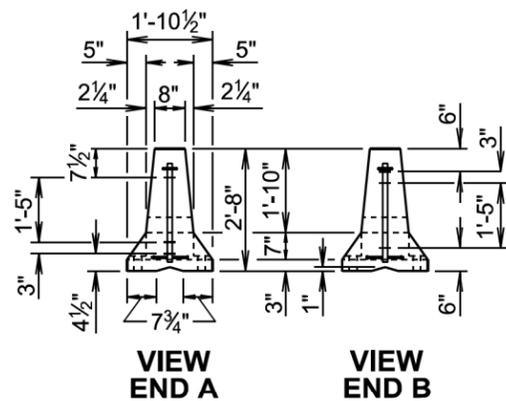
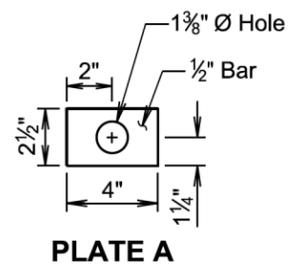
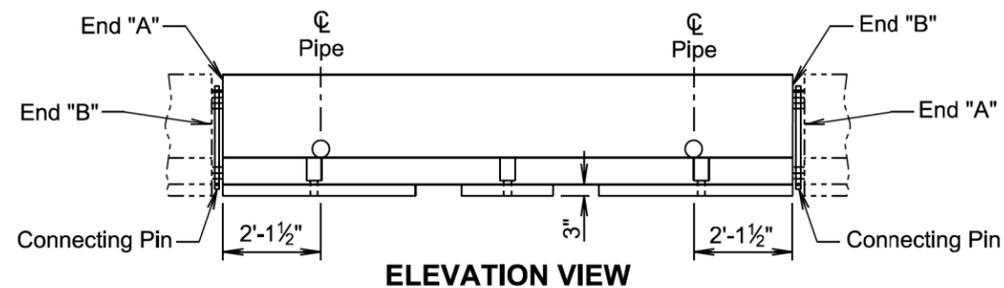
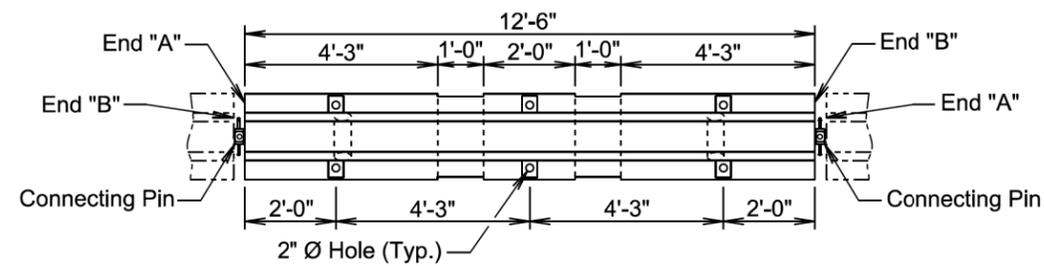
M	o	u	n	t			
7.8	11.6	14.6	17.9	20.6			
R	u	s	h	m	o	r	e
3.3	6.5	9.4	11.9	15.0	19.5	22.6	24.4
←							
11.0							



2.3" Radius, 0.8" Border, White on Brown;
"Mount", D 2K;
"Rushmore", D 2K;
Standard Arrow Custom 8.0" X 4.0" 0°;

Table of letter and object lefts

M	o	u	n	t			
7.8	11.6	14.6	17.9	20.6			
R	u	s	h	m	o	r	e
3.3	6.5	9.4	11.9	15.0	19.5	22.6	24.4
→							
11.0							



September 14, 2018

September 14, 2018

Published Date: 2026

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**TRAFFIC CONTROL MOVABLE CONCRETE BARRIERS
(F SHAPE INTERIOR SECTION)**

PLATE NUMBER
628.01

Sheet 1 of 2

GENERAL NOTES:

The detailed drawings are for illustrative purpose and depicts the current version of the F shape concrete barrier. If new movable concrete barriers are requested on a project, they will be constructed according to the F shape movable concrete barrier details on standard plate 628.10.

Each movable concrete barrier section weighs 5030 ± pounds.

Each movable concrete barrier section is detailed to provide end "A" to end "B" connection by insertion of a pin through steel loops.

The Jersey shape or any version of the F shape traffic control movable concrete barriers may be used on a project, however, only the same type or version will be used for each run of barriers.

Movable concrete barrier sections will be placed to provide uniform bearing of the sections with the paved surface as approved by the Engineer.

Movable concrete barrier sections will never be moved or lifted using the end loops.

Movable concrete barrier sections that have been damaged will not be used. Barrier sections are considered damaged if the loops are end welded onto existing damaged loops, loops are fractured, or there is exposed rebar from fractured concrete.

All cost for transporting the barriers from the specified location to the project site, installing, and returning the barriers to the specified location will be incidental to the contract unit price per each for "Traffic Control Movable Concrete Barrier".

If the concrete barriers need to be moved and reset on the project, requiring the barriers to be transported by truck, all cost for removing, transporting, and resetting the barriers will be incidental to the contract unit price per each for "Remove and Reset Traffic Control Movable Concrete Barrier". All cost for small shifts in alignment of the barriers, not requiring the barriers to be transported by truck, will be incidental to various contract items.

Published Date: 2026

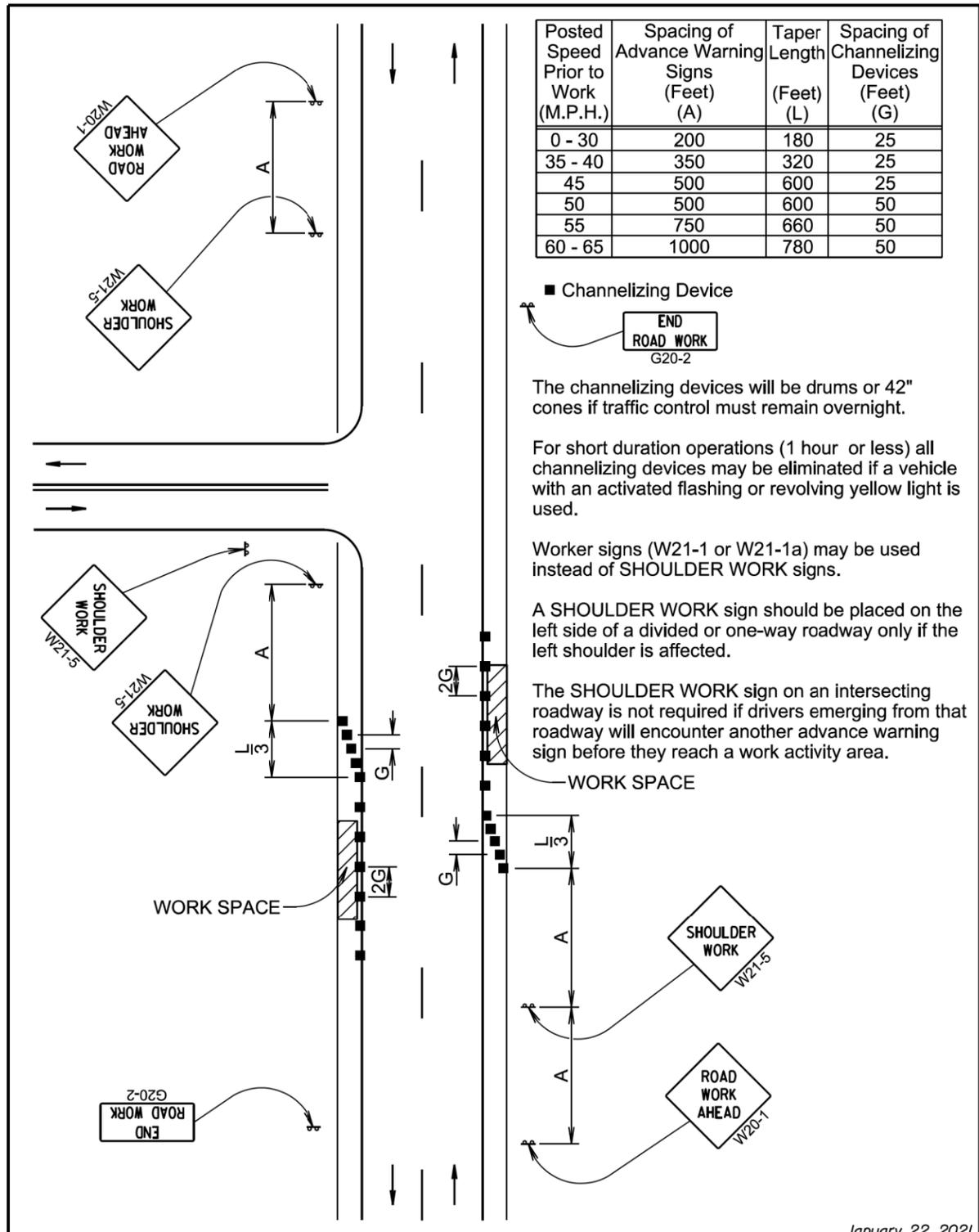
**S
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**TRAFFIC CONTROL MOVABLE CONCRETE BARRIERS
(F SHAPE INTERIOR SECTION)**

PLATE NUMBER
628.01

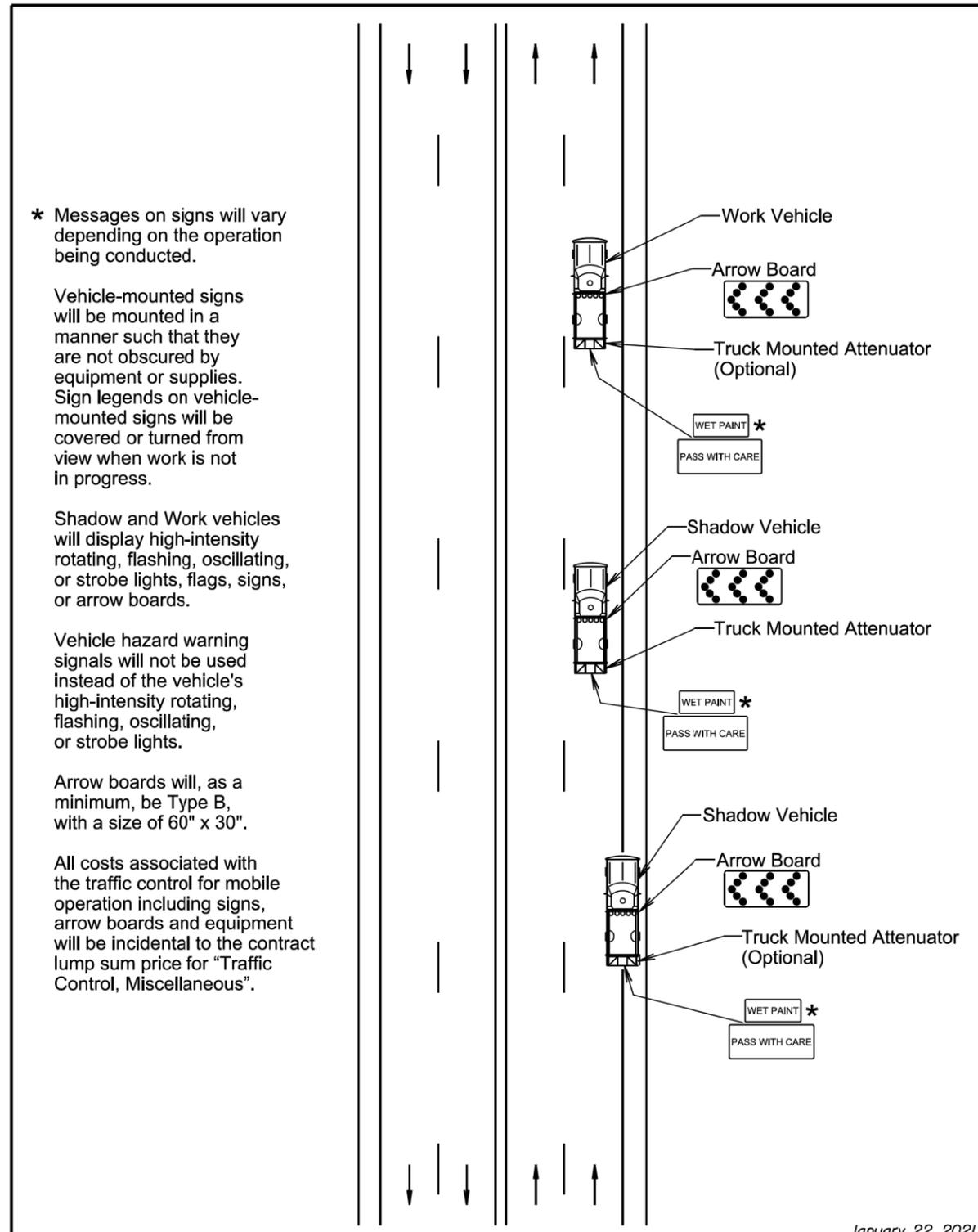
Sheet 2 of 2

Plot Scale - 1:200



January 22, 2021

S D D O T	WORK ON SHOULDERS	PLATE NUMBER 634.03
	Published Date: 2026	Sheet 1 of 1



January 22, 2021

S D D O T	MOBILE OPERATIONS ON MULTI-LANE HIGHWAYS	PLATE NUMBER 634.08
	Published Date: 2026	Sheet 1 of 1

Plotted From - TRCU10208

File - ...Section C05V8_StdPlates.dgn

Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)	Spacing of Channelizing Devices (Feet) (G)
0 - 30	200	25
35 - 40	350	25
45	500	25
50	500	50
55	750	50
60 - 65	1000	50

- Flagger
- Channelizing Device

For low-volume traffic situations with short work zones on straight roadways where the flagger is visible to road users approaching from both directions, a single flagger may be used.

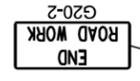
The ROAD WORK AHEAD and the END ROAD WORK signs may be omitted for short duration operations (1 hour or less).

For tack and/or flush seal operations, when flaggers are not being used, the FRESH OIL sign (W21-2) will be displayed in advance of the liquid asphalt areas.

Flashing warning lights and/or flags may be used to call attention to the advance warning signs.

The channelizing devices will be drums or 42" cones.

Channelizing devices are not required along the centerline adjacent to work area when pilot cars are utilized for escorting traffic through the work area.

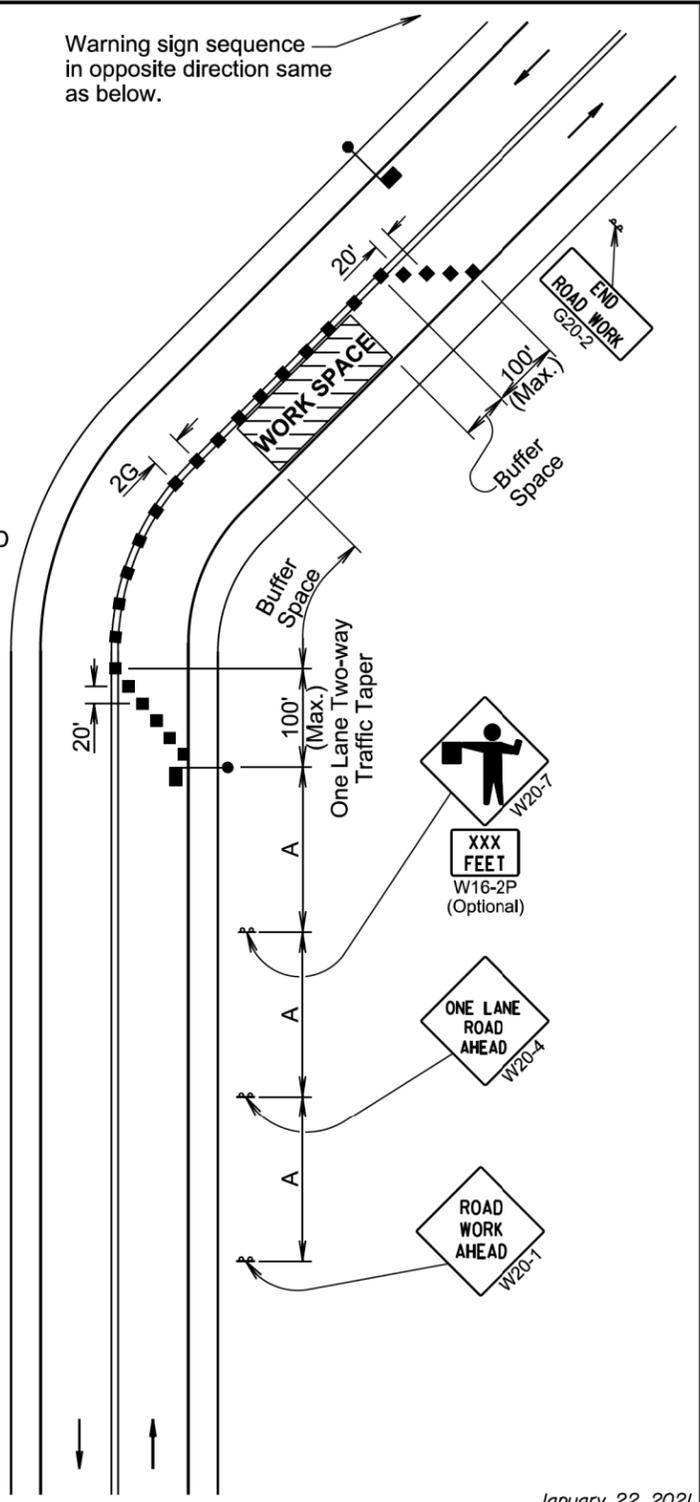


Channelizing devices and flaggers will be used at intersecting roads to control intersecting road traffic as required.

The buffer space should be extended so that the two-way traffic taper is placed before a horizontal or vertical curve to provide adequate sight distance for the flagger and queue of stopped vehicles.

The length of A may be adjusted to fit field conditions.

Warning sign sequence in opposite direction same as below.

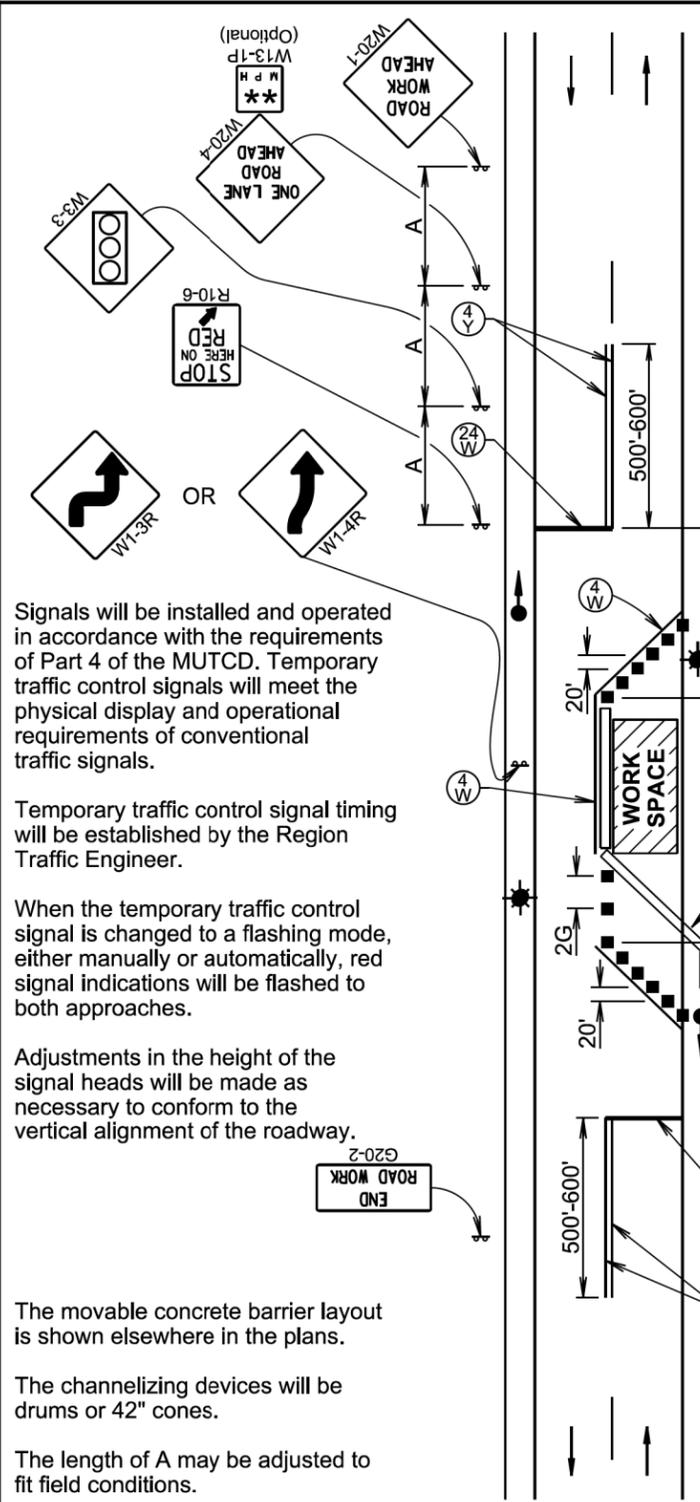


January 22, 2021

S D D O T	LANE CLOSURE WITH FLAGGER PROVIDED	PLATE NUMBER 634.23
	Published Date: 2026	Sheet 1 of 1

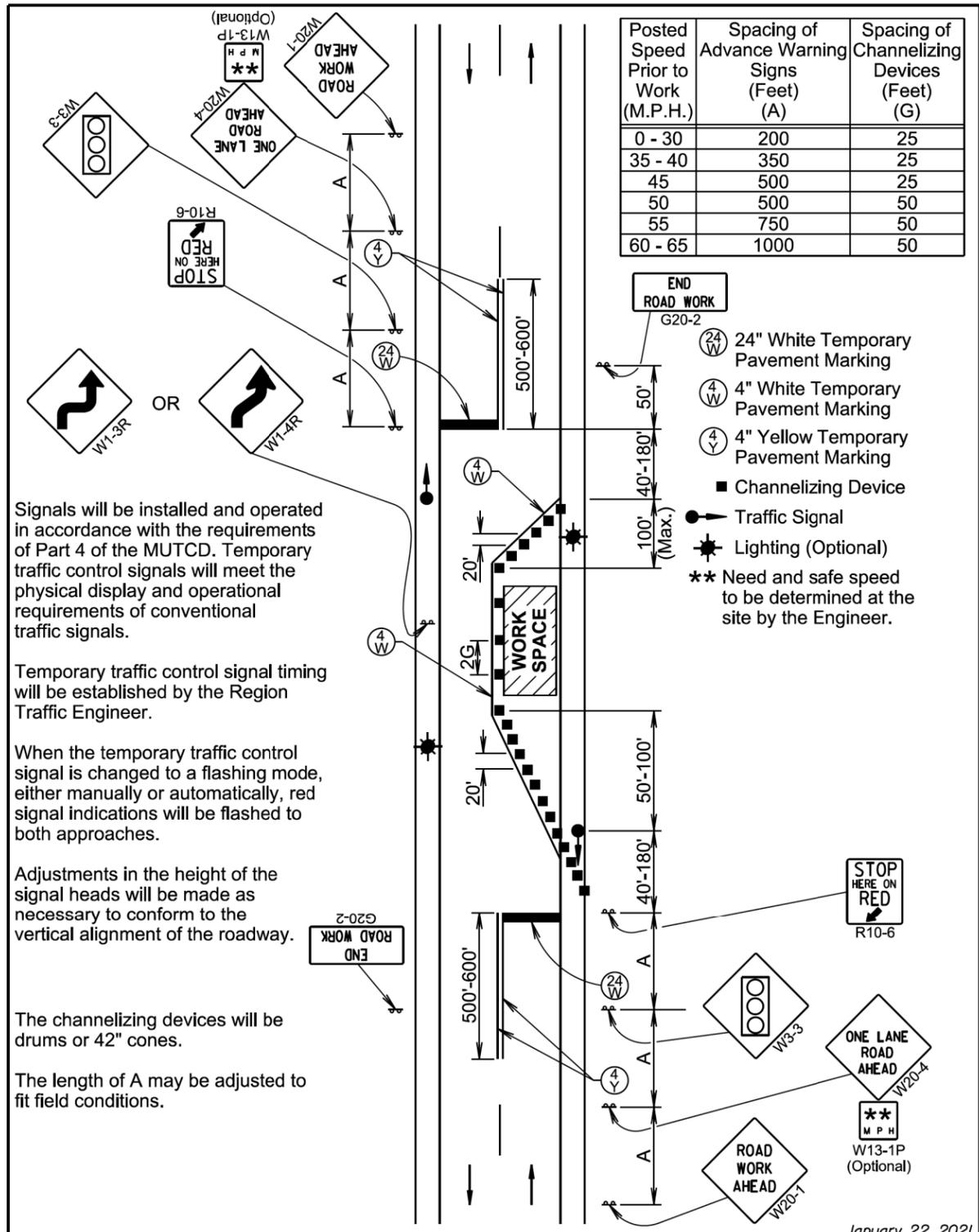
Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)	Spacing of Channelizing Devices (Feet) (G)
0 - 30	200	25
35 - 40	350	25
45	500	25
50	500	50
55	750	50
60 - 65	1000	50

- END ROAD WORK G20-2
- 24" White Temporary Pavement Marking (24 W)
- 4" White Temporary Pavement Marking (4 W)
- 4" Yellow Temporary Pavement Marking (4 Y)
- Channelizing Device
- Traffic Signal
- Lighting (Optional)
- ** Need and safe speed to be determined at the site by the Engineer.



January 22, 2021

S D D O T	LANE CLOSURE USING TRAFFIC SIGNALS AND BARRIER	PLATE NUMBER 634.24
	Published Date: 2026	Sheet 1 of 1



Signals will be installed and operated in accordance with the requirements of Part 4 of the MUTCD. Temporary traffic control signals will meet the physical display and operational requirements of conventional traffic signals.

Temporary traffic control signal timing will be established by the Region Traffic Engineer.

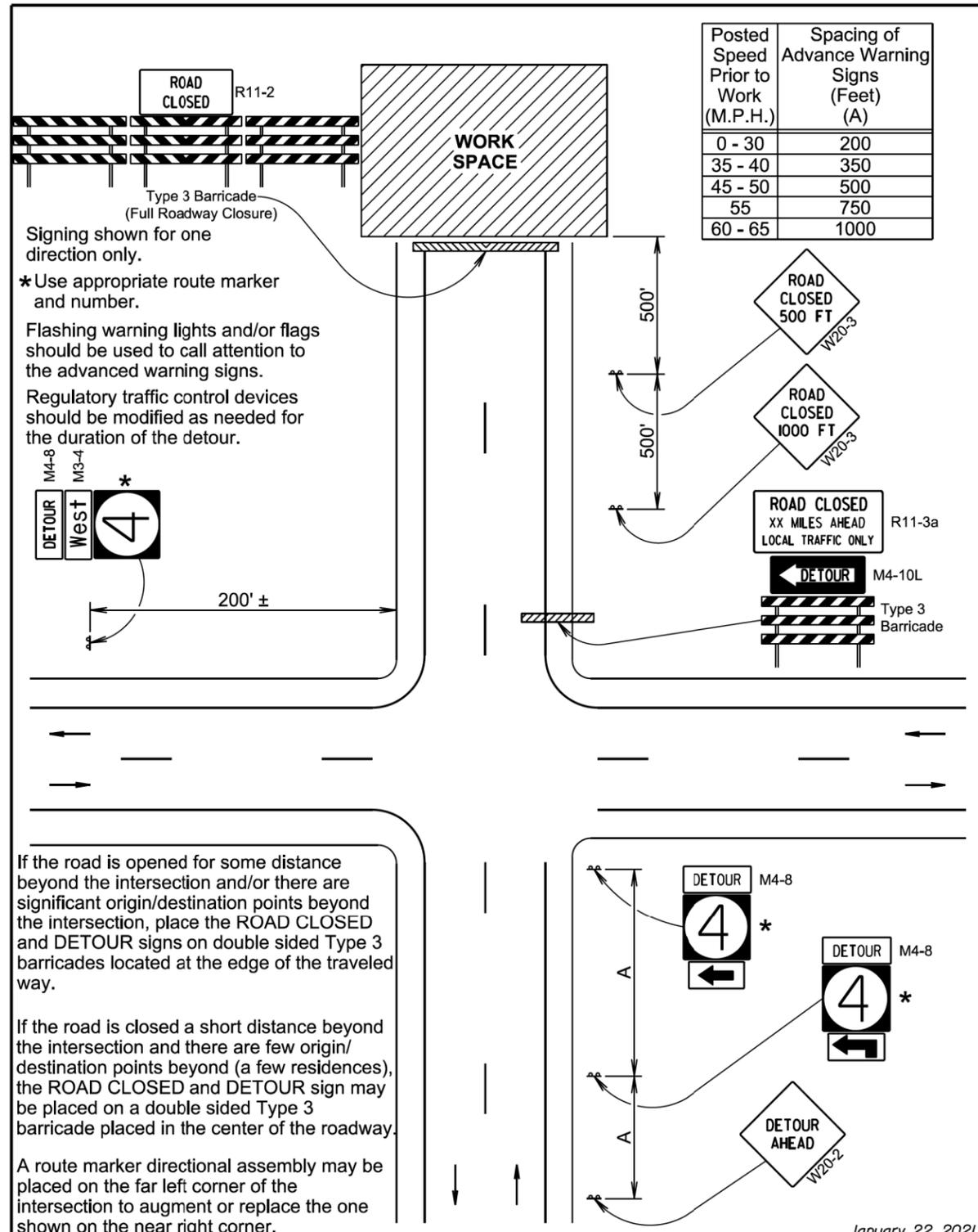
When the temporary traffic control signal is changed to a flashing mode, either manually or automatically, red signal indications will be flashed to both approaches.

Adjustments in the height of the signal heads will be made as necessary to conform to the vertical alignment of the roadway.

The channelizing devices will be drums or 42" cones.

The length of A may be adjusted to fit field conditions.

S D D O T	LANE CLOSURE USING TRAFFIC SIGNALS	PLATE NUMBER 634.26
	Published Date: 2026	Sheet 1 of 1



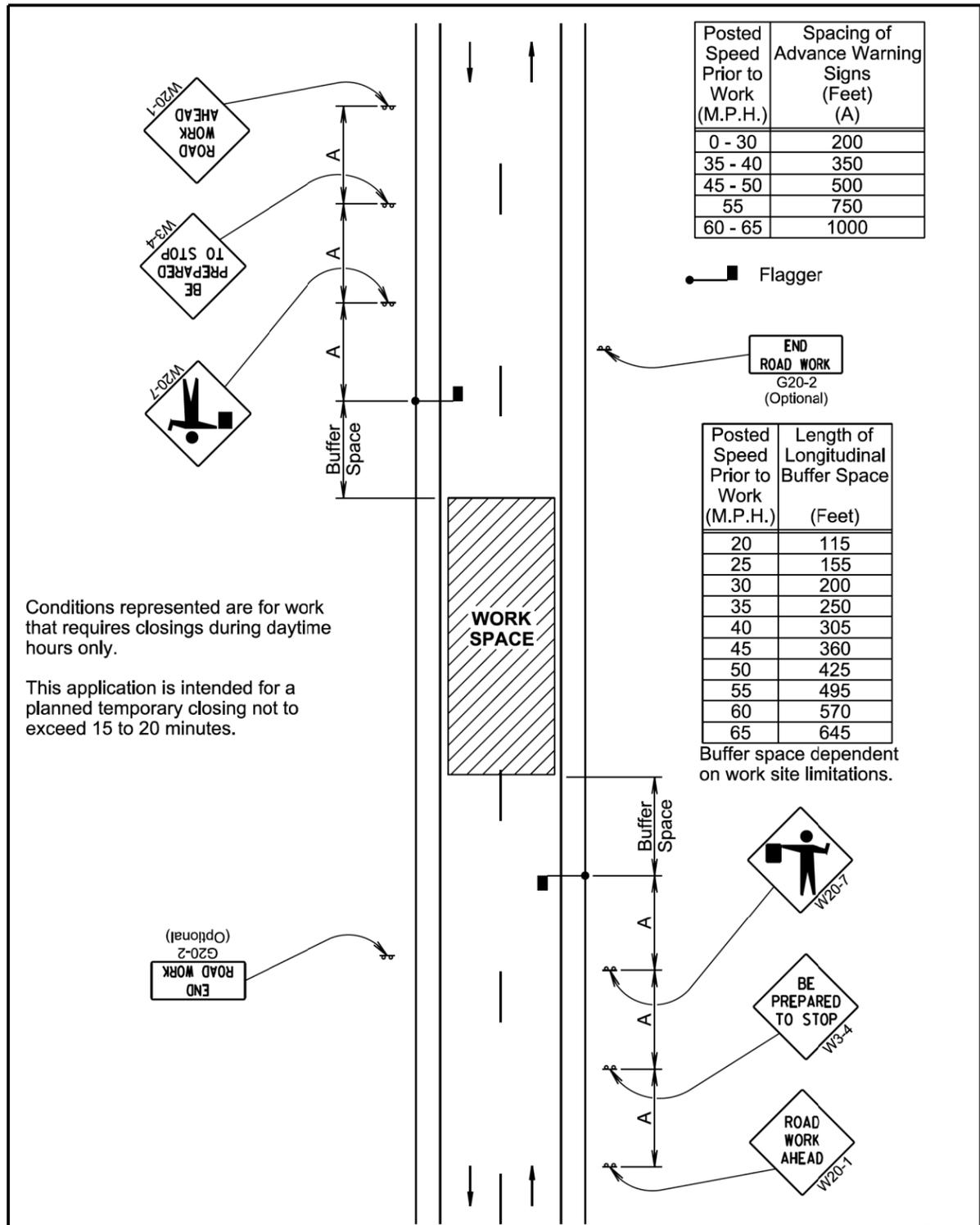
If the road is opened for some distance beyond the intersection and/or there are significant origin/destination points beyond the intersection, place the ROAD CLOSED and DETOUR signs on double sided Type 3 barricades located at the edge of the traveled way.

If the road is closed a short distance beyond the intersection and there are few origin/destination points beyond (a few residences), the ROAD CLOSED and DETOUR sign may be placed on a double sided Type 3 barricade placed in the center of the roadway.

A route marker directional assembly may be placed on the far left corner of the intersection to augment or replace the one shown on the near right corner.

S D D O T	ROAD CLOSED WITH OFF-SITE DETOUR	PLATE NUMBER 634.29
	Published Date: 2026	Sheet 1 of 1

Plot Scale - 1:200

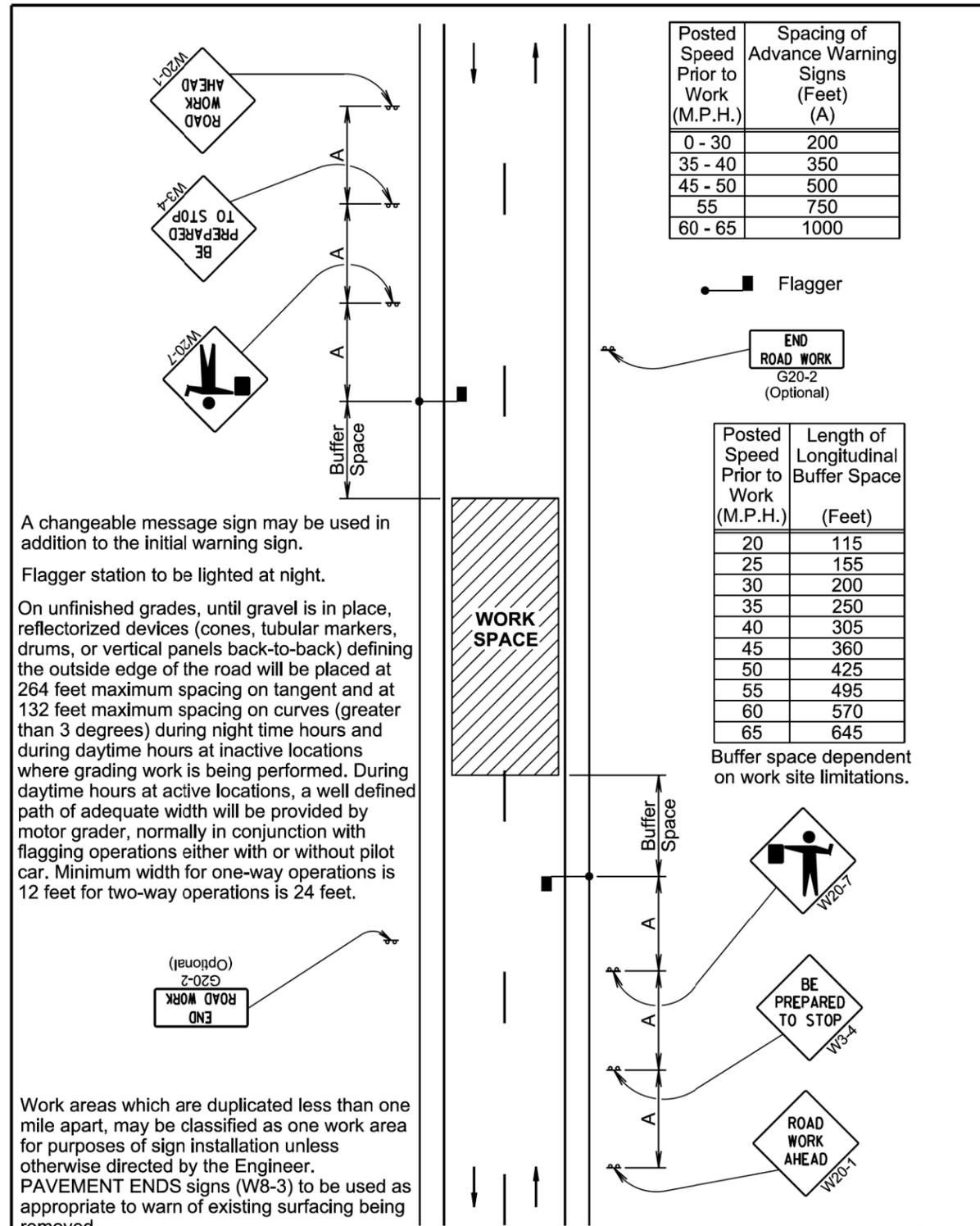


Conditions represented are for work that requires closings during daytime hours only.

This application is intended for a planned temporary closing not to exceed 15 to 20 minutes.

January 22, 2021

S D D O T	TEMPORARY ROAD WORK	PLATE NUMBER 634.30
	Published Date: 2026	Sheet 1 of 1



A changeable message sign may be used in addition to the initial warning sign.

Flagger station to be lighted at night.

On unfinished grades, until gravel is in place, reflectorized devices (cones, tubular markers, drums, or vertical panels back-to-back) defining the outside edge of the road will be placed at 264 feet maximum spacing on tangent and at 132 feet maximum spacing on curves (greater than 3 degrees) during night time hours and during daytime hours at inactive locations where grading work is being performed. During daytime hours at active locations, a well defined path of adequate width will be provided by motor grader, normally in conjunction with flagging operations either with or without pilot car. Minimum width for one-way operations is 12 feet for two-way operations is 24 feet.

Work areas which are duplicated less than one mile apart, may be classified as one work area for purposes of sign installation unless otherwise directed by the Engineer. PAVEMENT ENDS signs (W8-3) to be used as appropriate to warn of existing surfacing being removed.

January 22, 2021

S D D O T	LONG TERM ROAD WORK	PLATE NUMBER 634.31
	Published Date: 2026	Sheet 1 of 1

File - ...Section C05V8_SldPlates.dgn

Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)	Taper Length (Feet) (L)	Spacing of Channelizing Devices (Feet) (G)
0 - 30	200	180	25
35 - 40	350	320	25
45	500	600	25
50	500	600	50 *
55	750	660	50 *
60 - 65	1000	780	50 *

* Spacing is 40' for 42" cones.

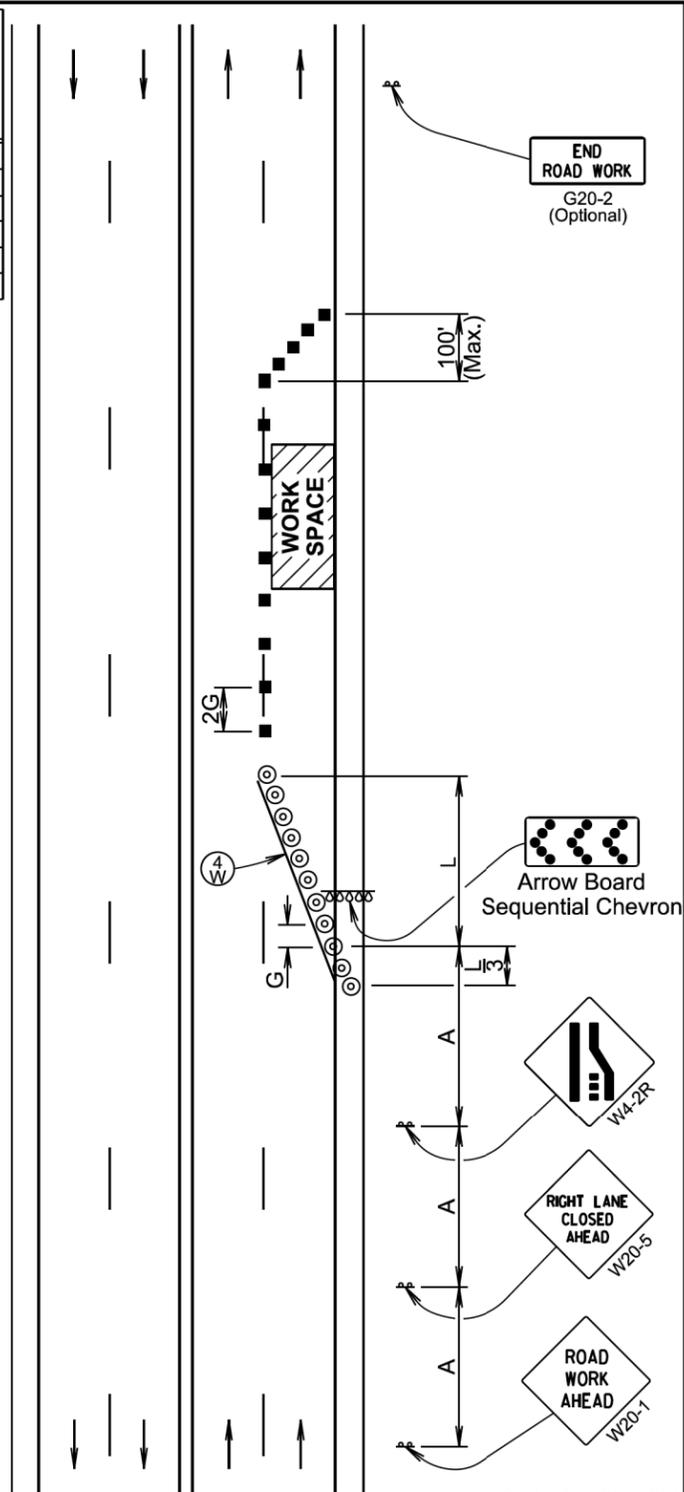
- ⊙ Reflectorized Drum
- Channelizing Device
- Ⓞ 4" White Temporary Pavement Marking

The channelizing devices will be 42" cones or drums.

42" cones may be used in place of the drums shown in the taper if setup will not be used during night time hours.

Temporary pavement markings will be used if traffic control must remain overnight.

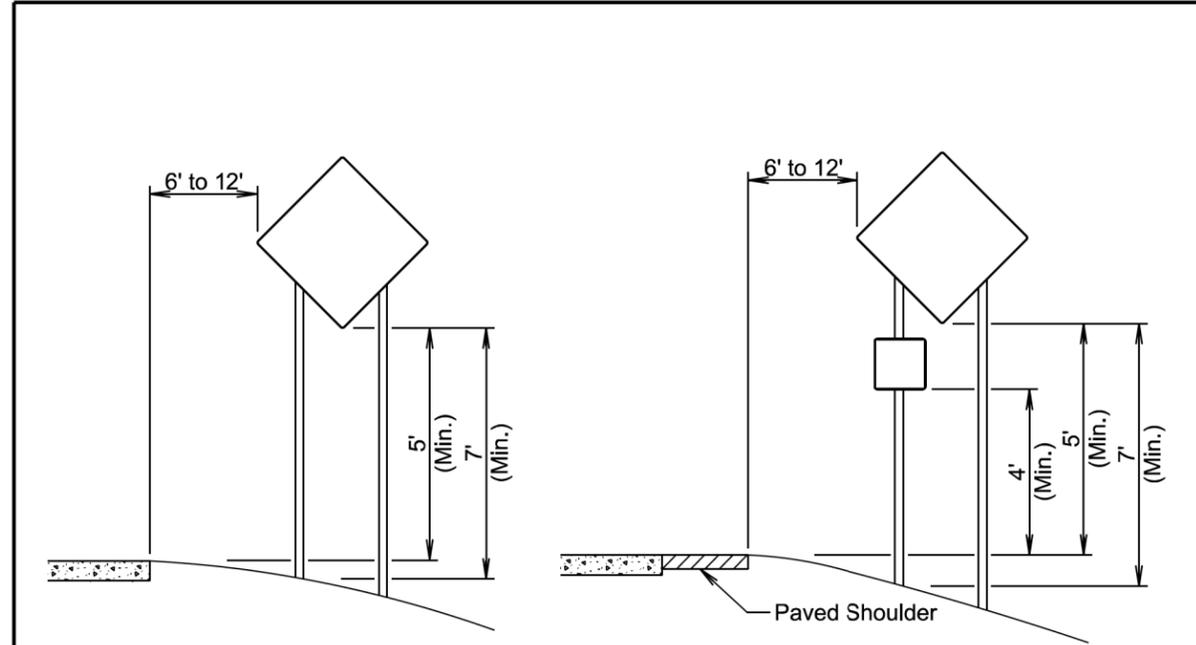
The length of A and L may be adjusted to fit field conditions.



September 22, 2021

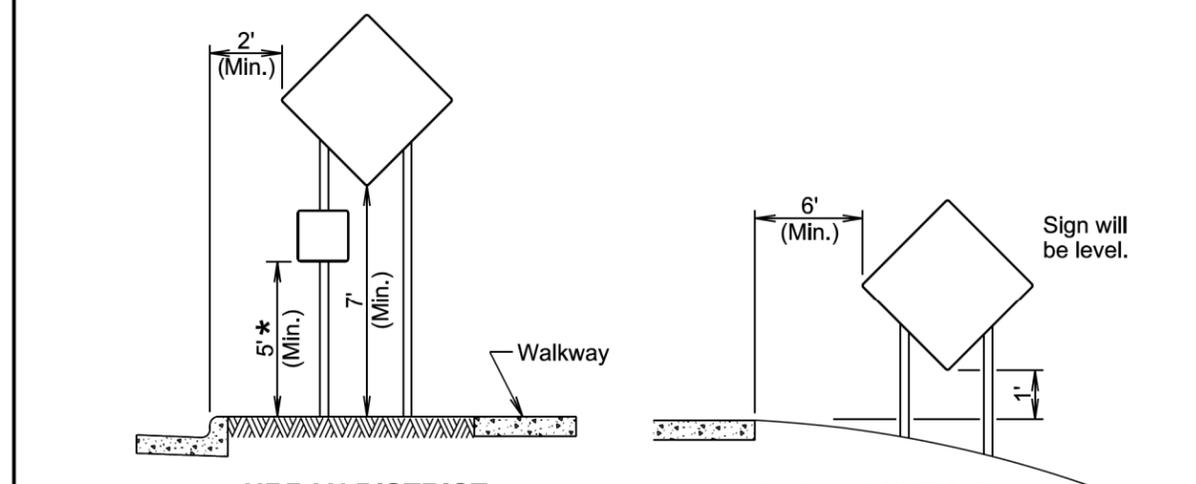
S D D O T	4-LANE UNDIVIDED, RIGHT LANE CLOSED	PLATE NUMBER 634.47
		Sheet 1 of 1

Published Date: 2026



RURAL DISTRICT

RURAL DISTRICT WITH SUPPLEMENTAL PLATE



URBAN DISTRICT

RURAL DISTRICT 3 DAY MAXIMUM

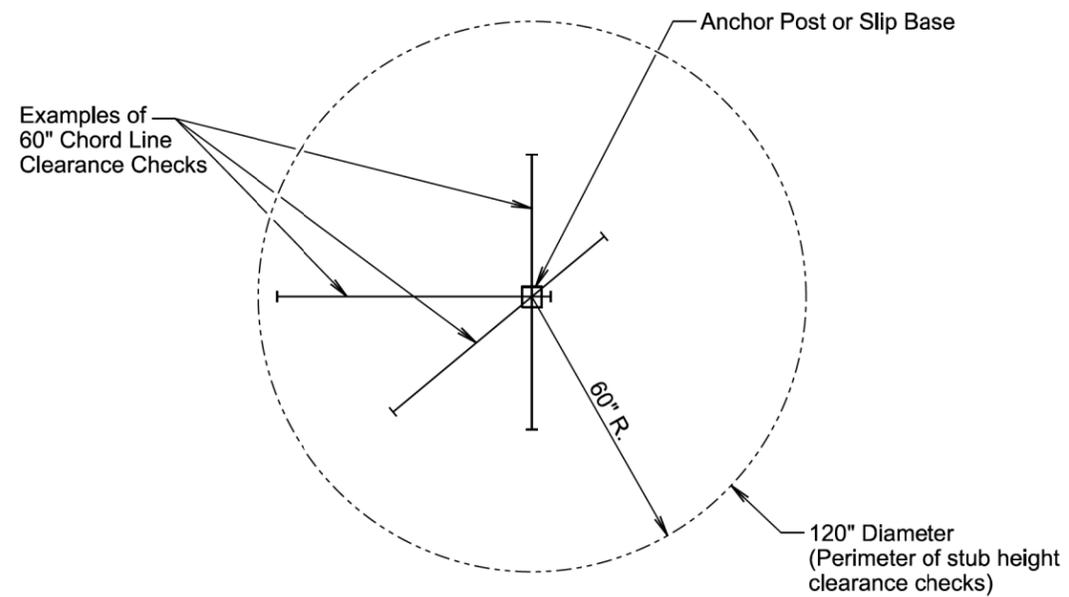
* If the bottom of supplemental plate is mounted lower than 7 feet above a pedestrian walkway, the supplemental plate should not project more than 4" into the pedestrian facility.

(Not applicable to regulatory signs)

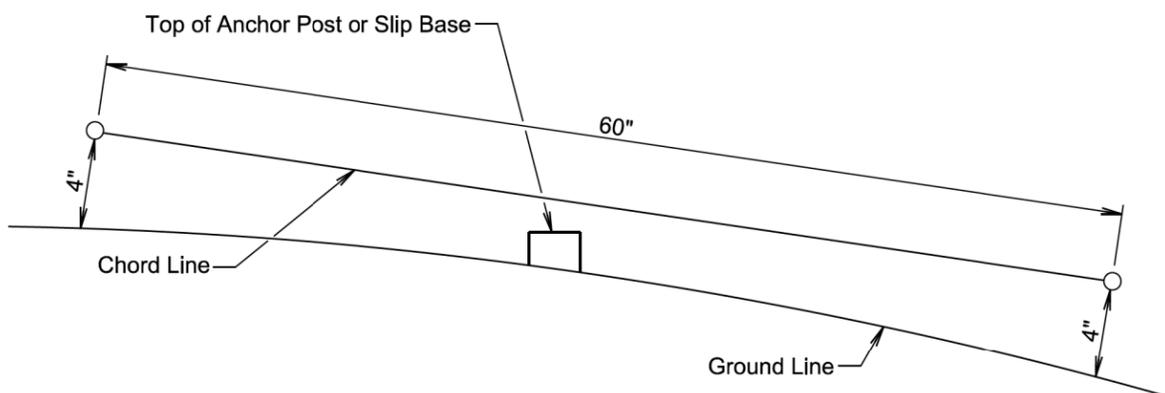
January 22, 2021

S D D O T	CRASHWORTHY SIGN SUPPORTS (Typical Construction Signing)	PLATE NUMBER 634.85
		Sheet 1 of 1

Published Date: 2026



PLAN VIEW
(Examples of stub height clearance checks)



ELEVATION VIEW

GENERAL NOTES:

The top of anchor posts and slip bases WILL NOT extend above a 60" chord line within a 120" diameter circle around the post with ends 4" above the ground.

At locations where there is curb and gutter adjacent to the breakaway sign support, the stub height will be a maximum of 4" above the ground line at the localized area adjacent to the breakaway support stub.

The 4" stub height clearance is not necessary for U-channel lap splices where the support is designed to yield (bend) at the base.

January 22, 2021

<i>Published Date: 2026</i>	S D D O T	BREAKAWAY SUPPORT STUB CLEARANCE	PLATE NUMBER 634.99
			Sheet 1 of 1

Plot Scale - 1:200

Plotted From - TRCU10208

File - ...Section C05V8_StdPlates.dgn