

SECTION C: TRAFFIC CONTROL PLANS

| | | | |
|-----------------------------|--------------------|-------|-----------------|
| STATE OF SOUTH DAKOTA | PROJECT | SHEET | TOTAL SHEETS |
| | __NH_212(206)313__ | C1 | C16 |

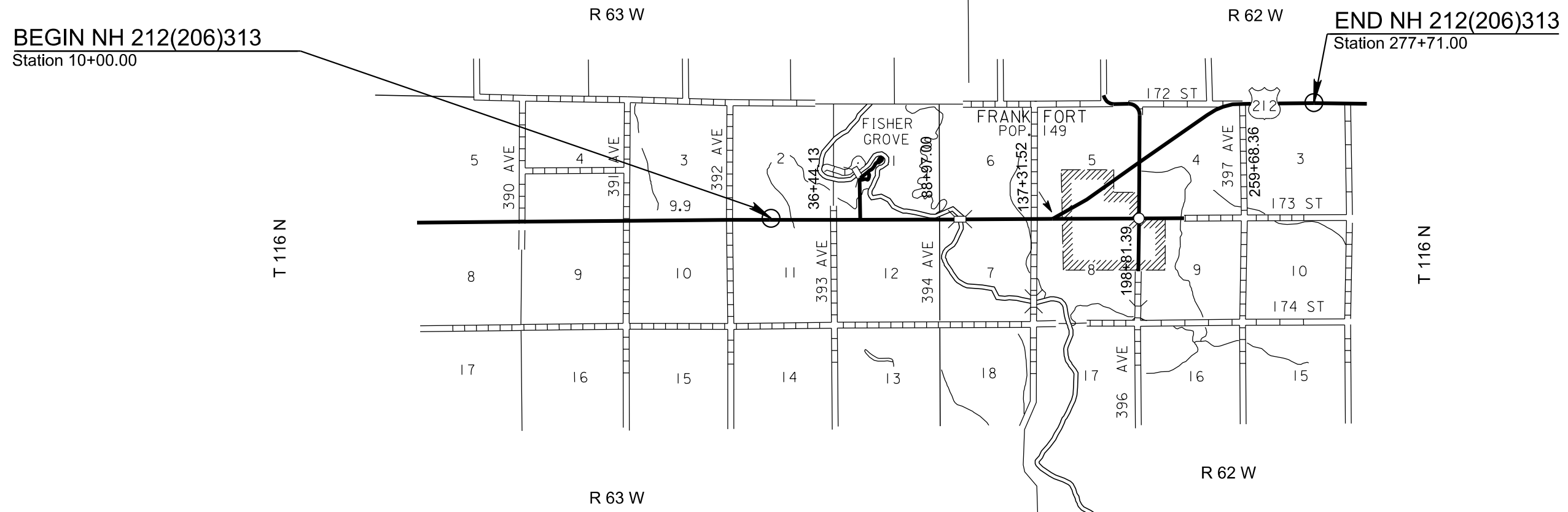
Plotting Date: 11/21/2024

INDEX OF SHEETS

- C1 General Layout with Index
- C2-C3 Estimate with General Notes and Tables
- C4 Fixed Location Sign Layout
- C5-C7 Detour Layout
- C8 Alternate Route Layout
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- C11 Itemized Signs
- C12-C16 Standard Plates

Plot Scale - 1:200

Plotted From - TRH\U\INT04



File - ...Section C\Title Section C.dgn

SECTION C ESTIMATE OF QUANTITIES

| BID ITEM NUMBER | ITEM | QUANTITY | UNIT |
|-----------------|--|----------|------|
| 004E0010 | Blading | 100 | Hour |
| 260E1010 | Base Course | 500.0 | Ton |
| 601E0600 | Dust Control Chloride, Haul Road Restoration | 16,000 | Lb |
| 634E0010 | Flagging | 100.0 | Hour |
| 634E0020 | Pilot Car | 50.0 | Hour |
| 634E0110 | Traffic Control Signs | 968.5 | SqFt |
| 634E0120 | Traffic Control, Miscellaneous | Lump Sum | LS |
| 634E0275 | Type 3 Barricade | 36 | Each |
| 634E1002 | Detour and Restriction Signing | 2,114.8 | SqFt |

SEQUENCE OF OPERATIONS

The following Sequence of Operations will be adhered to. Contractor request to deviate from the sequence of operations will be submitted in writing to the Engineer for review. Approval of an alternate sequence of operations will only be allowed when the proposed changes meet with the Department's intent for traffic control and sequencing of the work. An alternate sequence will be submitted for review a minimum of one week prior to potential implementation.

1. Install Detour and Road Closure signing prior to closure of US Highway 212.
2. Install storm water pollution prevention devices and strip inslopes.
3. Complete grading work and install surfacing.
4. Restore inslopes, install permanent signing, pavement marking, and erosion control.
5. Remove road closure signing and open US Highway 212 to traffic.
6. Remove detour signing.

GENERAL TRAFFIC CONTROL

Existing guide, route, informational logo, regulatory, and warning signs will be temporarily reset and maintained during construction. Removing, relocating, covering, salvaging, and resetting of existing traffic control devices, including delineation, will be the responsibility of the Contractor. Cost for this work will be incidental to the contract unit prices for the various items unless otherwise specified in the plans. Any delineators and signs damaged or lost will be replaced by the Contractor at no cost to the State.

All temporary traffic control sign locations will be set in the field by the Contractor and verified by the Engineer prior to installation.

All construction operations will be conducted in the general direction of traffic movement.

If there is a discrepancy between the traffic control plans, standard plates, and the MUTCD, whichever is more stringent will be used, as determined by the Engineer.

Unless otherwise stated in these plans, work will not be allowed during hours of darkness.

Fixed location signing placed more than 4 calendar days prior to the start of construction will be covered or laid down until the time of construction. The

covers must be approved by the Engineer prior to installation. The cost of materials, labor, and equipment necessary to complete this work will be incidental to other contract items. No separate payment will be made.

All fixed location signs, sign posts, and breakaway bases will be removed within 7 calendar days following pavement marking.

All haul trucks will be equipped with an additional flashing amber light that is visible from the backside of the haul truck. The costs for the flashing amber lights will be incidental to the various related contract items.

Existing STOP signs that are temporarily removed must be reset prior to the end of each day's work. A STOP sign on portable supports must be used whenever a permanent ground mounted STOP sign is removed. Cost for this work will be incidental to the contract unit price per square foot for Traffic Control Signs.

The Contractor will maintain access to all farms, homes, business and other used entrances along the project. A minimum 10 foot wide path free of obstructions will be maintained for all accesses.

The Contractor will maintain access to all mailboxes along the project. With Post Office and Engineer approval, mailboxes may temporarily be relocated. There will be no additional payment for temporary relocation of a mailbox.

No more than 3 consecutive intersecting roads will be closed to traffic simultaneously. 400 tons of Base Course has been included in the Section F Estimate of Quantities for providing temporary traffic control maintenance of intersecting roads, business, residence, farm and field entrances.

Landowners of field entrances will also be notified. Written notices will include specific instructions on how access is going to be maintained to those impacted by the closing of an entrance.

If an entrance is closed to traffic, a double sided Type 3 Barricade will be placed on the closed entrance.

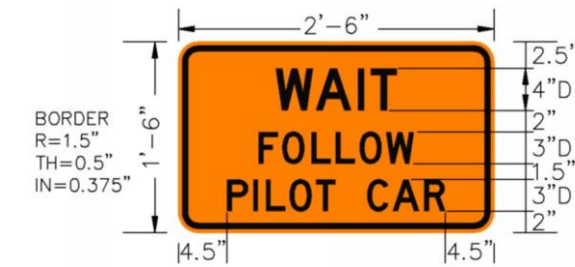
The Contractor will provide the Spink County Sheriff's Office, Redfield Community Memorial Hospital, and the Avera St. Luke's Hospital Emergency Department a detailed map showing roadway segment construction work limits and the detour routing for emergency vehicles. Updated maps will be provided to the departments 24 hours prior to any changes in work limits. Changes in work limits will not be allowed until the 24 hour advance notice requirement has been satisfied.

The Contractor must have the project open during the winter months, with complete access throughout the entire length. In the event the Contractor does not complete the grading portion of this project prior to shutting down operations for the winter, the Contractor will be required to place granular surfacing and asphalt surface treatment on all areas where the in-place surfacing has been removed to satisfy Section 4.5 B of the Specifications. Granular Surfacing will be Base Course or Base Course, Salvaged and will be placed to the thickness of six inches, or as otherwise directed by the Engineer. The Contractor will be responsible for all costs for granular surfacing and asphalt surface treatment. No measurement or payment will be made.

FLAGGING

Operations will be conducted so that the traveling public will not have to wait longer than 15 minutes at the flagger station.

Additional flagger warning signs and flagger hours have been included in the Estimate of Quantities for use on intersecting roads. These flaggers will be used as directed by the Engineer and will be used primarily during daytime hours. Also included in the Estimate of Quantities are WAIT FOLLOW PILOT CAR signs for use on low volume intersecting roads as determined by the Engineer. WAIT FOLLOW PILOT CAR signs will not block the view of the stop sign.



It is required that the flaggers and pilot car operators be able to communicate with one another. If an emergency vehicle needs to pass through the project, the Contractor will be required to expedite traffic movement. All costs associated with this will be incidental to the contract unit price per hour for "Flagging".

PRESS RELEASE ANNOUNCEMENTS

The SDDOT will prepare a press release to be released 5 days prior to any phase change or any other major change that affects traffic flow. The SDDOT will be responsible to keep law enforcement, emergency services, and the traveling public notified of changes in project access. The Contractor will provide the Engineer with pertinent information 7 days prior to any phase change or any other major change that affects traffic flow.

ALTERNATE ROUTE AND DETOUR SIGNING

The Contractor will furnish and install the alternate route and detour signs as shown in these plans. Prior to installing the signs, the Contractor will mark the sign locations and review them with the Engineer. Alternate route and detour signs will be installed on fixed location, ground mounted, breakaway supports. It will be the responsibility of the Contractor to maintain and reinstall these signs during the project as required by the construction progress. Upon completion of the project, the Contractor will remove the alternate route and detour signs.

All costs for furnishing the signs, posts, and mounting hardware, and for installing, maintaining, covering, and removing the alternate route and detour signs will be incidental to the contract unit price per square foot for "Detour and Restriction Signing".

DETOUR MAINTENANCE

Contractor will maintain gravel portion of Car Detour Route with Blade at the discretion of the Engineer and repair blowouts with Base Course.

| | | | |
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| STATE OF SOUTH DAKOTA | PROJECT | SHEET NO. | TOTAL SHEETS |
| | NH 0212 (206) 313 | C3 | C16 |

Plotting Date: 02/21/2024

FISHER GROVE COUNTRY CLUB/STATE PARK CAMPGROUND

The Contractor will be responsible to maintain access at all times for the Fisher Groove Country Club and Fisher Groove State Park Campground located on Fishers Lane.

DUST CONTROL CHLORIDE

Dust Control Chloride will be placed adjacent to the 5 homes along the Car Detour Route. 16,000 Lbs of Dust Control Chloride has been included in the Section C Estimate of Quantities for providing 1000 FT of coverage per home.

PLOT SCALE - 1:1000

PLOT NAME - 1

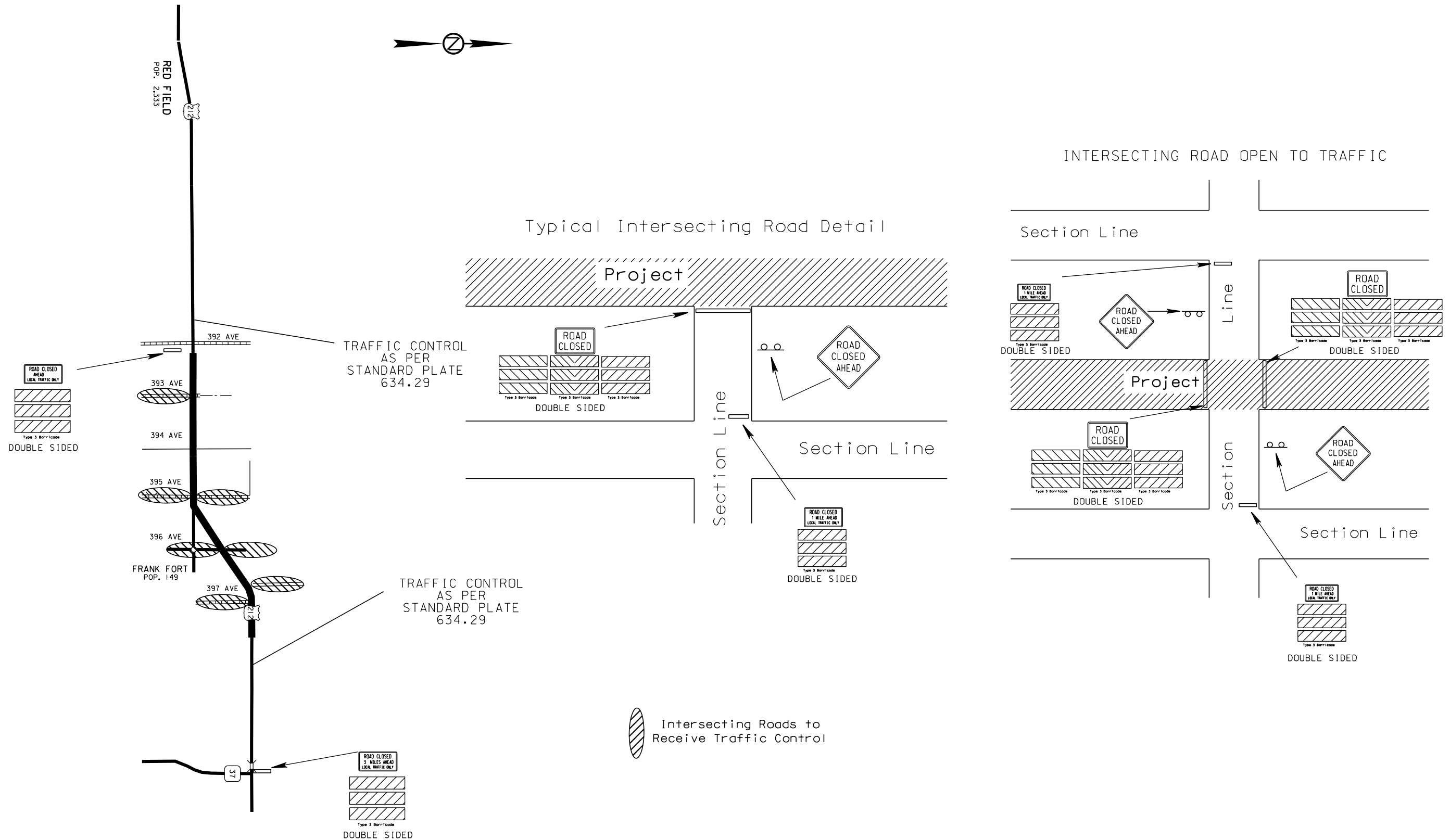
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PLOTTED FROM - TRHJINT04

FIXED LOCATION GROUND MOUNTED, BREAKAWAY SUPPORTS

PLOT SCALE - 1:1500.03

PLOT NAME - 1

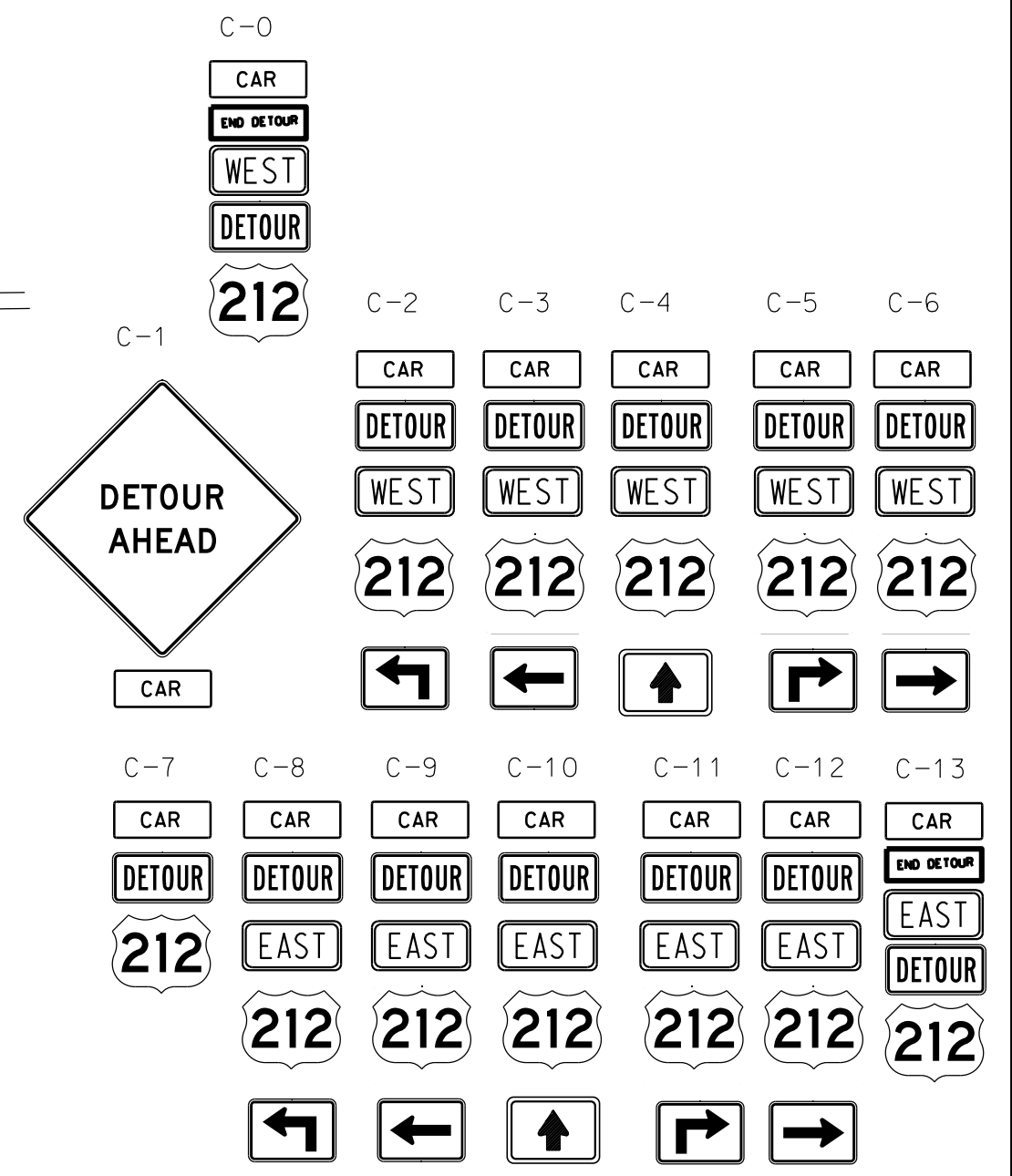
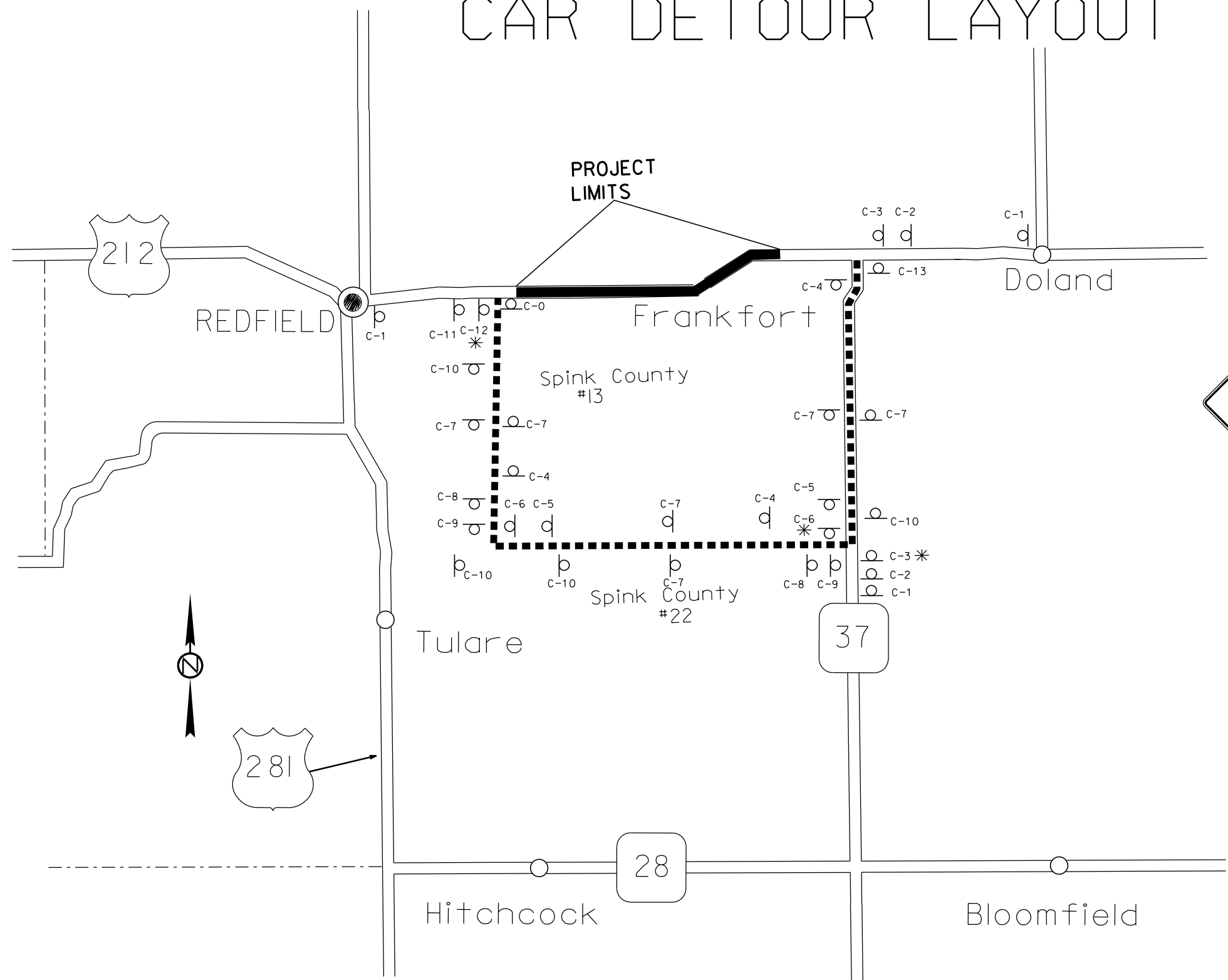


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CAR DETOUR LAYOUT

PLOT SCALE - 1:1500



* Add "No Thru Trucks" Signs to these sign assemblies at the direction of the Engineer.

PLOTTED FROM - TRUJINT04

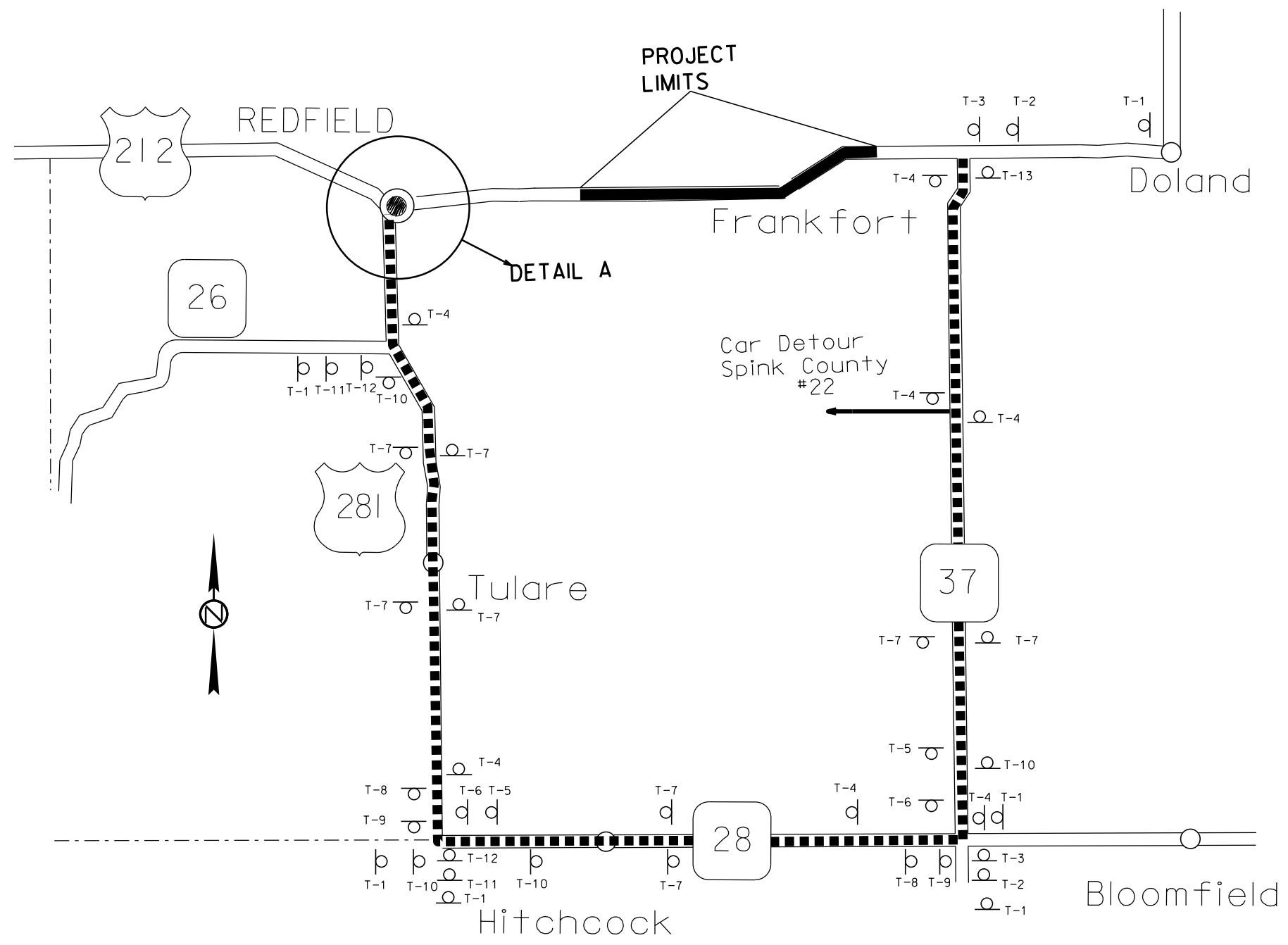
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PLOT SCALE - 1:1500.06

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| STATE OF SOUTH DAKOTA | PROJECT | SHEET NO. | TOTAL SHEETS |
| | NH 0212(206)313 | C6 | C16 |
| Plotting Date: 11/15/2024 | | | |

TRUCK DETOUR LAYOUT



T-0
TRUCK
END DETOUR
WEST
212

T-1
DETOUR AHEAD
TRUCK

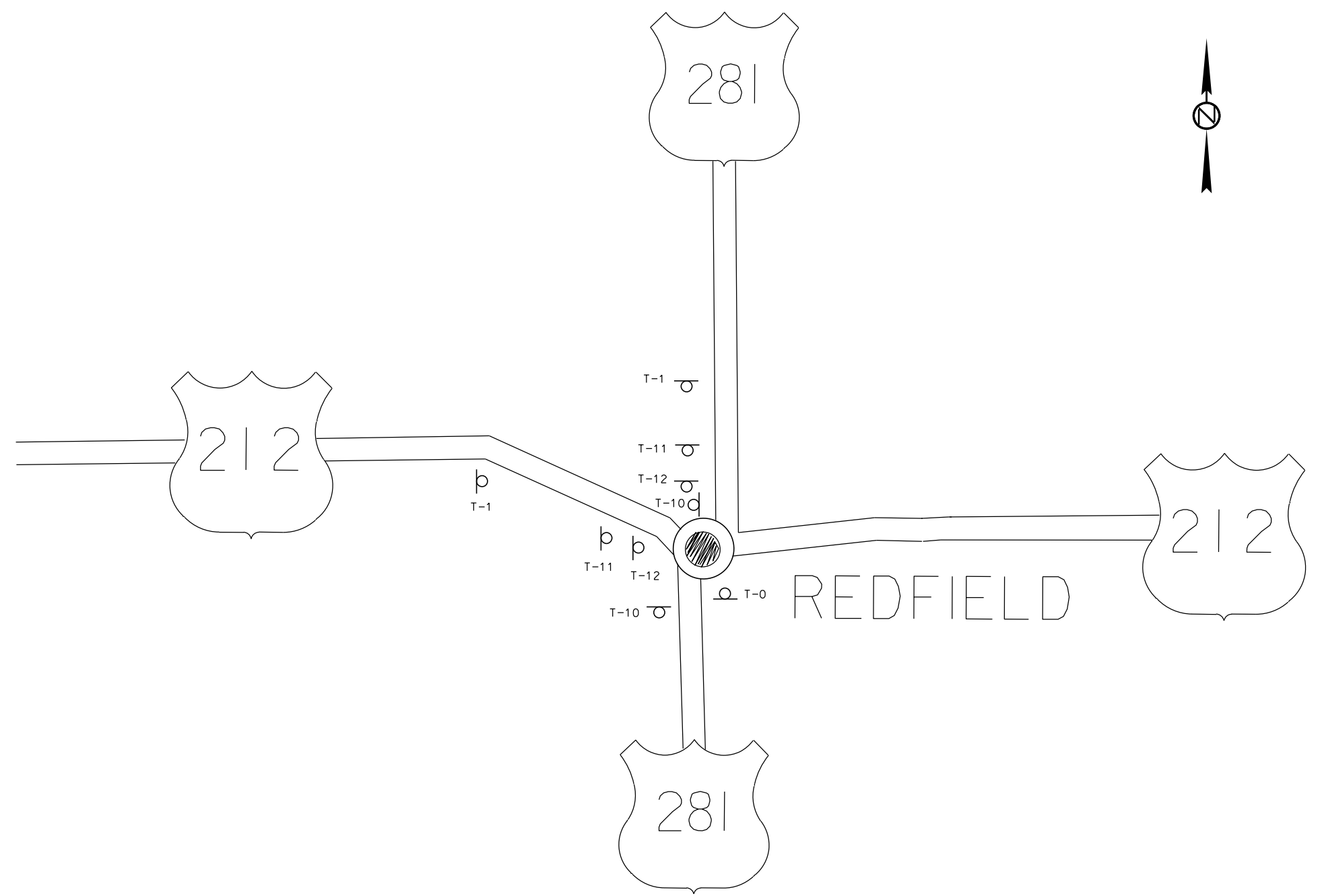
| | | | | |
|--------|--------|--------|--------|--------|
| T-2 | T-3 | T-4 | T-5 | T-6 |
| TRUCK | TRUCK | TRUCK | TRUCK | TRUCK |
| DETOUR | DETOUR | DETOUR | DETOUR | DETOUR |
| WEST | WEST | WEST | WEST | WEST |
| 212 | 212 | 212 | 212 | 212 |
| ↙ | ← | ↑ | ↗ | → |

T-7

| | | | | | | |
|--------|--------|--------|--------|--------|--------|------------|
| T-7 | T-8 | T-9 | T-10 | T-11 | T-12 | T-13 |
| TRUCK | TRUCK | TRUCK | TRUCK | TRUCK | TRUCK | TRUCK |
| DETOUR | DETOUR | DETOUR | DETOUR | DETOUR | DETOUR | END DETOUR |
| 212 | EAST | EAST | EAST | EAST | EAST | EAST |
| 212 | 212 | 212 | 212 | 212 | 212 | 212 |
| ↙ | ← | ↑ | ↗ | → | | |

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TRUCK DETOUR SIGNING LAYOUT - DETAIL A



| | | | | | | | | | | |
|-----|-----|------|------|------|-----|-----|--|--|--|--|
| | | | | | T-1 | | | | | |
| | | | | | | | | | | |
| | | | | | | T-0 | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| T-8 | T-9 | T-10 | T-11 | T-12 | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
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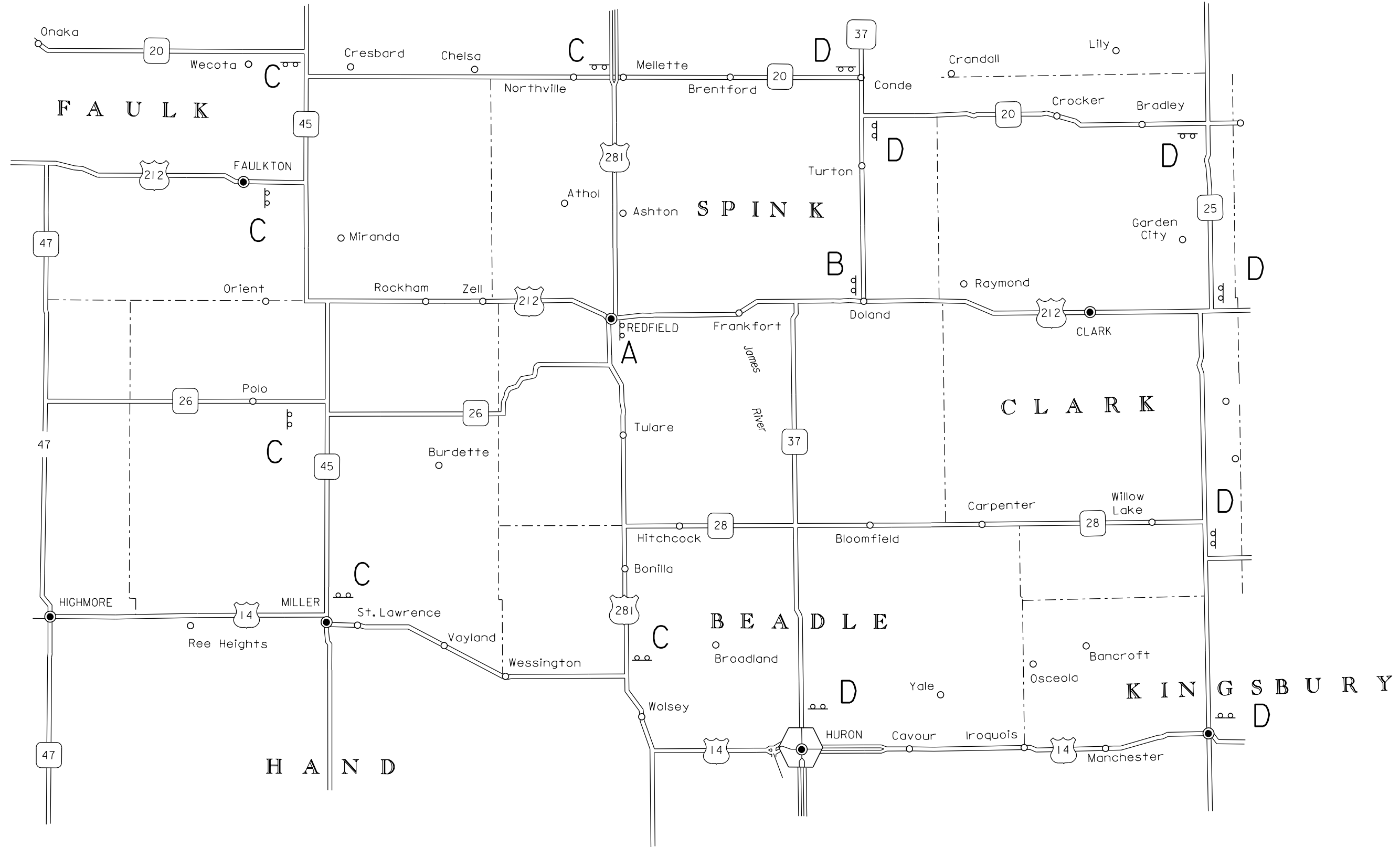
PLOT SCALE - 1:1500.06

PLOTTED FROM - TRUJINT04

PLOT NAME - 1
FILE - ... \TRAFFIC CONTROL DESIGNS 0808.DGN

ADVANCE DETOUR SIGN LAYOUT

FIXED LOCATION GROUND MOUNTED,
BREAKAWAY SUPPORTS



PLOT SCALE - 1:1500.01

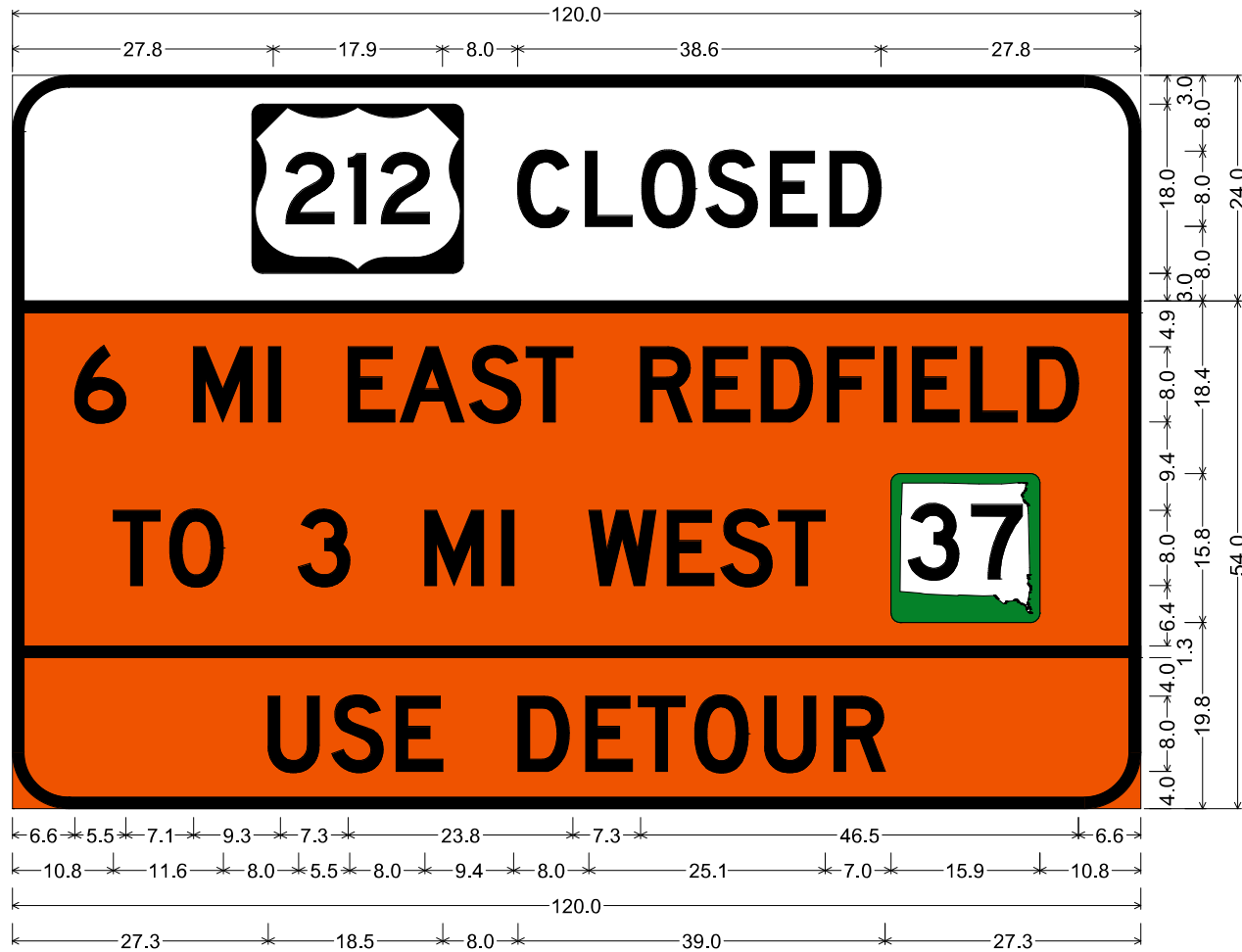
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PLOT NAME - 1
FILE - ... \TRAFFIC CONTROL DESIGNS 0808.DGN

Revised PAR 1/13/25

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| STATE OF SOUTH DAKOTA | PROJECT | SHEET NO. | TOTAL SHEETS |
| | NH 0212(206)313 | C9 | C16 |
| Plotting Date: 02/21/2024 | | | |

A



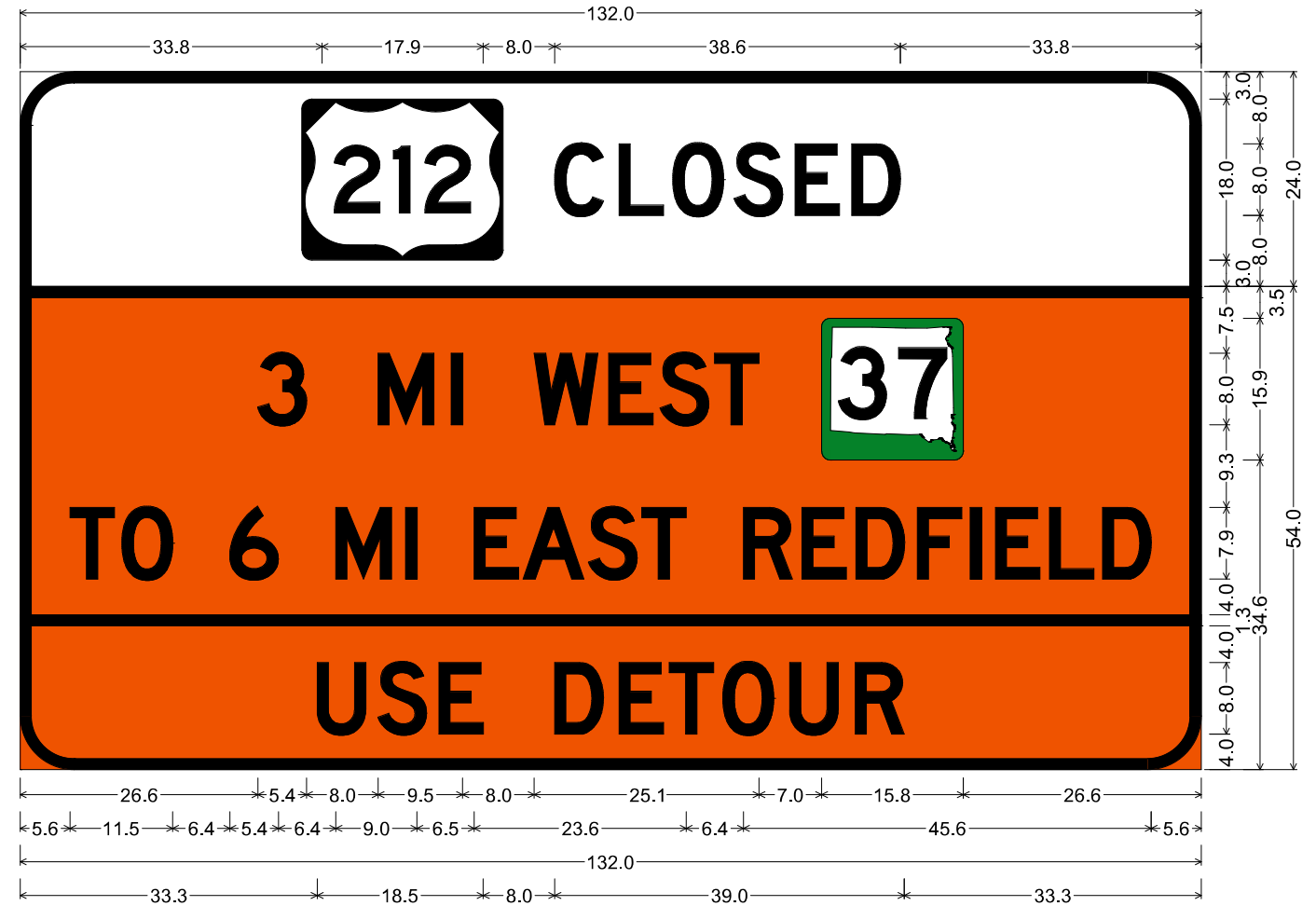
6.0" Radius, 1.3" Border, Black on White;
US Independent 212 M1-4; "CLOSED", D 2K;

6.0" Radius, 1.3" Border, Black on Orange;
"6 MI EAST REDFIELD", D 2K 90% spacing; "TO 3 MI WEST", D 2K; Rounded Rectangle 1.0" Radius Green;
"USE DETOUR", D 2K;

Table of letter and object lefts

| | | | | | | | | | | | | | | |
|------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| 212 | C | L | O | S | E | D | | | | | | | | |
| 27.8 | 53.8 | 60.9 | 66.9 | 73.8 | 80.6 | 86.9 | | | | | | | | |
| 6 | M | I | E | A | S | T | R | E | D | F | I | E | L | D |
| 6.6 | 19.3 | 27.3 | 35.8 | 41.3 | 48.6 | 54.6 | 66.8 | 73.5 | 79.6 | 86.6 | 92.8 | 95.8 | 101.9 | 108.0 |
| T | O | 3 | M | I | W | E | S | T | ■ | | | | | |
| 10.8 | 16.8 | 30.4 | 43.9 | 52.0 | 61.3 | 69.6 | 75.4 | 81.5 | 93.4 | | | | | |
| — | | | | | | | | | | | | | | |
| -0.0 | | | | | | | | | | | | | | |
| U | S | E | D | E | T | O | U | R | | | | | | |
| 27.3 | 34.0 | 40.8 | 53.8 | 61.0 | 66.5 | 72.5 | 80.0 | 87.4 | | | | | | |

B



6.0" Radius, 1.3" Border, Black on White;
US Independent 212 M1-4; "CLOSED", D 2K;

6.0" Radius, 1.3" Border, Black on Orange;
"3 MI WEST", D 2K; Rounded Rectangle 1.0" Radius Green;
"TO 6 MI EAST REDFIELD", D 2K 80% spacing; "USE DETOUR", D 2K;

Table of letter and object lefts

| | | | | | | | | | | | | | | | | |
|------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|
| 212 | C | L | O | S | E | D | | | | | | | | | | |
| 33.8 | 59.8 | 66.9 | 72.9 | 79.8 | 86.6 | 92.9 | | | | | | | | | | |
| 3 | M | I | W | E | S | T | ■ | | | | | | | | | |
| 26.6 | 40.0 | 48.1 | 57.5 | 65.8 | 71.5 | 77.6 | 89.6 | | | | | | | | | |
| T | O | 6 | M | I | E | A | S | T | R | E | D | F | I | E | L | D |
| 5.6 | 11.4 | 23.5 | 35.3 | 43.0 | 50.8 | 56.3 | 63.5 | 69.5 | 80.9 | 87.4 | 93.4 | 100.3 | 106.1 | 109.0 | 115.0 | 121.0 |
| — | | | | | | | | | | | | | | | | |
| -0.0 | | | | | | | | | | | | | | | | |
| U | S | E | D | E | T | O | U | R | | | | | | | | |
| 33.3 | 40.0 | 46.8 | 59.8 | 67.0 | 72.5 | 78.5 | 86.0 | 93.4 | | | | | | | | |

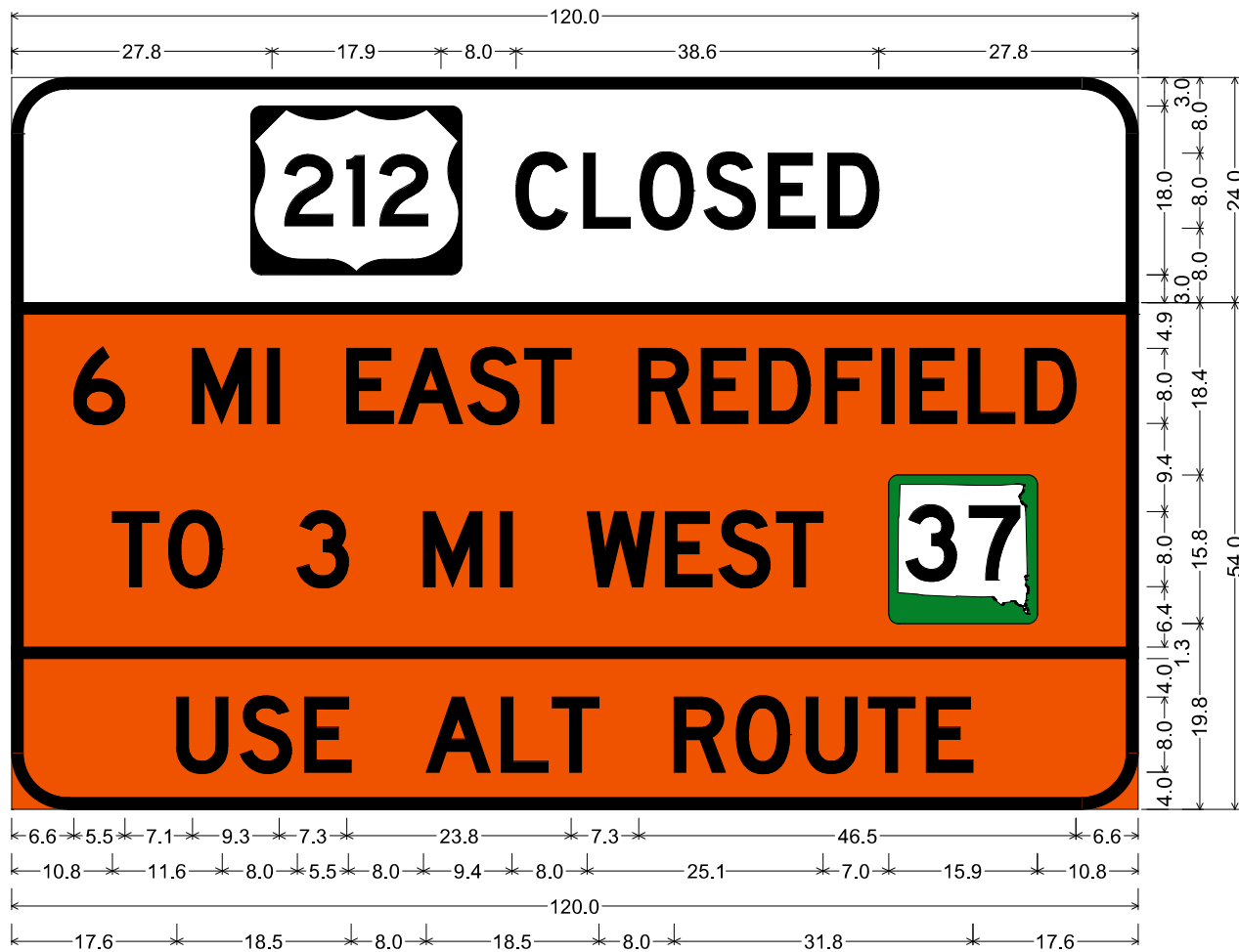
PLOT SCALE - 1:1000

PLOTTED FROM - TRHJINT04

PLOT NAME - 1

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C



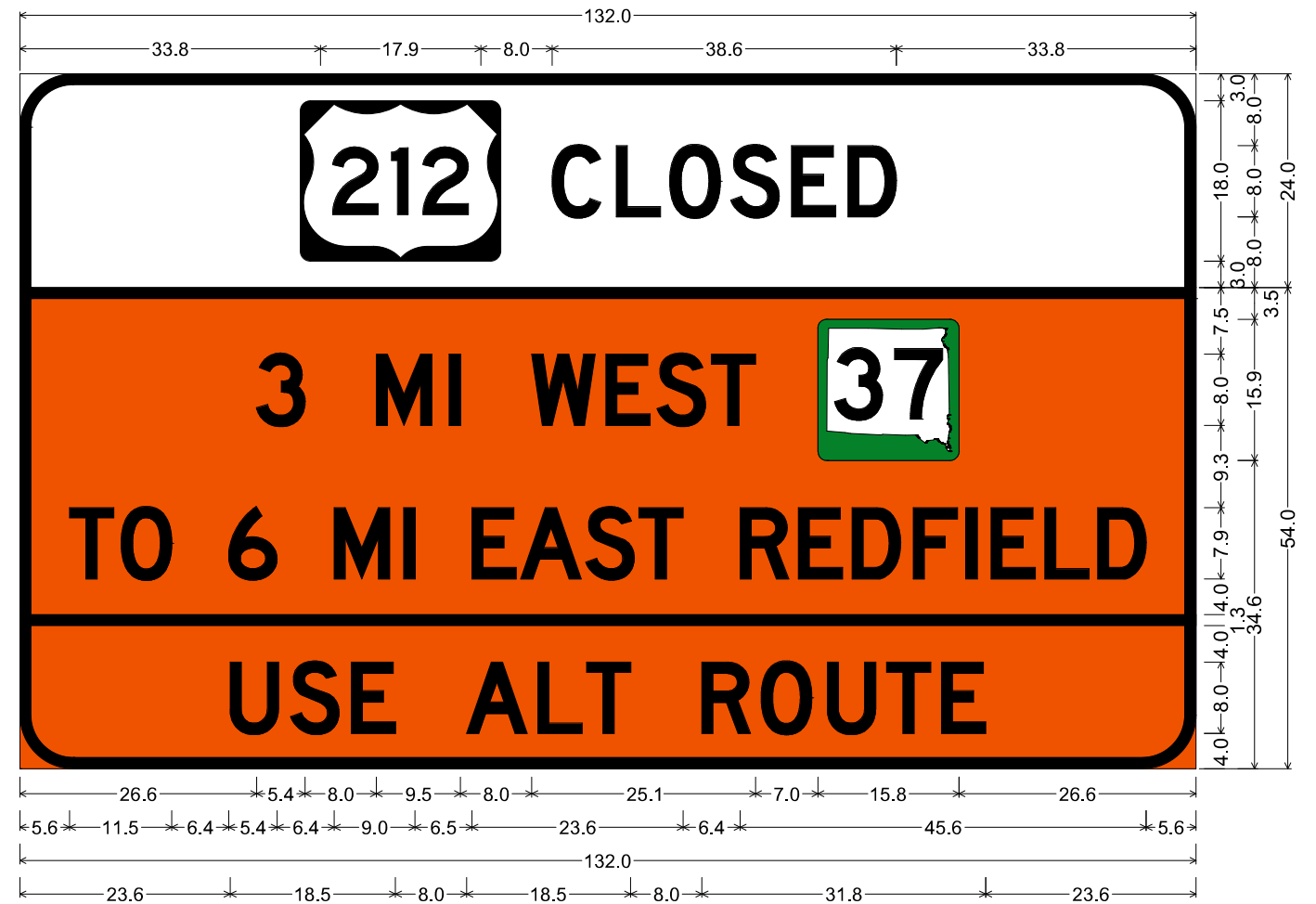
6.0" Radius, 1.3" Border, Black on White;
US Independent 212 M1-4; "CLOSED", D 2K;

6.0" Radius, 1.3" Border, Black on Orange;
"6 MI EAST REDFIELD", D 2K 90% spacing; "TO 3 MI WEST", D 2K; Rounded Rectangle 1.0" Radius Green;
"USE ALT ROUTE", D 2K;

Table of letter and object lefts

| | | | | | | | | | | | | | | |
|------|------|------|------|------|------|------|------|------|------|------|------|------|-------|-------|
| 27.8 | 53.8 | 60.9 | 66.9 | 73.8 | 80.6 | 86.9 | | | | | | | | |
| 6.6 | 19.3 | 27.3 | 35.8 | 41.3 | 48.6 | 54.6 | 66.8 | 73.5 | 79.6 | 86.6 | 92.8 | 95.8 | 101.9 | 108.0 |
| 10.8 | 16.8 | 30.4 | 43.9 | 52.0 | 61.3 | 69.6 | 75.4 | 81.5 | 93.4 | | | | | |
| -0.0 | | | | | | | | | | | | | | |
| 17.6 | 24.4 | 31.3 | 44.1 | 52.1 | 57.6 | 70.5 | 77.3 | 84.6 | 91.3 | 97.5 | | | | |

D



6.0" Radius, 1.3" Border, Black on White;
US Independent 212 M1-4; "CLOSED", D 2K;

6.0" Radius, 1.3" Border, Black on Orange;
"3 MI WEST", D 2K; Rounded Rectangle 1.0" Radius Green;
"TO 6 MI EAST REDFIELD", D 2K 80% spacing; "USE ALT ROUTE", D 2K;

Table of letter and object lefts

| | | | | | | | | | | | | | | | | |
|------|------|------|------|------|------|------|------|------|------|-------|------|-------|-------|-------|-------|-------|
| 33.8 | 59.8 | 66.9 | 72.9 | 79.8 | 86.6 | 92.9 | | | | | | | | | | |
| 26.6 | 40.0 | 48.1 | 57.5 | 65.8 | 71.5 | 77.6 | 89.6 | | | | | | | | | |
| 5.6 | 11.4 | 23.5 | 35.3 | 43.0 | 50.8 | 56.3 | 63.5 | 69.5 | 80.9 | 87.4 | 93.4 | 100.3 | 106.1 | 109.0 | 115.0 | 121.0 |
| -0.0 | | | | | | | | | | | | | | | | |
| 23.6 | 30.4 | 37.3 | 50.1 | 58.1 | 63.6 | 76.5 | 83.3 | 90.6 | 97.3 | 103.4 | | | | | | |

ITEMIZED LIST FOR TRAFFIC CONTROL SIGNS

| SIGN CODE | SIGN DESCRIPTION | CONVENTIONAL ROAD | | | |
|-----------|---|---|-----------|---------------|--------------|
| | | NUMBER | SIGN SIZE | SQFT PER SIGN | SQFT |
| R1-1 | STOP | 7 | 30" | 5.2 | 36.4 |
| R11-2 | ROAD CLOSED | 9 | 48" x 30" | 10.0 | 90.0 |
| R11-3a | ROAD CLOSED <u>1</u> MILES AHEAD LOCAL TRAFFIC ONLY | 9 | 60" x 30" | 12.5 | 112.5 |
| R11-3a | ROAD CLOSED <u>3</u> MILES AHEAD LOCAL TRAFFIC ONLY | 1 | 60" x 30" | 12.5 | 12.5 |
| R11-3a | ROAD CLOSED <u>6</u> MILES AHEAD LOCAL TRAFFIC ONLY | 1 | 60" x 30" | 12.5 | 12.5 |
| W3-1 | STOP AHEAD (symbol) | 2 | 48" x 48" | 16.0 | 32.0 |
| W3-4 | BE PREPARED TO STOP | 2 | 48" x 48" | 16.0 | 32.0 |
| W8-1 | BUMP | 4 | 48" x 48" | 16.0 | 64.0 |
| W8-6 | TRUCK CROSSING | 4 | 48" x 48" | 16.0 | 64.0 |
| W20-1 | ROAD WORK AHEAD | 4 | 48" x 48" | 16.0 | 64.0 |
| W20-2 | DETOUR AHEAD | 2 | 48" x 48" | 16.0 | 32.0 |
| W20-3 | ROAD CLOSED AHEAD | 9 | 48" x 48" | 16.0 | 144.0 |
| W20-3 | ROAD CLOSED 1000 FEET AHEAD | 2 | 48" x 48" | 16.0 | 32.0 |
| W20-3 | ROAD CLOSED 500 FEET AHEAD | 2 | 48" x 48" | 16.0 | 32.0 |
| W20-4 | ONE LANE ROAD AHEAD | 4 | 48" x 48" | 16.0 | 64.0 |
| W20-7 | FLAGGER (symbol) | 2 | 48" x 48" | 16.0 | 32.0 |
| W21-2 | FRESH OIL | 4 | 48" x 48" | 16.0 | 64.0 |
| W21-5 | SHOULDER WORK | 2 | 48" x 48" | 16.0 | 32.0 |
| SPECIAL | WAIT FOLLOW PILOT CAR | 2 | 30" x 18" | 3.8 | 7.6 |
| G20-2 | END ROAD WORK | 2 | 36" x 18" | 4.5 | 9.0 |
| | | CONVENTIONAL ROAD TRAFFIC CONTROL SIGNS SQFT | | | 968.5 |

ITEMIZED LIST FOR DETOUR AND RESTRICTION SIGNING

| SIGN CODE | SIGN DESCRIPTION | CONVENTIONAL ROAD | | | |
|-----------|---|--|------------|---------------|---------------|
| | | NUMBER | SIGN SIZE | SQFT PER SIGN | SQFT |
| W20-2 | DETOUR AHEAD | 11 | 48" x 48" | 16.0 | 176.0 |
| M1-4 | US 212 ROUTE MARKER | 70 | 30" x 24" | 5.0 | 350.0 |
| M3-2 | DIRECTION MARKER - EAST | 30 | 24" x 12" | 2.0 | 60.0 |
| M3-4 | DIRECTION MARKER - WEST | 28 | 24" x 12" | 2.0 | 56.0 |
| M4-4 | TRUCK | 51 | 24" x 12" | 2.0 | 102.0 |
| M4-8 | DETOUR | 70 | 24" x 12" | 2.0 | 140.0 |
| M4-8a | END DETOUR | 2 | 24" x 18" | 3.0 | 6.0 |
| M5-1 | ADVANCE TURN ARROW 90° (L) | 8 | 21" x 15" | 2.2 | 17.6 |
| M5-1 | ADVANCE TURN ARROW 90° (R) | 9 | 21" x 15" | 2.2 | 19.8 |
| M6-1 | DIRECTION ARROW - Horizontal Single Head (L) | 8 | 21" x 15" | 2.2 | 17.6 |
| M6-1 | DIRECTION ARROW - Horizontal Single Head (R) | 9 | 21" x 15" | 2.2 | 19.8 |
| M6-3 | DIRECTION ARROW - Vertical Single Head | 20 | 21" x 15" | 2.2 | 44.0 |
| SPECIAL | CAR | 32 | 24" x 12" | 2.0 | 64.0 |
| C | 212 CLOSED 6 MI E REDFIELD TO 3 MI W 37 USE ALT ROUTE | 6 | 120" x 78" | 65.0 | 390.0 |
| D | 212 CLOSED 3 MI W 37 TO 6 MI E REDFIELD USE ALT ROUTE | 7 | 132" x 78" | 71.5 | 500.5 |
| A | 212 CLOSED 6 MI E REDFIELD TO 3 MI W 37 USE DETOUR | 1 | 120" x 78" | 65.0 | 65.0 |
| B | 212 CLOSED 3 MI W 37 TO 6 MI E REDFIELD USE DETOUR | 1 | 132" x 78" | 71.5 | 71.5 |
| SPECIAL | NO THRU TRUCKS | 3 | 24" x 30" | 5.0 | 15.0 |
| | | CONVENTIONAL ROAD DETOUR AND RESTRICTION SIGNING SQFT | | | 2114.8 |

The signs illustrated are not required if the work space is behind a barrier, more than 2 feet behind the curb, or 15 feet or more from the edge of any roadway.

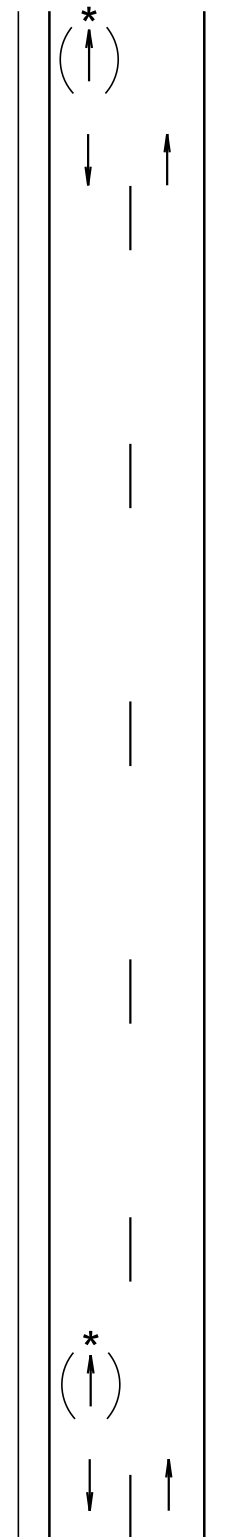
The signs illustrated will be used where there are distracting situations; such as: vehicles parked on shoulder, vehicles accessing the work site via the highway, and equipment traveling on or crossing the roadway to perform work operations.

The ROAD WORK AHEAD sign may be replaced with other appropriate signs, such as the SHOULDER WORK sign. The SHOULDER WORK sign may be used for work adjacent to the shoulder.

* If the work space is on a divided highway, an advance warning sign should also be placed on the left side of the directional roadway.

For short term, short duration, or mobile operations, all signs and channelizing devices may be eliminated if a vehicle with an activated flashing or revolving yellow light is used.

| Posted Speed Prior to Work (M.P.H.) | Spacing of Advance Warning Signs (Feet) (A) |
|-------------------------------------|---|
| 0 - 30 | 200 |
| 35 - 40 | 350 |
| 45 - 50 | 500 |
| 55 | 750 |
| 60 - 80 | 1000 |



January 22, 2021

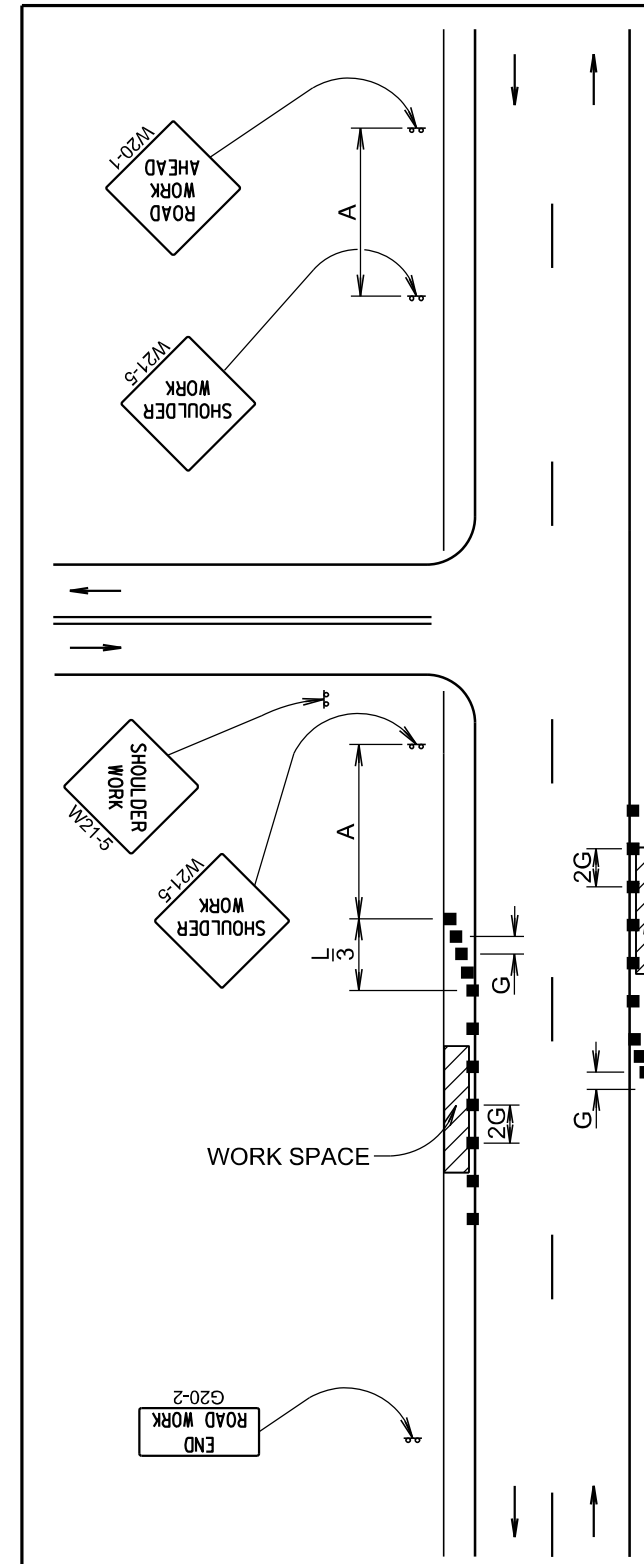
Published Date: 2025

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WORK BEYOND THE SHOULDER

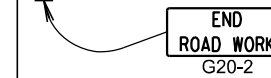
PLATE NUMBER
634.01

Sheet 1 of 1



| Posted Speed Prior to Work (M.P.H.) | Spacing of Advance Warning Signs (Feet) (A) | Taper Length (Feet) (L) | Spacing of Channelizing Devices (Feet) (G) |
|-------------------------------------|---|-------------------------|--|
| 0 - 30 | 200 | 180 | 25 |
| 35 - 40 | 350 | 320 | 25 |
| 45 | 500 | 600 | 25 |
| 50 | 500 | 600 | 50 |
| 55 | 750 | 660 | 50 |
| 60 - 65 | 1000 | 780 | 50 |

■ Channelizing Device



The channelizing devices will be drums or 42" cones if traffic control must remain overnight.

For short duration operations (1 hour or less) all channelizing devices may be eliminated if a vehicle with an activated flashing or revolving yellow light is used.

Worker signs (W21-1 or W21-1a) may be used instead of SHOULDER WORK signs.

A SHOULDER WORK sign should be placed on the left side of a divided or one-way roadway only if the left shoulder is affected.

The SHOULDER WORK sign on an intersecting roadway is not required if drivers emerging from that roadway will encounter another advance warning sign before they reach a work activity area.

WORK SPACE

January 22, 2021

Published Date: 2025

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WORK ON SHOULDERS

PLATE NUMBER
634.03

Sheet 1 of 1

PLOT SCALE - 1:1000

* Messages on signs will vary depending on the operation being conducted.

Vehicle-mounted signs will be mounted in a manner such that they are not obscured by equipment or supplies. Sign legends on vehicle-mounted signs will be covered or turned from view when work is not in progress.

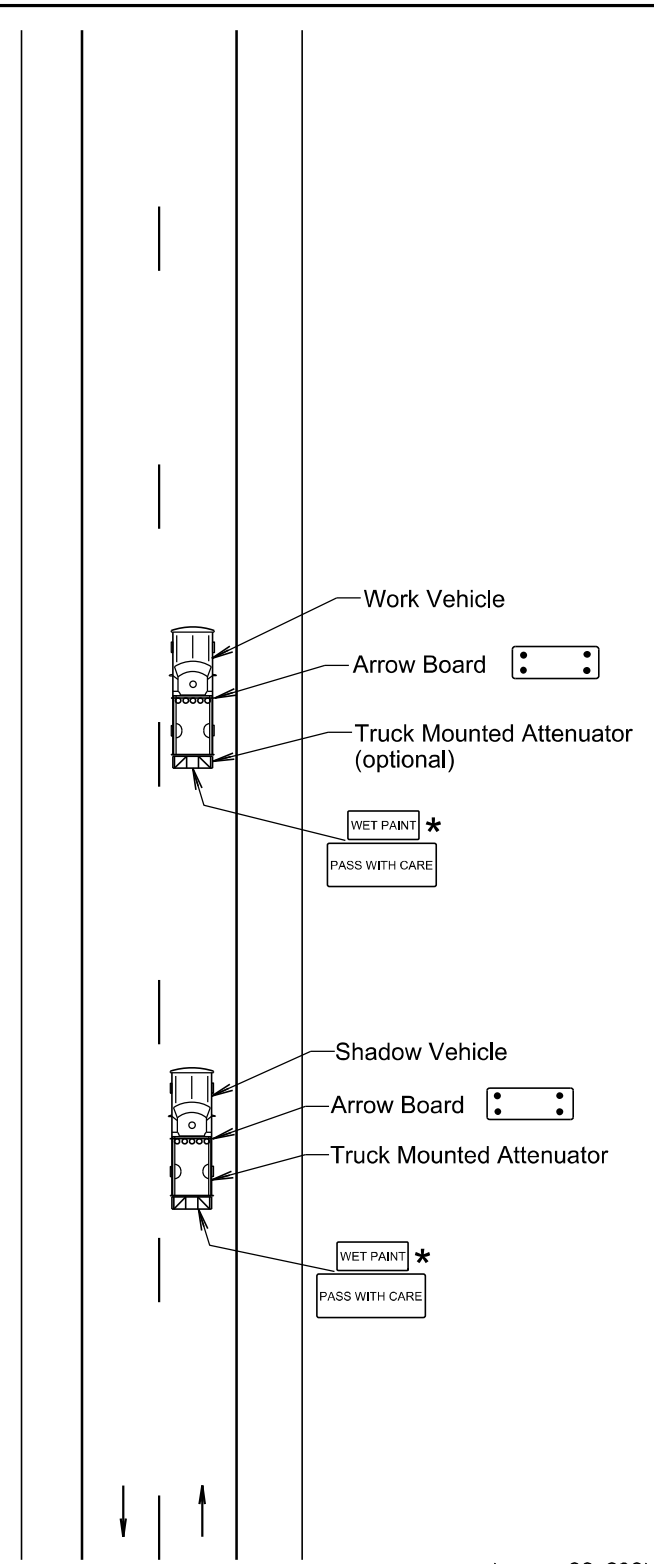
Shadow and Work vehicles will display high-intensity rotating, flashing, oscillating, or strobe lights, flags, signs, or arrow boards.

Vehicle hazard warning signals will not be used instead of the vehicle's high-intensity rotating, flashing, oscillating, or strobe lights.

When an arrow board is used, it will be used in the caution mode. Marching Diamonds are acceptable.

Arrow boards will, as a minimum, be Type B, with a size of 60" x 30".

All costs associated with the traffic control for mobile operation including signs, arrow boards and equipment will be incidental to the contract lump sum price for "Traffic Control, Miscellaneous".



January 22, 2021

| | | | |
|----------------------|-----------------------|----------------------------------|------------------------|
| Published Date: 2025 | S D D O T | MOBILE OPERATIONS ON 2-LANE ROAD | PLATE NUMBER 634.06 |
| | | | Sheet 1 of 1 |

| Posted Speed Prior to Work (M.P.H.) | Spacing of Advance Warning Signs (Feet) (A) | Spacing of Channelizing Devices (Feet) (G) |
|-------------------------------------|---|--|
| 0 - 30 | 200 | 25 |
| 35 - 40 | 350 | 25 |
| 45 | 500 | 25 |
| 50 | 500 | 50 |
| 55 | 750 | 50 |
| 60 - 65 | 1000 | 50 |

● Flagger
■ Channelizing Device

For low-volume traffic situations with short work zones on straight roadways where the flagger is visible to road users approaching from both directions, a single flagger may be used.

The ROAD WORK AHEAD and the END ROAD WORK signs may be omitted for short duration operations (1 hour or less).

For tack and/or flush seal operations, when flaggers are not being used, the FRESH OIL sign (W21-2) will be displayed in advance of the liquid asphalt areas.

Flashing warning lights and/or flags may be used to call attention to the advance warning signs.

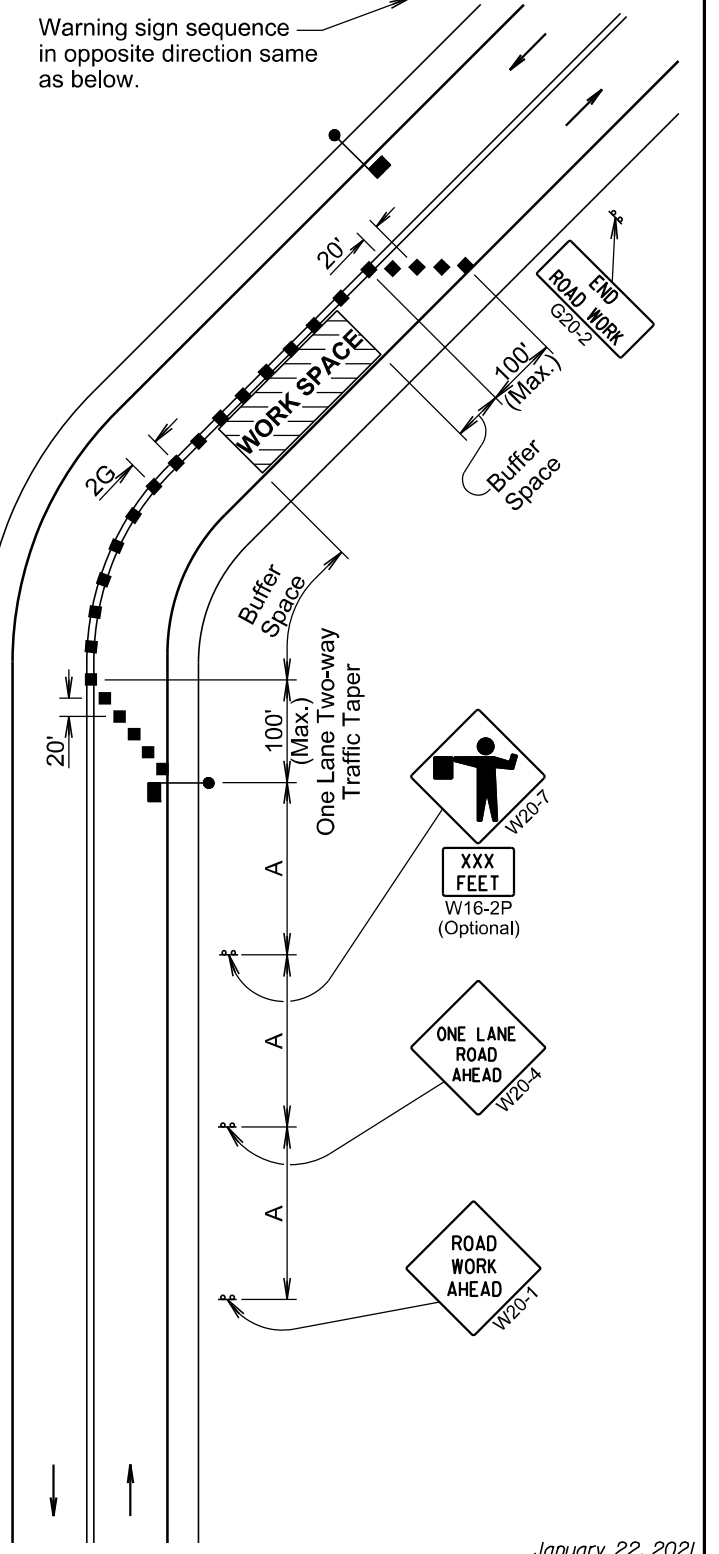
The channelizing devices will be drums or 42" cones.

Channelizing devices are not required along the centerline adjacent to work area when pilot cars are utilized for escorting traffic through the work area.

Channelizing devices and flaggers will be used at intersecting roads to control intersecting road traffic as required.

The buffer space should be extended so that the two-way traffic taper is placed before a horizontal or vertical curve to provide adequate sight distance for the flagger and queue of stopped vehicles.

The length of A may be adjusted to fit field conditions.



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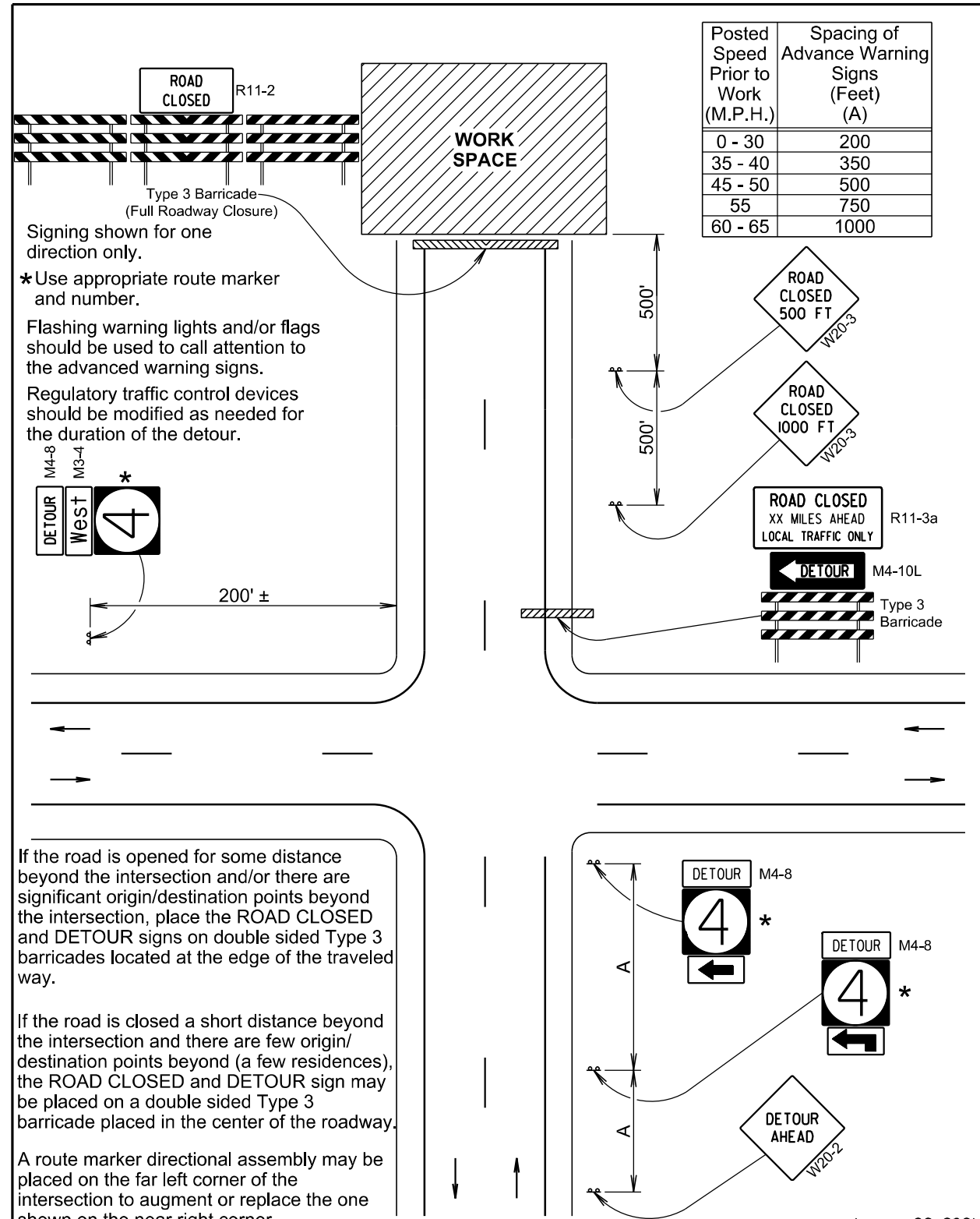
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| Published Date: 2025 | S D D O T | LANE CLOSURE WITH FLAGGER PROVIDED | PLATE NUMBER 634.23 |
| | | | Sheet 1 of 1 |

PLOTTED FROM - TRHJUNT04

PLOT NAME - 1

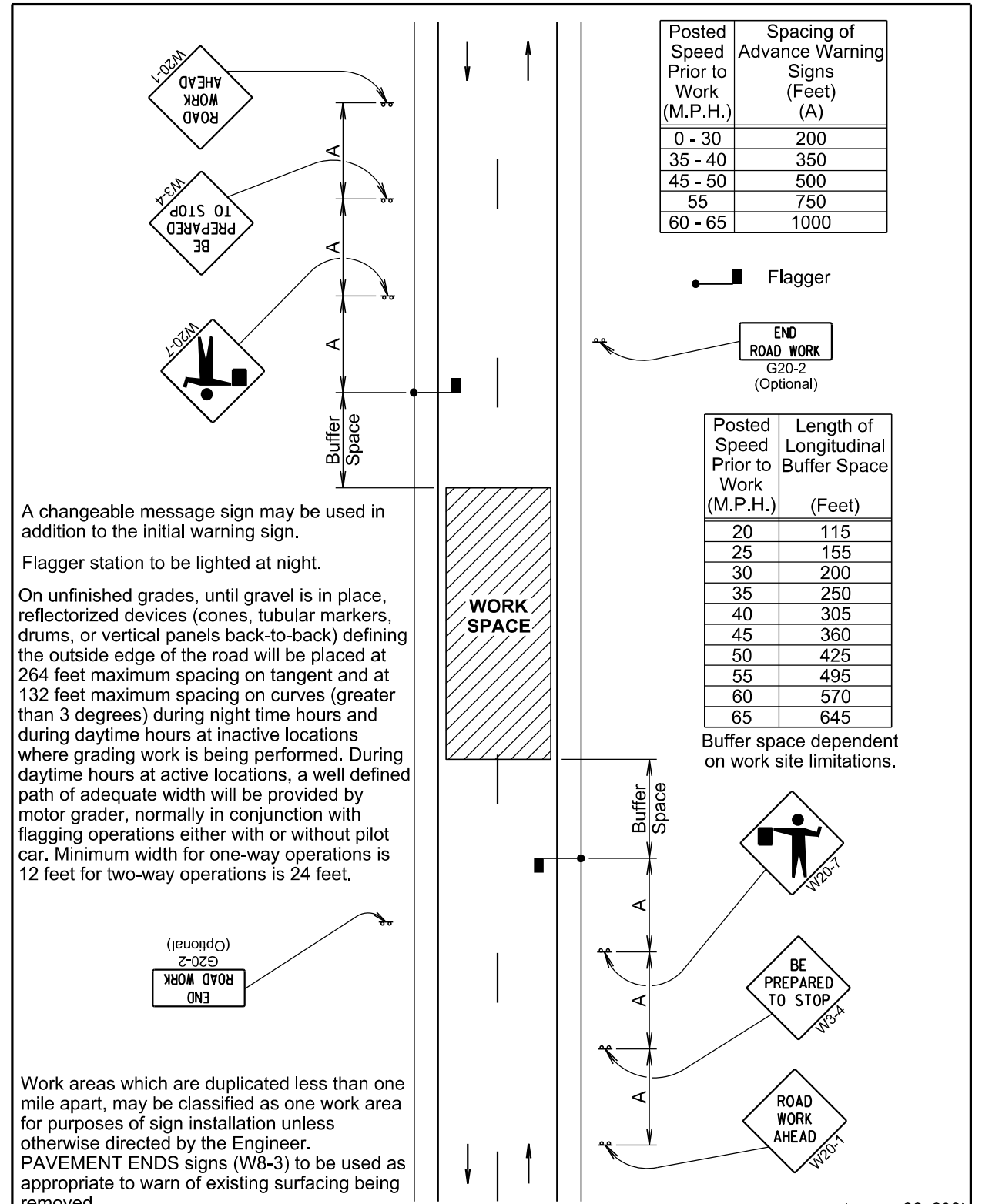
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PLOT SCALE - 1:1000



January 22, 2021

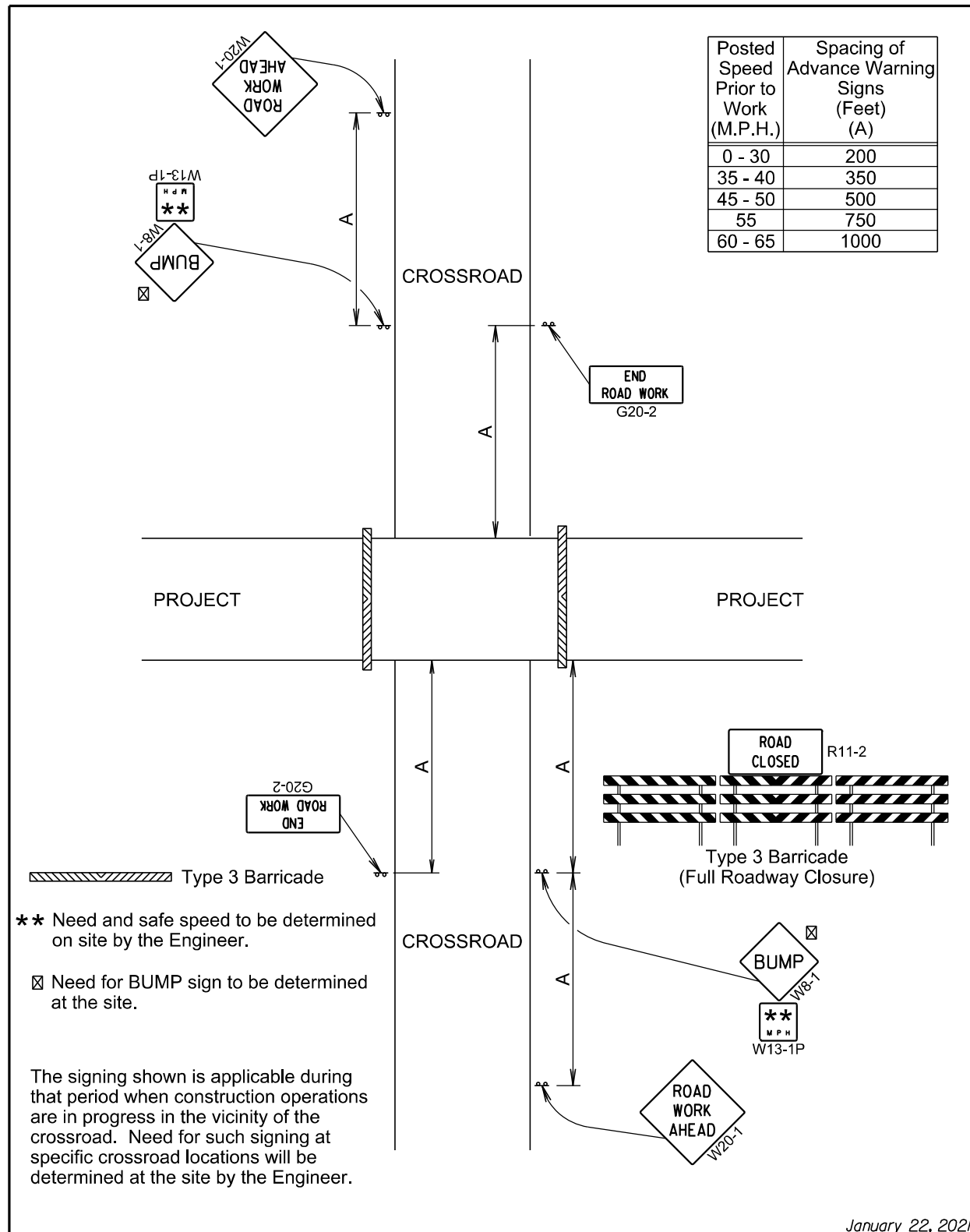
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|----------------------------------|---|-------------------------------|
| S D D O T | ROAD CLOSED WITH OFF-SITE DETOUR | PLATE NUMBER 634.29 |
| | Published Date: 2025 | Sheet 1 of 1 |



January 22, 2021

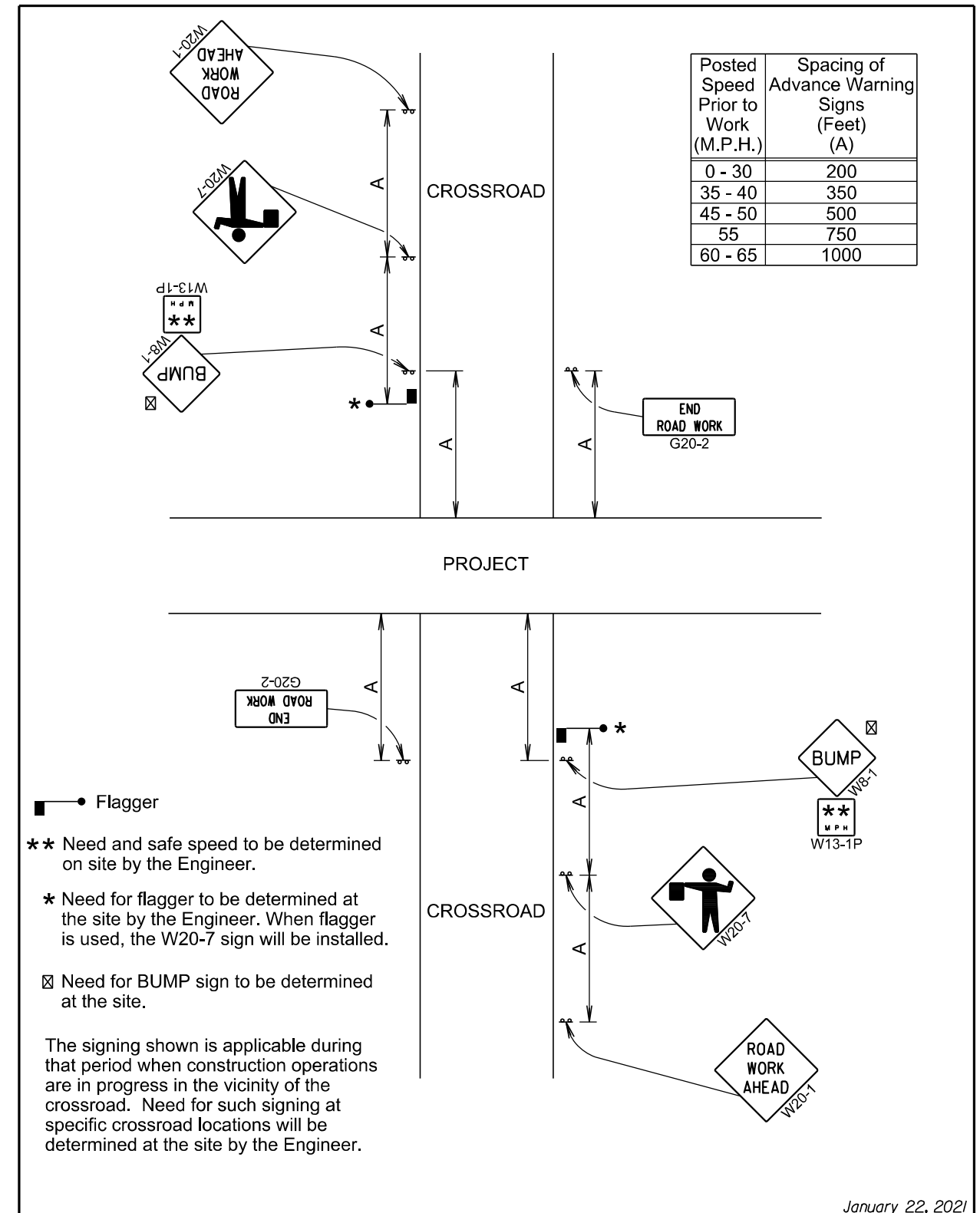
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| S D D O T | LONG TERM ROAD WORK | PLATE NUMBER 634.31 |
| | Published Date: 2025 | Sheet 1 of 1 |

PLOT NAME - 1
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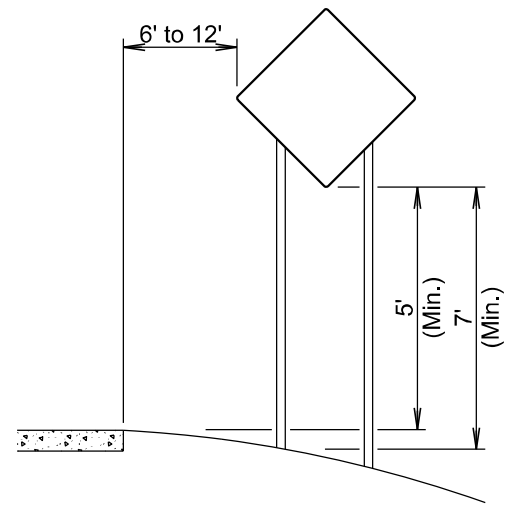
January 22, 2021

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| Published Date: 2025 | S D D O T | PROJECT CLOSED TO TRAFFIC FROM CROSSROAD | PLATE NUMBER 634.37 |
| | | | Sheet 1 of 1 |

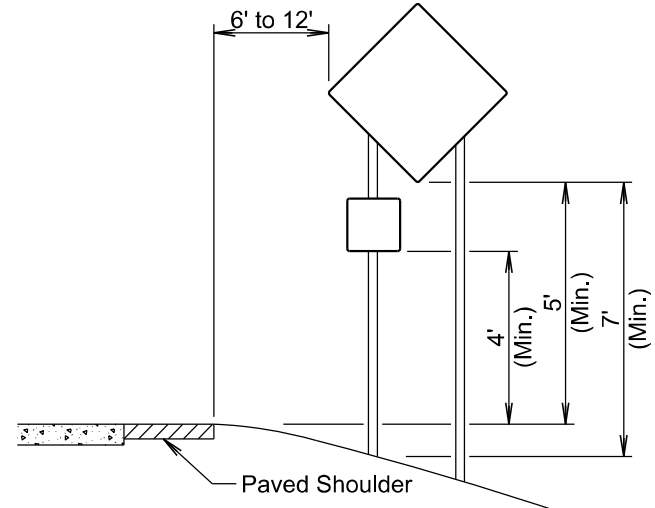


January 22, 2021

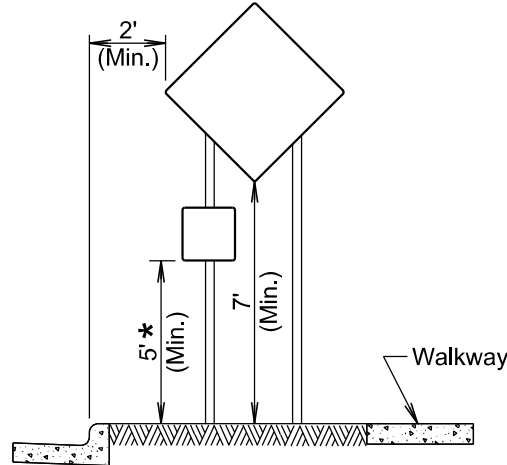
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| Published Date: 2025 | S D D O T | PROJECT OPEN TO TRAFFIC FROM CROSSROAD | PLATE NUMBER 634.38 |
| | | | Sheet 1 of 1 |



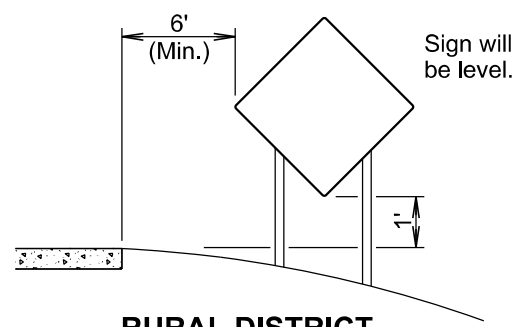
RURAL DISTRICT



RURAL DISTRICT WITH SUPPLEMENTAL PLATE



URBAN DISTRICT

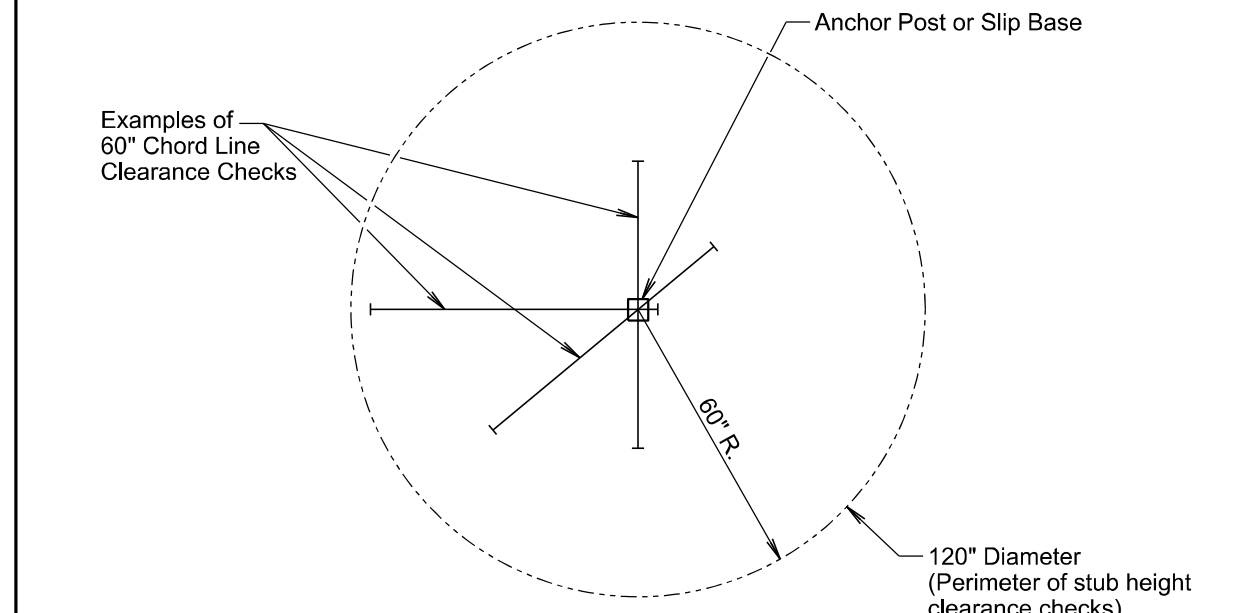


RURAL DISTRICT 3 DAY MAXIMUM
(Not applicable to regulatory signs)

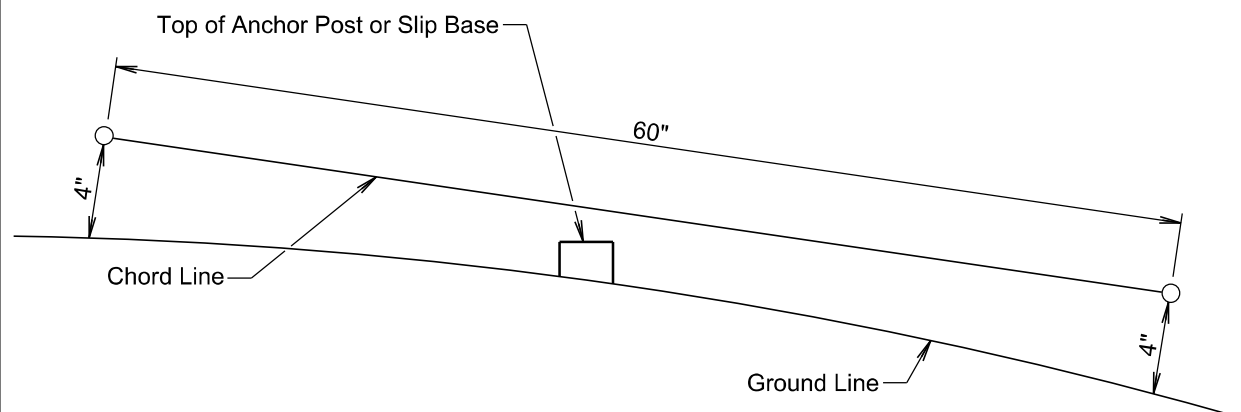
* If the bottom of supplemental plate is mounted lower than 7 feet above a pedestrian walkway, the supplemental plate should not project more than 4" into the pedestrian facility.

January 22, 2021

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| Published Date: 2025 | S D D O T | CRASHWORTHY SIGN SUPPORTS (Typical Construction Signing) | PLATE NUMBER 634.85 |
| | | | Sheet 1 of 1 |



PLAN VIEW
(Examples of stub height clearance checks)



ELEVATION VIEW

GENERAL NOTES:

- The top of anchor posts and slip bases WILL NOT extend above a 60" chord line within a 120" diameter circle around the post with ends 4" above the ground.
- At locations where there is curb and gutter adjacent to the breakaway sign support, the stub height will be a maximum of 4" above the ground line at the localized area adjacent to the breakaway support stub.
- The 4" stub height clearance is not necessary for U-channel lap splices where the support is designed to yield (bend) at the base.

January 22, 2021

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| Published Date: 2025 | S D D O T | BREAKAWAY SUPPORT STUB CLEARANCE | PLATE NUMBER 634.99 |
| | | | Sheet 1 of 1 |

PLOT SCALE - 1:1000

PLOTTED FROM - TRHJUNT04

PLOT NAME - 1

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