


SECTION C: TRAFFIC CONTROL PLANS

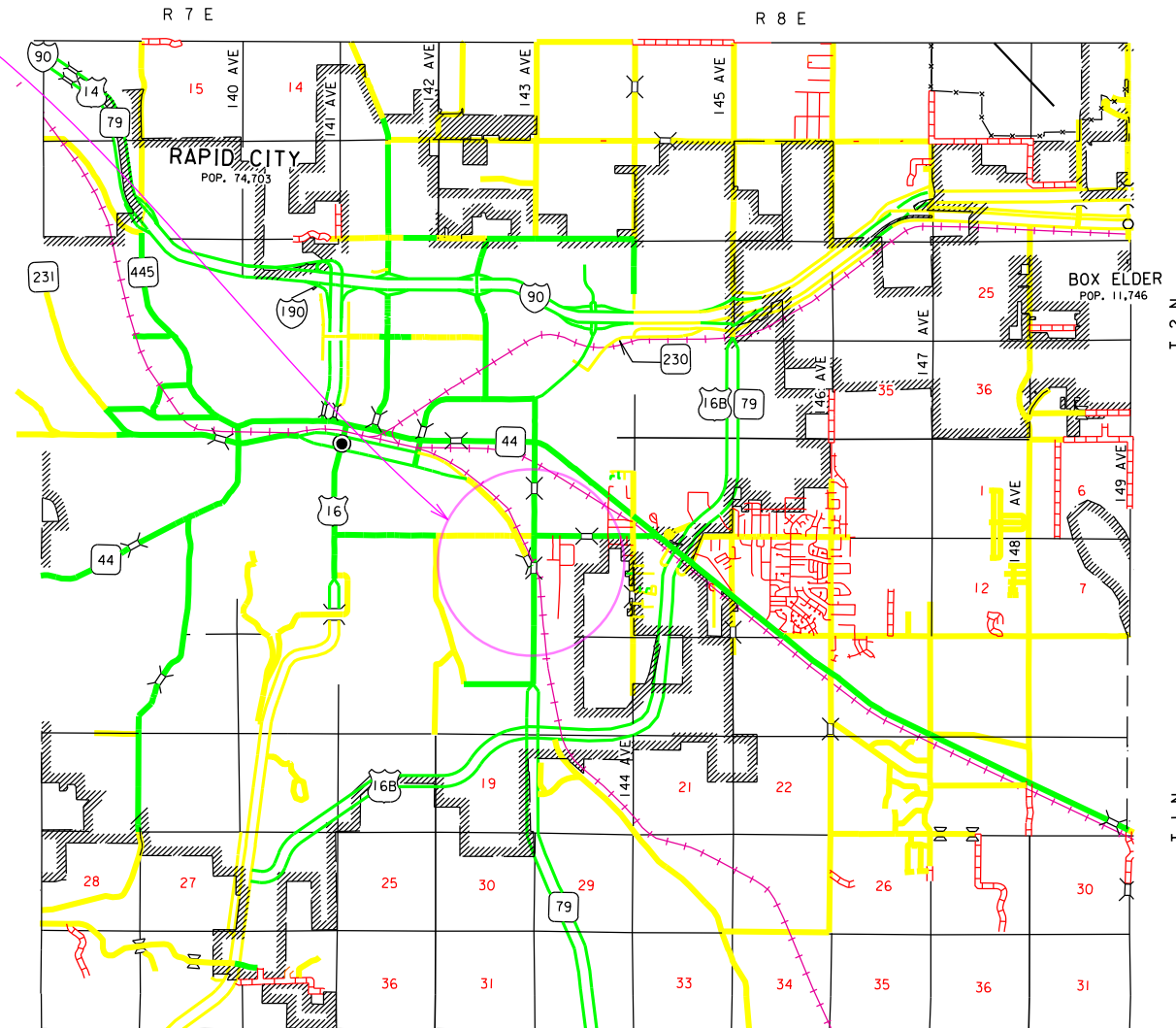
	PROJECT	SECTION	SHEET
	P 79NF(03)69	C	1/4

Plotting Date: 12/9/2025

PROJECT
Str. No. 52-430-314
SD 79 NF - MRM 69.38

INDEX OF SHEETS

- C1 General Layout with Index
- C2 Estimate With General Notes & Tables
- C3-C4 Standard Plates



SECTION C ESTIMATE OF QUANTITIES

BID ITEM NUMBER	ITEM	QUANTITY	UNIT
009E4200	Construction Schedule, Category II	Lump Sum	LS
634E0010	Flagging	200.0	Hour
634E0110	Traffic Control Signs	211.0	SqFt
634E0120	Traffic Control, Miscellaneous	Lump Sum	LS
634E0275	Type 3 Barricade	2	Each
634E0310	Temporary Flexible Vertical Markers (Tabs)	1,840	Ft
634E0420	Type C Advance Warning Arrow Board	2	Each
634E1260	Truck/Trailer Mounted Attenuator	2	Each
998E0100	Railroad Protective Insurance	Lump Sum	LS

SEQUENCE OF OPERATIONS

Standard plate 634.47 and 634.48 will be used to close either the northbound and southbound driving lanes or the northbound and southbound passing lanes of SD-79NF. Closure of a passing lane and a driving lane at the same time will not be permitted.

The RWA sign will be placed north of the intersection of Cambell Street and East Saint Patrick Street along with on East Saint Patrick Street.

The Contractor will submit a sequence of operations for approval two weeks prior to the preconstruction meeting. If changes to the sequence of operations are proposed during the project, these must be submitted for review a minimum of one week prior to potential implementation. Approval for changes to the sequence of operations will only be allowed when the proposed changes meet with the Department's intent for traffic control and sequencing of the work.

GENERAL TRAFFIC CONTROL

Existing guide, route, informational logo, regulatory, and warning signs will be temporarily reset and maintained during construction. Removing, relocating, covering, salvaging, and resetting of existing traffic control devices, including delineation, will be the responsibility of the Contractor. Cost for this work will be incidental to the contract unit prices for the various items unless otherwise specified in the plans. Any delineators and signs damaged or lost will be replaced by the Contractor at no cost to the State.

All temporary traffic control sign locations will be set in the field by the Contractor and verified by the Engineer prior to installation.

If there is a discrepancy between the traffic control plans, standard plates, and the MUTCD, whichever is more stringent will be used, as determined by the Engineer.

Unless otherwise stated in these plans, work will not be allowed during hours of darkness.

Fixed location signing placed more than 4 calendar days prior to the start of construction will be covered or laid down until the time of construction. The covers must be approved by the Engineer prior to installation. The cost of materials, labor, and equipment necessary to complete this work will be incidental to other contract items. No separate payment will be made.

All fixed location signs, signposts, and breakaway bases will be removed within 7 calendar days following pavement marking.

All haul trucks will be equipped with an additional flashing amber light that is visible from the backside of the haul truck. The costs for the flashing amber lights will be incidental to the various related contract items.

At no time will Contractor's equipment and vehicles be left within the work zone over night. Equipment will be parked overnight outside of the clear zone, at least 30 feet from the traveled way. Vehicles without amber flashing lights will not be permitted within the work zone.

WORK ZONE SPEED REDUCTION

The Department is required to obtain a speed reduction resolution prior to the installation of any SPEED LIMIT (R2-1) signs. The speed limit will be lowered to 30 MPH within the work zone. Advanced warning sign W3-5 will not be required for the SBL. To provide adequate time for the resolution to be enacted, the Contractor will inform the Engineer a minimum of 3 weeks prior to the scheduled installation of any work zone speed reduction signs on the project. The information provided by the Contractor will include the anticipated date of sign installation, the newly reduced speed limit, the location of the work zone, and the anticipated completion date of work requiring the speed reduction.

TEMPORARY FLEXIBLE VERTICAL MARKERS (TABS)

Temporary Flexible Vertical Markers (tabs) will be used on tapers spaced at 5' when traffic control must remain overnight. Full reflectivity of all tabs is required at all times. The Contractor will be required to replace any missing or non-reflective tabs after each installation at no additional cost to the State.

Total tabs used will be 1,840' on tapers.

Tabs will be paid for by the foot under the Temporary Flexible Vertical Markers (tabs) bid item.

PRESS RELEASE ANNOUNCEMENTS

The SDDOT will prepare a press release to be released 5 days prior to any phase change or any other major change that affects traffic flow. The SDDOT will be responsible to keep law enforcement, emergency services, and the traveling public notified of changes in project access. The Contractor will provide the Engineer with pertinent information 7 days prior to any phase change or any other major change that affects traffic flow.

TRUCK/TRAILER MOUNTED ATTENUATOR

The Contractor will furnish truck or trailer mounted attenuator(s) to be used for the duration of the project. Truck or trailer mounted attenuators (TMAs) will meet the crashworthy requirements of NCHRP 350 or MASH Test Level 3. TMAs will be used and maintained in accordance with the manufacturers' recommendations.

The TMAs should be utilized on the project where workers and/or equipment are working next to the centerline of the roadway with live traffic in the adjacent lane, or as directed by the Engineer. The TMAs will be removed from the roadway at the end of each working day. The TMAs will remain the property of the Contractor at the end of the project.

The TMAs will be paid for at the contract unit price per each for Truck/Trailer Mounted Attenuator. Payment will be full compensation for furnishing, maintaining, relocating and removing as many times as required by the Engineer and the Contractor's operations.

In the event a TMA is hit while in service, the manufacturer will assess the TMA and make a recommendation as to whether it can be repaired or needs to be

Revised 02/02/26 BP

replaced. The Department will reimburse the Contractor for repairs as documented by invoices or pay for another TMA to be deployed to the project as needed.

TRAFFIC CONTROL SIGNS

ITEMIZED LIST FOR TRAFFIC CONTROL SIGNS

SIGN CODE	SIGN DESCRIPTION	CONVENTIONAL ROAD			
		NUMBER	SIGN SIZE	SQFT PER SIGN	SQFT
R2-1	SPEED LIMIT 30	2	24" x 30"	5.0	10.0
R2-1	SPEED LIMIT 45	2	24" x 30"	5.0	10.0
R2-6aP	FINES DOUBLE (plaque)	2	24" x 18"	3.0	6.0
W3-5	SPEED REDUCTION AHEAD (30 MPH)	1	48" x 48"	16.0	16.0
W4-2	LEFT or RIGHT LANE ENDS (symbol)	2	48" x 48"	16.0	32.0
W20-1	ROAD WORK AHEAD	4	48" x 48"	16.0	64.0
W20-5	LEFT or RIGHT LANE CLOSED AHEAD	2	48" x 48"	16.0	32.0
W20-7	FLAGGER (symbol)	2	48" x 48"	16.0	32.0
G20-2	END ROAD WORK	2	36" x 18"	4.5	9.0
CONVENTIONAL ROAD TRAFFIC CONTROL SIGNS SQFT					211.0

Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)	Taper Length (Feet) (L)	Spacing of Channelizing Devices (Feet) (G)
0 - 30	200	180	25
35 - 40	350	320	25
45	500	600	25
50	500	600	50 *
55	750	660	50 *
60 - 65	1000	780	50 *

* Spacing is 40' for 42" cones.

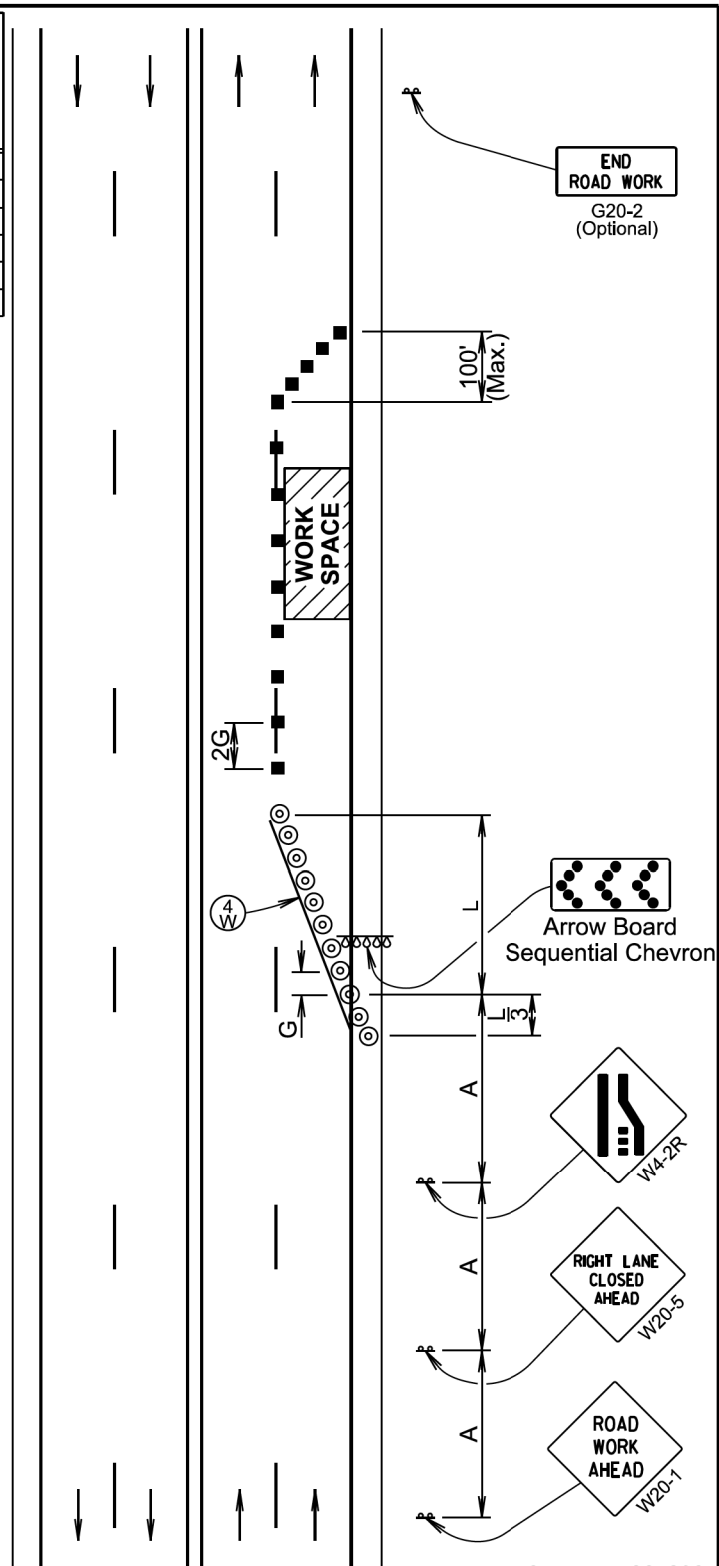
- ⊙ ReflectORIZED Drum
- Channelizing Device
- Ⓞ 4" White Temporary Pavement Marking

The channelizing devices will be 42" cones or drums.

42" cones may be used in place of the drums shown in the taper if setup will not be used during night time hours.

Temporary pavement markings will be used if traffic control must remain overnight.

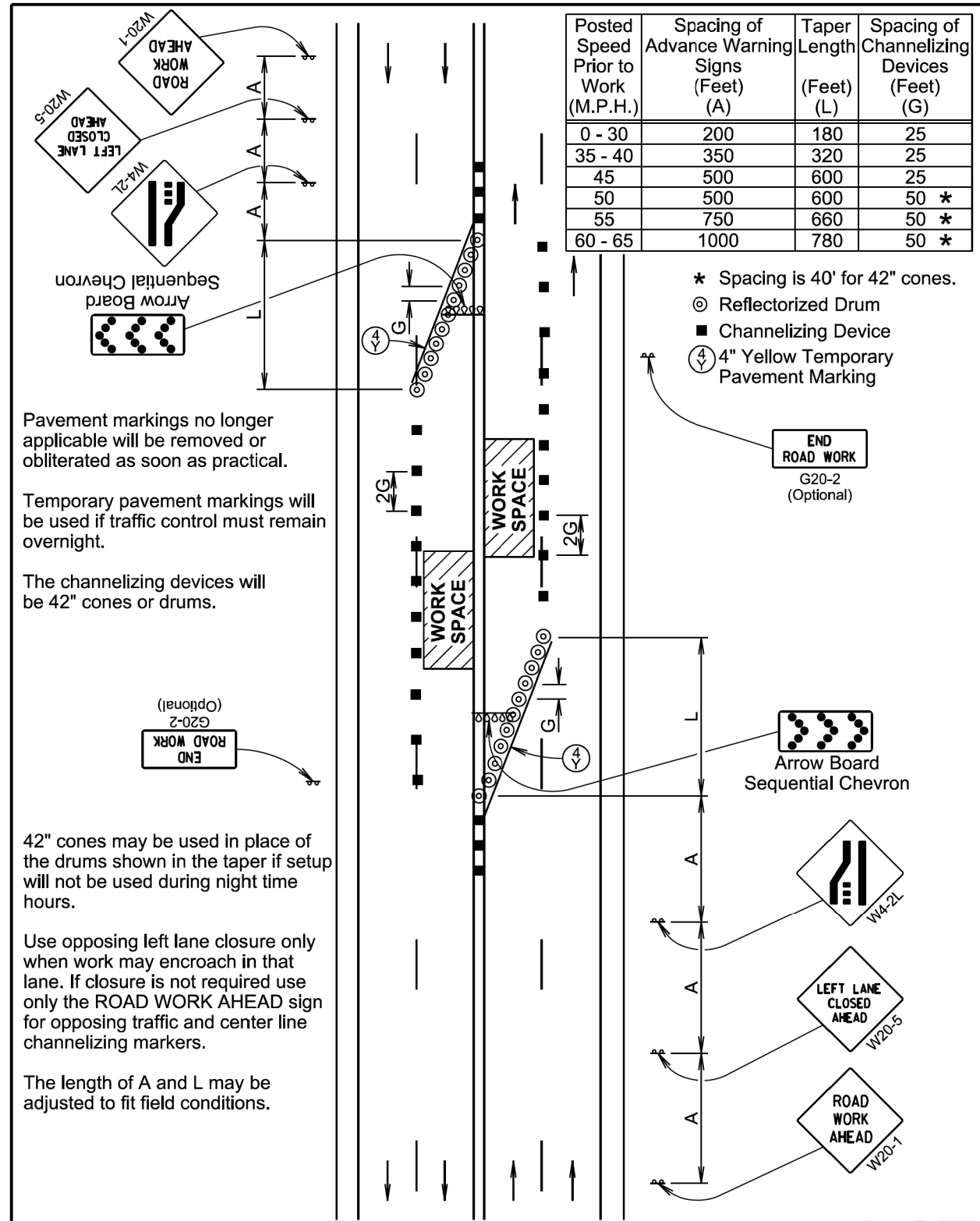
The length of A and L may be adjusted to fit field conditions.



September 22, 2021

SD DOT	4-LANE UNDIVIDED, RIGHT LANE CLOSED	PLATE NUMBER 634.47
		Sheet 1 of 1

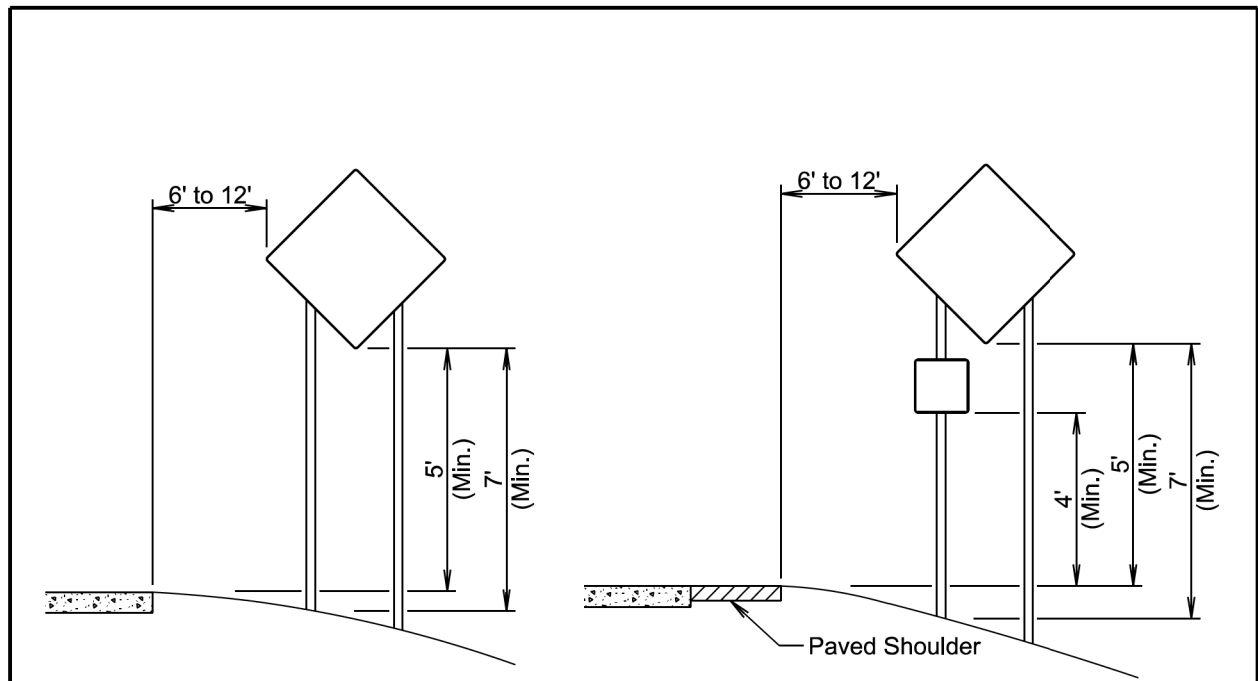
Published Date: 2026



August 31, 2022

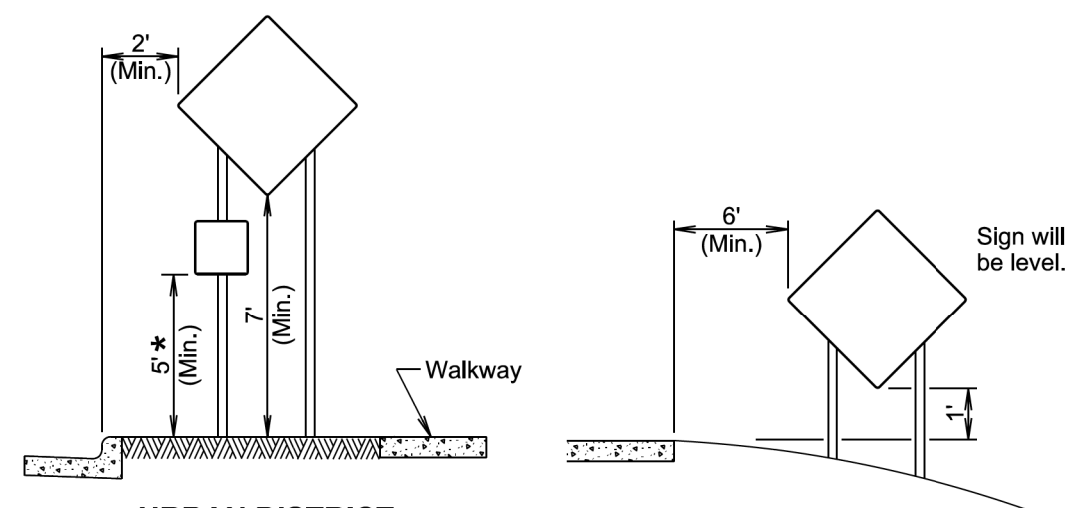
SD DOT	4-LANE UNDIVIDED, LEFT LANE CLOSED	PLATE NUMBER 634.48
		Sheet 1 of 1

Published Date: 2026



RURAL DISTRICT

RURAL DISTRICT WITH SUPPLEMENTAL PLATE



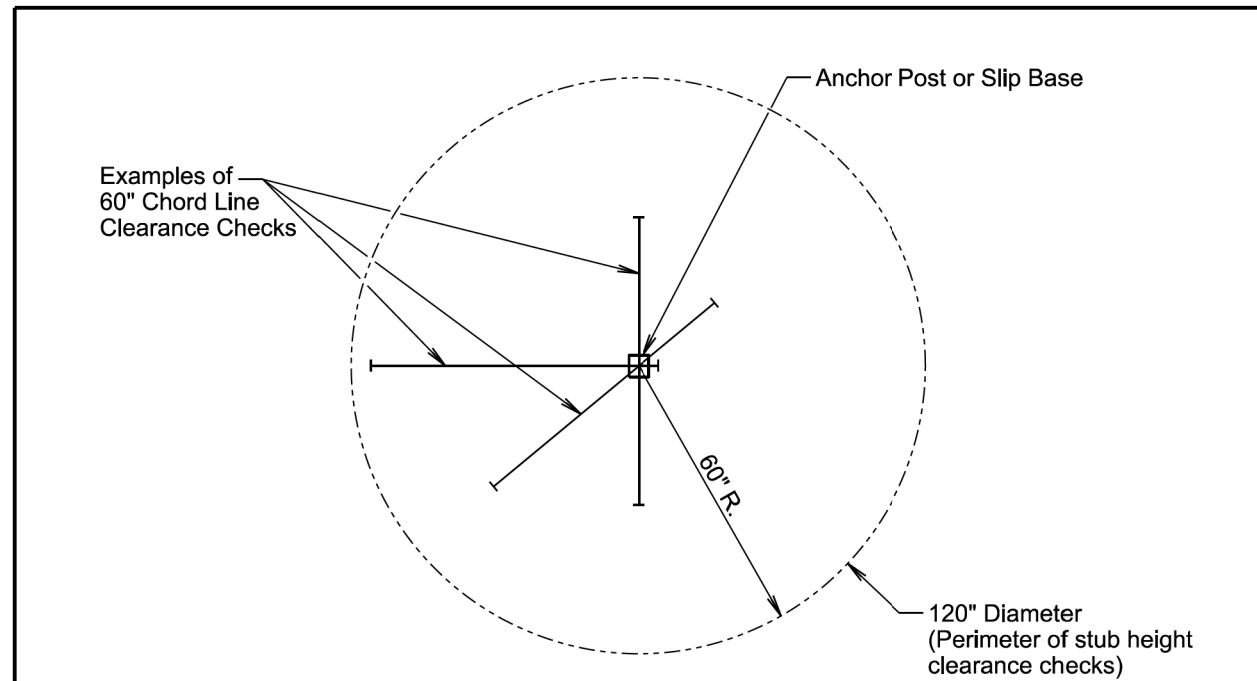
URBAN DISTRICT

RURAL DISTRICT 3 DAY MAXIMUM
(Not applicable to regulatory signs)

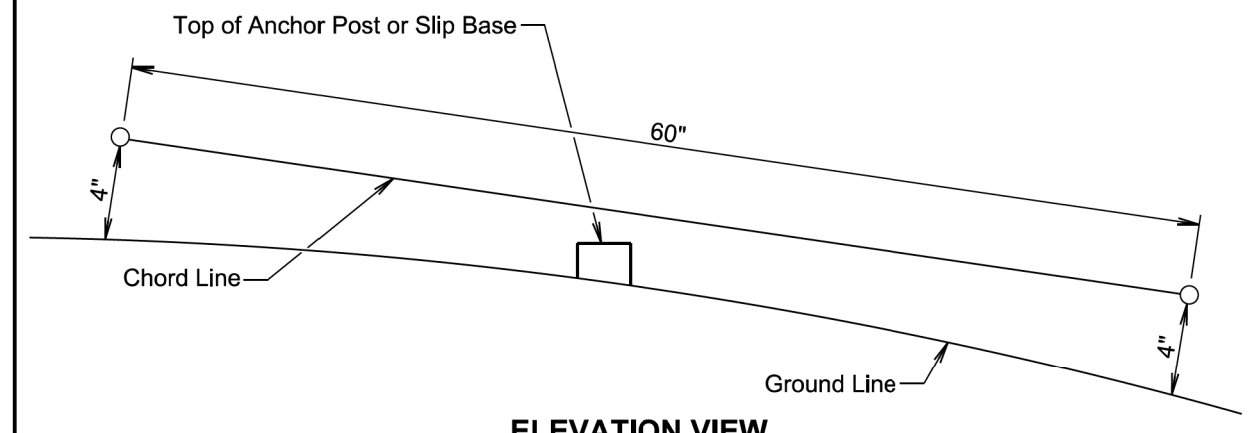
* If the bottom of supplemental plate is mounted lower than 7 feet above a pedestrian walkway, the supplemental plate should not project more than 4" into the pedestrian facility.

January 22, 2021

	CRASHWORTHY SIGN SUPPORTS (Typical Construction Signing)	PLATE NUMBER 634.85
		Sheet 1 of 1
Published Date: 2026		



PLAN VIEW
(Examples of stub height clearance checks)



ELEVATION VIEW

GENERAL NOTES:

- The top of anchor posts and slip bases WILL NOT extend above a 60" chord line within a 120" diameter circle around the post with ends 4" above the ground.
- At locations where there is curb and gutter adjacent to the breakaway sign support, the stub height will be a maximum of 4" above the ground line at the localized area adjacent to the breakaway support stub.
- The 4" stub height clearance is not necessary for U-channel lap splices where the support is designed to yield (bend) at the base.

January 22, 2021

	BREAKAWAY SUPPORT STUB CLEARANCE	PLATE NUMBER 634.99
		Sheet 1 of 1
Published Date: 2026		