

## SECTION C: TRAFFIC CONTROL PLANS



PROJECT

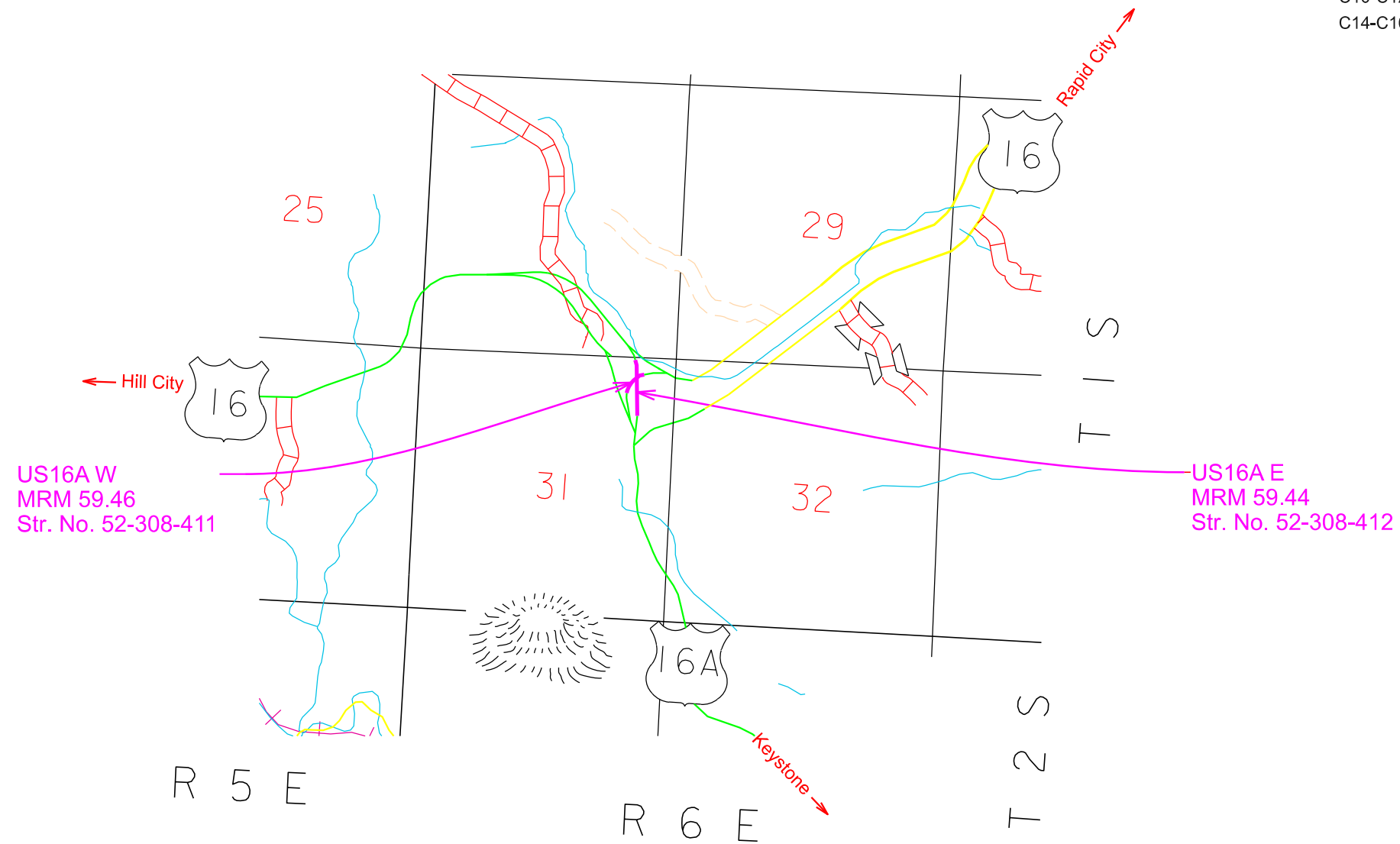
SECTION	SHEET
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P0016(110)59 & 016AW-468

C	C1
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|---------|--|
| C1      | General Layout with Index                |
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| C4-C7   | Traffic Control Details                  |
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SECTION C ESTIMATE OF QUANTITIES

BID ITEM NUMBER	ITEM	QUANTITY	UNIT
009E0010	Mobilization	Lump Sum	LS
110E7020	Remove Interim Crossover Closure for Reset	128	Ft
110E7150	Remove Sign for Reset	5	Each
250E0020	Incidental Work, Grading	Lump Sum	LS
629E9060	Reset Interim Crossover Closure	128	Ft
632E3500	Reset Sign	5	Each
634E0010	Flagging	500.0	Hour
634E0110	Traffic Control Signs	604.4	SqFt
634E0120	Traffic Control, Miscellaneous	Lump Sum	LS
634E0275	Type 3 Barricade	12	Each
634E0310	Temporary Flexible Vertical Markers (Tabs)	1,750	Ft
634E0420	Type C Advance Warning Arrow Board	3	Each
634E0560	Remove Pavement Marking, 4" or Equivalent	2,700	Ft
634E0600	4" Temporary Pavement Marking Tape Type I	270	Ft
634E1002	Detour and Restriction Signing	158.1	SqFt
634E1215	Contractor Furnished Portable Changeable Message Sign	3	Each
634E1255	Contractor Furnished Speed Monitoring Radar Trailer	1	Each

SEQUENCE OF OPERATIONS

The Contractor will submit a sequence of operations for approval two weeks prior to the preconstruction meeting. If changes to the sequence of operations are proposed during the project, these must be submitted for review a minimum of one week prior to potential implementation. Approval for changes to the sequence of operations will only be allowed when the proposed changes meet with the Department’s intent for traffic control and sequencing of the work.

GENERAL TRAFFIC CONTROL

Existing guide, route, informational logo, regulatory, and warning signs will be temporarily reset and maintained during construction. Removing, relocating, covering, salvaging, and resetting of existing traffic control devices, including delineation, will be the responsibility of the Contractor. Cost for this work will be incidental to the contract unit prices for the various items unless otherwise specified in the plans. Any delineators and signs damaged or lost will be replaced by the Contractor at no cost to the State.

All temporary traffic control sign locations will be set in the field by the Contractor and verified by the Engineer prior to installation.

All temporary speed limit signs will have a minimum mounting height of 5 feet in rural locations, even when mounted on portable supports.

If there is a discrepancy between the traffic control plans, standard plates, and the MUTCD, whichever is more stringent will be used, as determined by the Engineer.

Unless otherwise stated in these plans, work will not be allowed during hours of darkness.

Fixed location signing placed more than 4 calendar days prior to the start of construction will be covered or laid down until the time of construction. The covers must be approved by the Engineer prior to installation. The cost of materials, labor, and equipment necessary to complete this work will be incidental to other contract items. No separate payment will be made.

All fixed location signs, sign posts, and breakaway bases will be removed within 7 calendar days following pavement marking.

All haul trucks will be equipped with an additional flashing amber light that is visible from the backside of the haul truck. The costs for the flashing amber lights will be incidental to the various related contract items.

Traffic will be maintained on the driving lanes. Use of the shoulder as a driving lane will not be permitted. Any damage to the shoulder due to rerouted traffic or Contractor’s equipment will be repaired at no expense to the Department.

If inappropriate or conflicting pavement markings exist, the markings will be removed and replaced with applicable temporary pavement markings when the work duration is more than 3 days. When the work duration is less than 3 days, the channelizing devices in the area where the pavement markings conflict will be placed at one-half of the normal channelizing device spacing. Pavement marking removals will be incidental to the contract unit price per foot for “Remove Pavement Marking, 4” or equivalent”. Temporary pavement marking will be paid for at the contract unit price per mile/foot for “4” Temporary Pavement Marking Tape Type I”. The additional channelizing devices will be incidental to the contract lump sum price for “Traffic Control, Miscellaneous”.

A Type 3 Barricade will be installed at the end of a lane closure taper as detailed in these plans. Additional Type 3 Barricades will be installed facing traffic within the closed lane at a spacing of ¼ mile.

DETOUR SIGNING

The Contractor will furnish and install the detour signs as shown in these plans. Prior to installing the signs, the Contractor will mark the sign locations and review them with the Engineer. Detour signs will be installed on fixed location, ground mounted, breakaway supports. It will be the responsibility of the Contractor to maintain and reinstall these signs during the project as required by the construction progress. Upon completion of the project, the Contractor will remove the detour signs.

All costs for furnishing the signs, posts, and mounting hardware, and for installing, maintaining, covering, and removing the detour signs will be incidental to the contract unit price per square foot for “Detour and Restriction Signing”.

FLAGGING

Operations will be conducted so that the traveling public will not have to wait longer than 15 minutes at the flagger station.

It is required that the flaggers and be able to communicate with one another. If an emergency vehicle needs to pass through the project, the Contractor will be required to expedite traffic movement. All costs associated with this will be incidental to the contract unit price per hour for “Flagging”.

WORK ZONE SPEED REDUCTION

The Department is required to obtain a speed reduction resolution prior to the installation of any SPEED LIMIT (R2-1) signs shown on standard plate 634.63 or as shown in the plans. To provide adequate time for the resolution to be enacted, the Contractor will inform the Engineer a minimum of 3 weeks prior to the scheduled installation of any work zone speed reduction signs on the project. The information provided by the Contractor will include the anticipated

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date of sign installation, the newly reduced speed limit, the location of the work zone, and the anticipated completion date of work requiring the speed reduction.

TEMPORARY PAVEMENT MARKING

Covers on the tabs will be sufficiently secured to prevent traffic from dislodging the cover and when removed, the covers will be properly disposed of. The Contractor will remove and properly dispose of the tabs after permanent pavement marking is applied. Method of removal will be nondestructive to the road surface and will be accomplished within one week of completion of the permanent pavement marking.

Full reflectivity of all temporary flexible vertical markers (tabs) is required at all times. The Contractor will be required to replace any missing or non-reflective tabs after each installation as detailed below at no additional cost to the State.

TEMPORARY PAVEMENT MARKING TAPE, TYPE I

Temporary pavement marking for stop lines will consist of 4” Temporary Pavement Marking Tape Type I. Placement of each 24” white stop line will be accomplished by placing six pieces of 4” x 12’ tape adjacent to one another. Temporary tape will be removed upon completion of the project.

CONTRACTOR FURNISHED PORTABLE CHANGEABLE MESSAGE SIGN

One week prior to starting work affecting the traveling public, portable changeable message signs (PCMS) will be installed at locations detailed in the plans to notify drivers of the upcoming construction. The Contractor will program the portable changeable message signs with the following message:

BRIDGE  
CLOSING  
(Date)

DETOUR  
WILL BE  
AVAILABLE

When work begins that will affect traffic patterns, the Contractor will re-program the PCMS with the messages as detailed in the plans.

REMOVE SIGN FOR RESET AND RESET SIGN

Signs that are scheduled for reset will be dismantled and reassembled to the extent needed by the Contractor to properly reset the sign. Signs will be handled with care so that the existing signs, posts, and bases are not damaged during the relocation process.



INCIDENTAL WORK, GRADING

At MRM 50.6 on US16 EB there is a traffic diversion that consists of 3” of asphalt concrete pavement, 6” of base course, and 82 cubic yards of embankment. The traffic diversion will be removed, and the inslopes will be restored to their original condition. Incidental Work, Grading will be plans quantity and will not be adjusted according to field measurements. All costs for removal of the traffic diversion and site restoration will be incidental to the contract lump sum for “Incidental Work, Grading”.

REMOVE SIGN FOR RESET AND RESET SIGN

Signs that are scheduled for reset will be dismantled and reassembled to the extent needed by the Contractor to properly reset the sign. Signs will be handled with care so that the existing signs, posts, and bases are not damaged during the relocation process. The Contractor will replace and pay for any reset signs damaged in their care. The Contractor will remove and dispose of any existing posts for all reset signs that require use of new posts as shown in the Table of Permanent Signing.

All costs for removing, dismantling, and disposing of any existing posts will be incidental to the contract unit price per each for “Remove Sign for Reset”. All costs for resetting the existing signs will be incidental to the contract unit price per each for “Reset Sign”. All quantities for Remove Sign for Reset and Reset Sign will be per assembly at the contract unit price per each.

TABLE OF REMOVE AND RESET SIGN LOCATIONS			
Sign	Highway	MRM	Number
Yield	US 16A Westbound	59.36	1
Left Lane Ends (Symbol)	US 16 Westbound	49.93	2
Speed Limit 55	US 16 Westbound	50.13	1
Maintenance and Authorized Vehicles Only	US 16 Crossover	50.84	1
		TOTAL	5

INCIDENTS

An incident is an emergency road user occurrence, a natural disaster, or other unplanned event that affects or impedes the normal flow of traffic such as a crash, hazardous materials spill, or other event.

The Contractor will set up a meeting prior to start of work to plan and coordinate responses to an incident. The Contractor will invite the Department of Transportation, the South Dakota Highway Patrol, the Pennington County Sheriff and local emergency response entities to the meeting.

The Contractor will assist to maintain traffic as required by these plan notes and as agreed to at that meeting.

Emergency vehicle access through the project will be considered and discussed at the meeting.

The Contractor may be required to modify messages on portable changeable message signs or relocate portable changeable message signs, and to provide flaggers to direct or detour traffic. The Contractor should be prepared to relocate advance warning signs if determined to be necessary for a major traffic incident lasting more than two hours. Fixed location ground mounted signs may be covered and additional portable signs provided.

No additional payment will be made for the modification of portable changeable message sign messages or the relocation of portable changeable message signs. Cost for the relocation of an advance warning sign due to an incident will be 50% of the designated sign rate. Flaggers will be paid for at the contract unit price per hour for “Flagging”.

COVERING EXISTING SIGNS

Covering arrows and destinations on permanent signs will be incidental to “Traffic Control, Miscellaneous”. Sign covers will be sized to fit existing signs and attached with minimal extra holes. Signs that are damaged resulting from covering materials and/or methods used to complete thus work will be replaced by the Contractor at no cost to the State.

PRESS RELEASE ANNOUNCEMENTS

The SDDOT will prepare a press release to be released 5 days prior to any phase change or any other major change that affects traffic flow. The SDDOT will be responsible to keep law enforcement, emergency services, and the traveling public notified of changes in project access. The Contractor will provide the Engineer with pertinent information 7 days prior to any phase change or any other major change that affects traffic flow.

ITEMIZED LIST FOR TRAFFIC CONTROL SIGNS

SIGN CODE	SIGN DESCRIPTION	EXPRESSWAY / INTERSTATE			
		NUMBER	SIGN SIZE	SQFT PER SIGN	SQFT
R1-1	STOP	2	36"	7.5	15.0
R2-1	SPEED LIMIT 50	2	36" x 48"	12.0	24.0
R2-1	SPEED LIMIT 35	4	36" x 48"	12.0	48.0
R2-1	SPEED LIMIT 55	1	36" x 48"	12.0	12.0
R2-6aP	FINES DOUBLE (plaque)	2	36" x 24"	6.0	12.0
R3-2	LEFT TURN PROHIBITION (symbol)	1	36" x 36"	9.0	9.0
R5-1	DO NOT ENTER	1	36" x 36"	9.0	9.0
R5-1a	WRONG WAY	1	42" x 30"	8.8	8.8
R6-1	ONE WAY	2	36" x 12"	3.0	6.0
R11-2	ROAD CLOSED	4	48" x 30"	10.0	40.0
W3-5	SPEED REDUCTION AHEAD (50 MPH)	2	48" x 48"	16.0	32.0
W3-5	SPEED REDUCTION AHEAD (35 MPH)	2	48" x 48"	16.0	32.0
W4-2	LEFT or RIGHT LANE ENDS (symbol)	5	48" x 48"	16.0	80.0
W4-3	ADDED LANE (symbol)	2	48" x 48"	16.0	32.0
W13-1P	ADVISORY SPEED (plaque)	2	30" x 30"	6.3	12.6
W20-1	ROAD WORK AHEAD	4	48" x 48"	16.0	64.0
W20-4	ONE LANE ROAD AHEAD	1	48" x 48"	16.0	16.0
W20-5	LEFT or RIGHT LANE CLOSED AHEAD	5	48" x 48"	16.0	80.0
W20-7	FLAGGER (symbol)	1	48" x 48"	16.0	16.0
W21-5	SHOULDER WORK	1	48" x 48"	16.0	16.0
G20-2	END ROAD WORK	3	48" x 24"	8.0	24.0
SPECIAL	WATCH FOR ENTERING TRAFFIC	1	48" x 48"	16.0	16.0
		EXPRESSWAY / INTERSTATE TRAFFIC CONTROL SIGNS SQFT 604.4			

ITEMIZED LIST FOR DETOUR AND RESTRICTION SIGNING

SIGN CODE	SIGN DESCRIPTION	EXPRESSWAY / INTERSTATE			
		NUMBER	SIGN SIZE	SQFT PER SIGN	SQFT
SPECIAL	SPECIAL SIGN A	1	48" x 30"	10.0	10.0
SPECIAL	SPECIAL SIGN B	1	48" x 30"	10.0	10.0
SPECIAL	SPECIAL SIGN C	1	84" x 30"	17.5	17.5
SPECIAL	SPECIAL SIGN D	1	78" x 30"	16.3	16.3
SPECIAL	SPECIAL SIGN E	1	72" x 48"	24.0	24.0
SPECIAL	SPECIAL SIGN F	1	72" x 48"	24.0	24.0
SPECIAL	SPECIAL SIGN G	1	90" x 42"	26.3	26.3
SPECIAL	SPECIAL SIGN H	1	90" x 48"	30.0	30.0
		EXPRESSWAY / INTERSTATE DETOUR AND RESTRICTION SIGNING SQFT 158.1			

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# US 16/US 16A SOUTH INTERSECTION



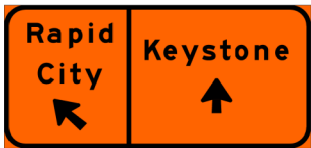
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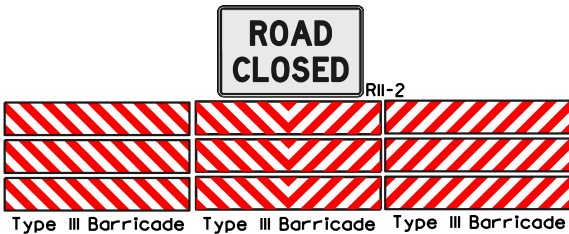
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Special Sign G

Remove Yield Sign for Reset



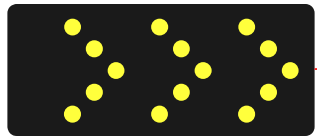
Special Sign D



Special Sign A



W13-IP



Arrow Board Sequential Chevron



Remove Existing Pavement Markings and Install Temporary Pavement Marking Tape for Two Lanes



W4-2



W20-5



W4-3



W4-3

Remove 5 Tubular Markers for Reset, Remove Conflicting Pavement Markings, and Open Crossover

600 ft Taper Length



Special Sign E

- Channelizing Device
- Reflectorized Drum



# US 16/US16A EAST INTERSECTION



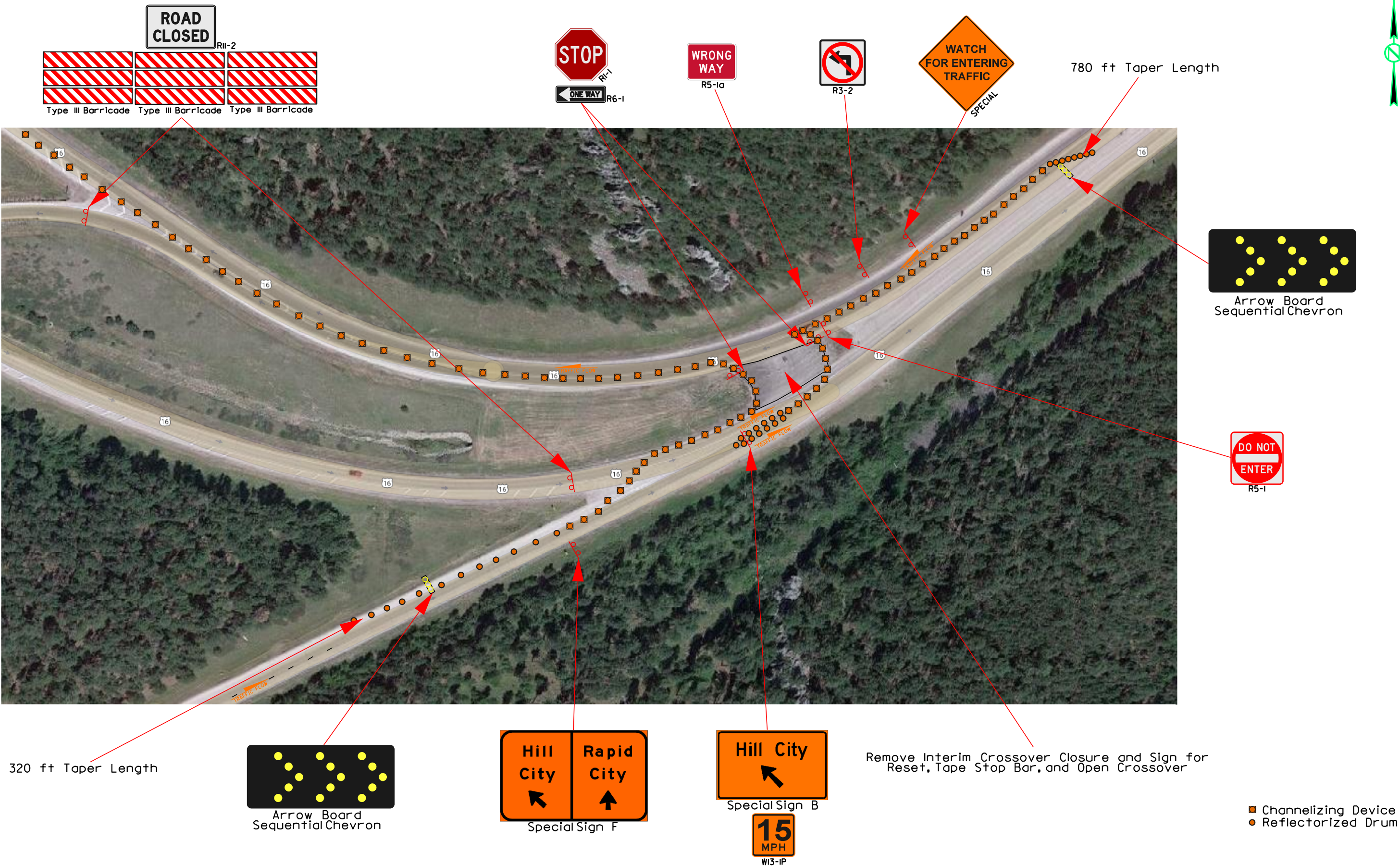
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# US 16/US16A WEST INTERSECTION

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Special Sign C

Remove Left Lane Ends  
Symbol Sign for Reset



Speical Sign H

Traffic Devices will Maintain  
One Lane of Traffic

- Channelizing Device
- Reflectorized Drum


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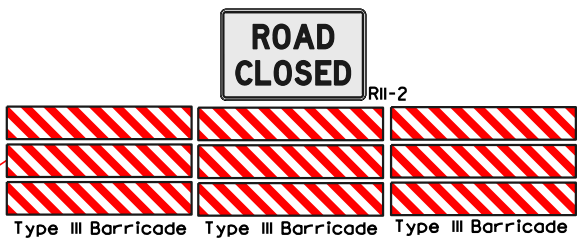
# US16/US16A WEST INTERSECTION

 <small>Plotting Date: 7/8/2025</small>	PROJECT	SECTION	SHEET
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Remove Advisory 45 M.P.H. Sign  
for Reset



- Channelizing Device
- Reflectorized Drum

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# CLOSURE COVERED SIGN LOCATIONS

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US I6 (MRM 51.42) West Bound East of Wye



Cover 'USE LEFT LANE' and Cover '1/2' with '1'

US I6 (MRM 51.08) West Bound East of Wye



Cover 'EXIT ONLY'

US I6 (MRM 50.79) West Bound East of Wye



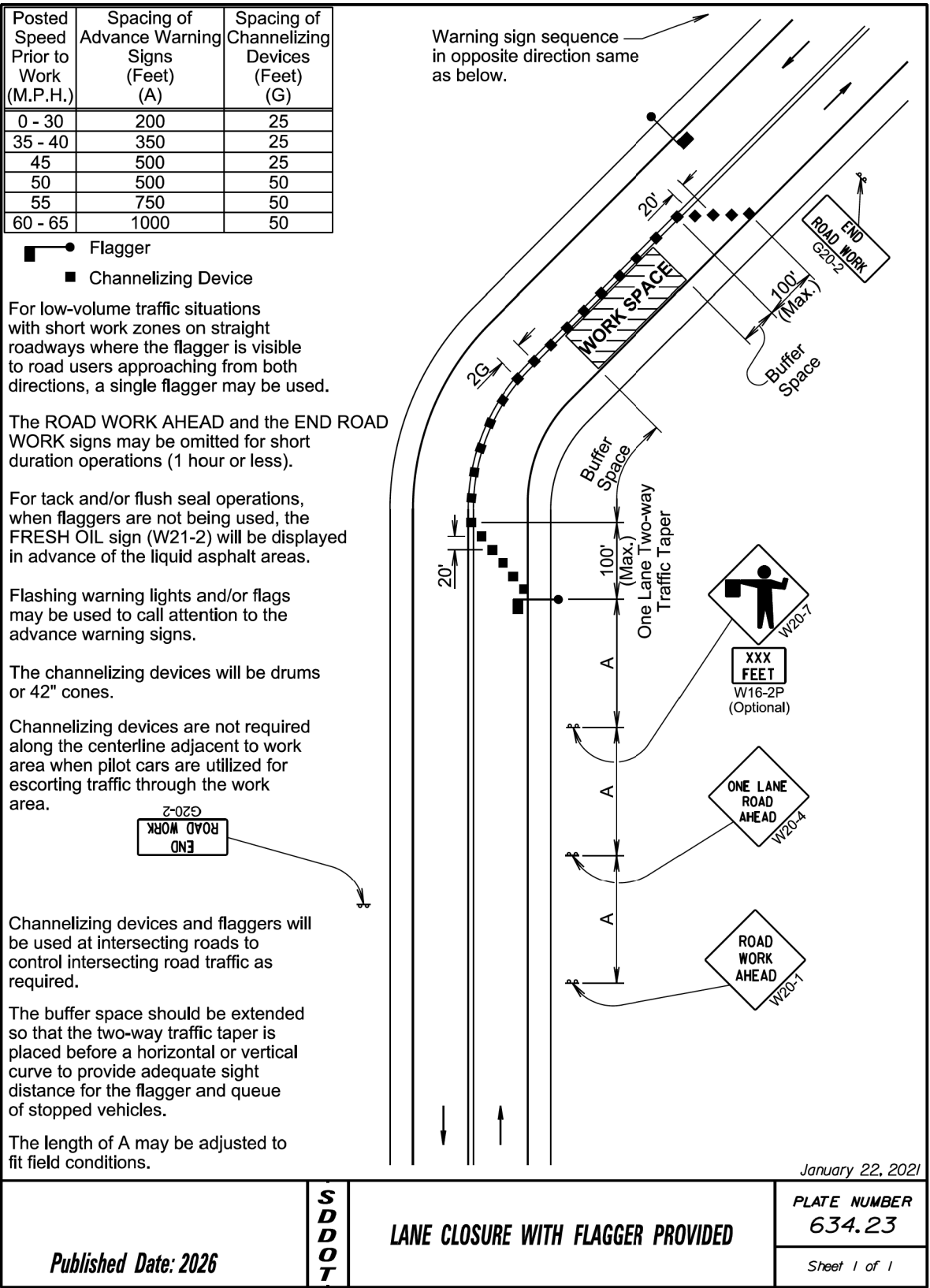
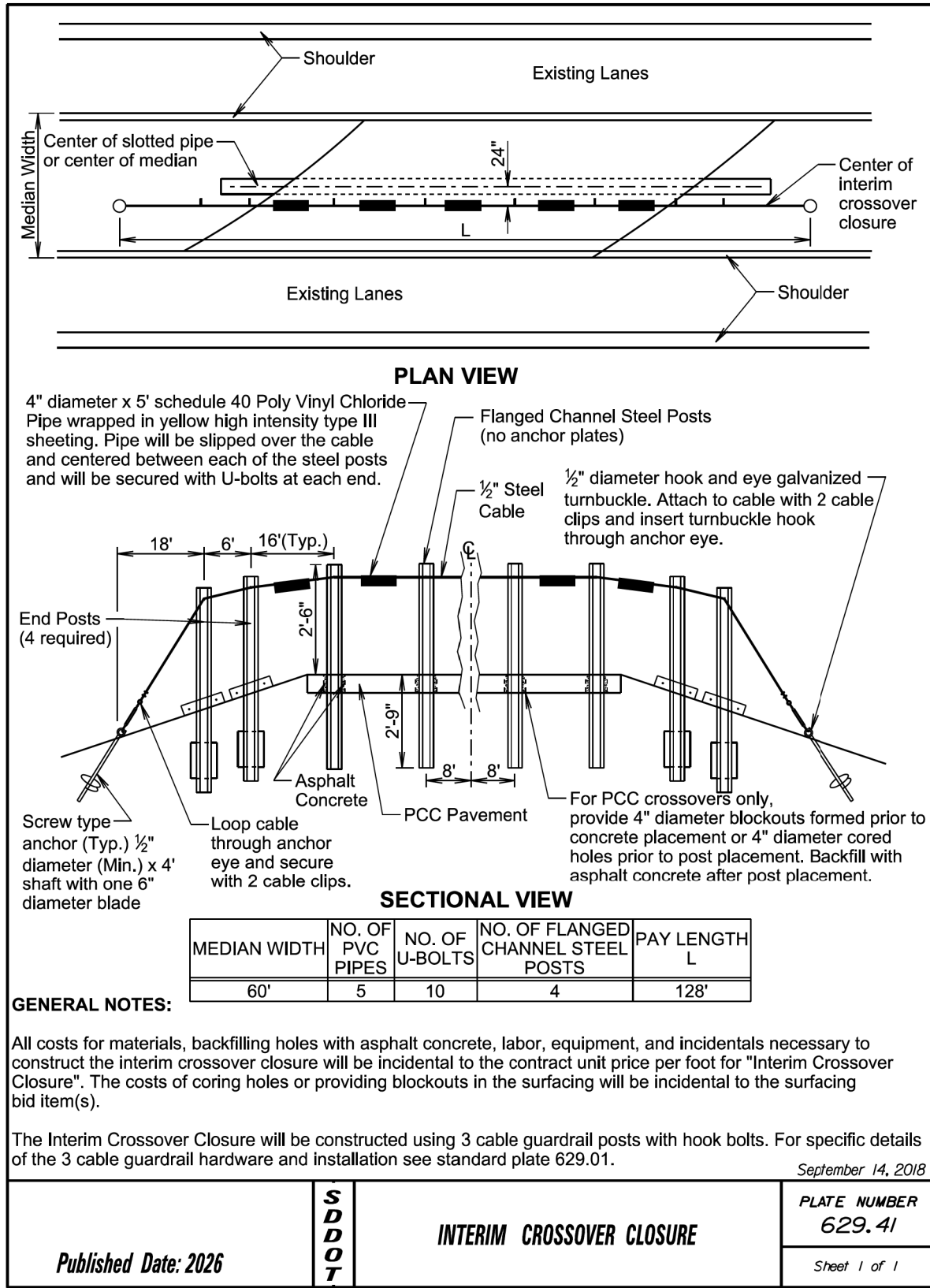
Cover Arrow for Keystone Mt Rushmore with Straight Ahead Arrow

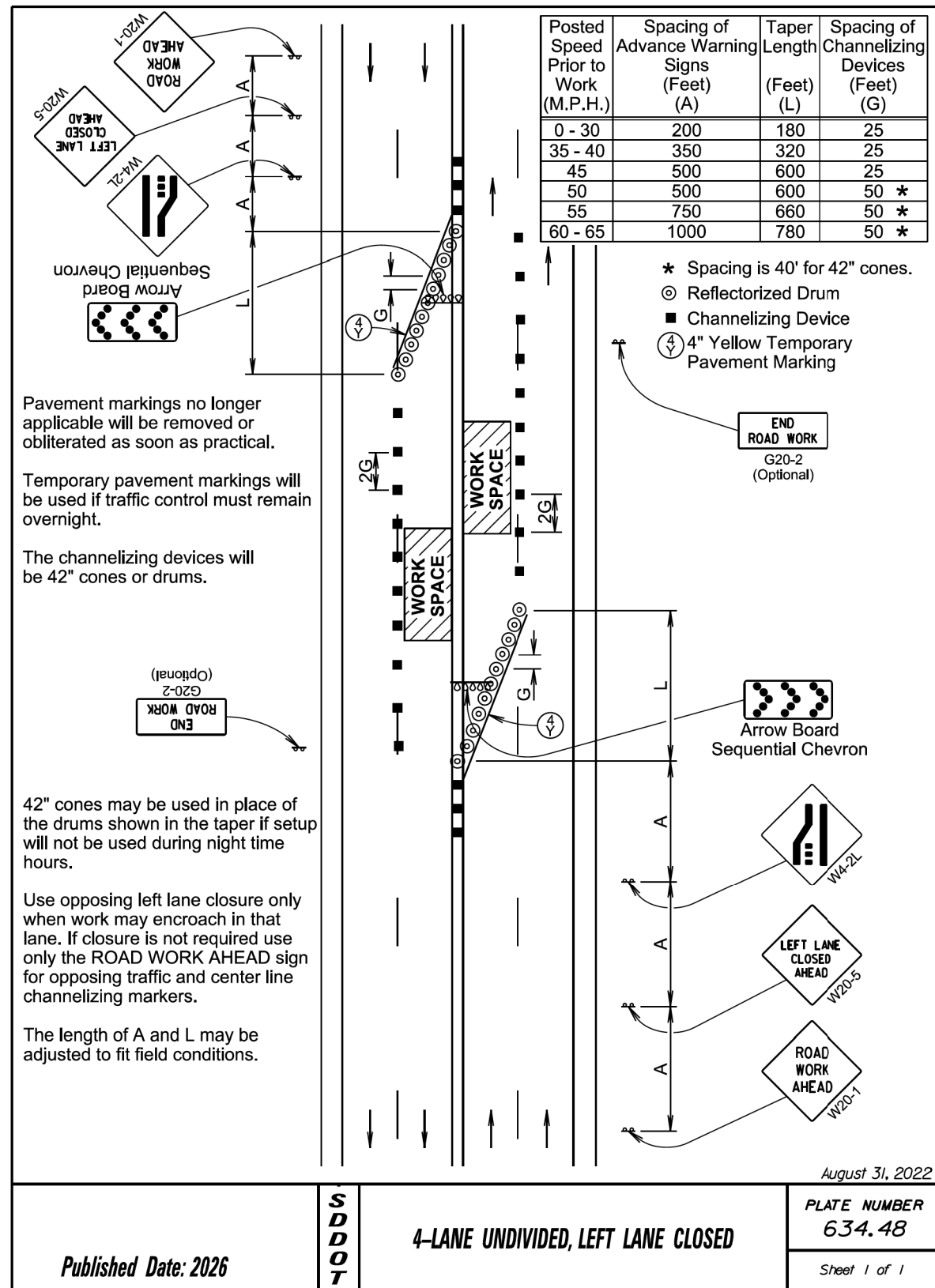




SPECIAL SIGNING DETAIL







Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet)		
	(A)	(B)	(C)
0 - 30	200		
35 - 40	350		
45 - 50	500		
55	750		
60 - 65	1000		
	(A)	(B)	(C)
70 - 80	1000	1500	2640

**\*\* Speed appropriate for location.**

© Reflectorized Drum

- Channelizing Device

ROAD WORK AHEAD sign is only required in advance of the first lane closure.

High speed is defined as having a posted speed limit greater than 45 mph.

