

STATE	PROJECT	SHEET	TOTAL
OF		NO.	SHEETS
SOUTH DAKOTA	P 0281(00)59	2	10

#### **ESTIMATE OF QUANTITIES**

BID ITEM NUMBER	ITEM	County Road 41 QUANTITY	County Road 4 QUANTITY	TOTAL QUANTITY	UNIT
009E0010	Mobilization	< Lump :	Sum>	Lump Sum	LS
110E5020	Salvage Traffic Sign	3	15	18	Each
120E6200	Water for Granular Material	147	-	147	MGal
280E0010	Process in Place Surfacing	49171	-	49171	SqYd
320E1200	Asphalt Concrete Composite	-	1000	1000	Ton
330E0010	MC-70 Asphalt for Prime	60.5	-	60.5	Ton
330E0100	SS-1h or CSS-1h Asphalt for Tack	-	4	4	Ton
330E1000	Blotting Sand for Prime	210	-	210	Ton
360E0020	AE150S Asphalt for Surface Treatment	73	-	73	Ton
360E0040	CRS-2 Asphalt for Surface Treatment	-	5.4	5.4	Ton
360E1030	Type 2A Cover Aggregate	-	50	50	Ton
360E1050	Type 3 Cover Aggregate	985	-	985	Ton
632E1505	4"x6" Wood Post	48	208	256	Ft
632E3205	Flat Aluminum Sign, Nonremovable Copy Super/Very High Intensity	16.8	100.8	117.6	SqFt
633E1300	Pavement Marking Paint, White	202	733	935	Gal
633E1305	Pavement Marking Paint, Yellow	19	87	106	Gal
634E0010	Flagging	40	40	80	Hour
634E0020	Pilot Car	20	20	40	Hour
634E0100	Traffic Control	424	256	680	Unit
634E0120	Traffic Control Miscellaneous	< Lump :	Sum>	Lump Sum	LS
634E0630	Temporary Pavement Marking	9	4.5	13.5	Mile

#### **TABLE OF MATERIALS QUANTITIES**

		WATER FOR GRAN. MATER.	PROCESS IN PLACE SURFACING	ASPHALT CONCRETE COMPOSITE	MC-70 ASPH. FOR PRIME	BLOTTING SAND FOR PRIME	SS-1h/ CSS-1h ASPH. FOR TACK	_	TYPE 2A COVER AGGR.	AE150S ASPH. FOR SURF. TREAT	TYPE 3 COVER AGGR.
SECT.	LOCATION	MGal	SqYd	Ton	Ton	Ton	Ton	Ton	Ton	Ton	Ton
1	County Road 41	147	49171	-	60.5	210	-	-	-	73.0	985
2	County Road 4	-	-	-	-	-	-	5.4	50	-	-
	Subtotals: Additional Quantities:	147	49171 -	- 1000	60.5 -	210 -	- 4.0	5.4 -	50 -	73.0 -	985 -
	Totals:	147	49171	1000	60.5	210	4.0	5.4	50	73.0	985

#### **RATES OF MATERIALS**

#### Section 1 County Road 41

Sta. 0+00.00 to Sta. 158+05.00

The Estimate of quantities is based on the following quantities of materials per mile.

#### PROCESS IN PLACE SURFACING

Water for Granular Material

49 MGals

MC-70 Asphalt for Prime at the rate of 20.2 tons applied 29 feet wide (Rate = 0.3 gallon per square yard).

Blotting Sand for Prime at the rate of 70 tons applied 24 feet wide (Rate = 10 pounds per square yard).

#### ASPHALT SURFACE TREATMENT

AE150S Asphalt for Surface Treatment at the rate of 24.4 tons applied 28 feet wide (Rate = 0.35 gallon per square yard).

Type 3 Cover Aggregate at the rate of 329 tons applied 28 feet wide (Rate = 40 pounds per square yard).

#### **RATES OF MATERIALS (For No Passing Zone Modification)**

Section 2 County Road 4

Sta. 136+00.00 to Sta. 239+00.00 Sta. 436+00.00 to Sta. 571+59.00

The Estimate of quantities is based on the following quantities of materials per mile.

#### ASPHALT SURFACE TREATMENT

CRS-2 Asphalt for Surface Treatment at the rate of 1.2 tons applied 2 feet wide on centerline (Rate = 0.25 gallon per SqYd).

Type 2A Cover Aggregate at the rate of 11 tons applied 2 feet wide on centerline (Rate = 18 pounds per SqYd).

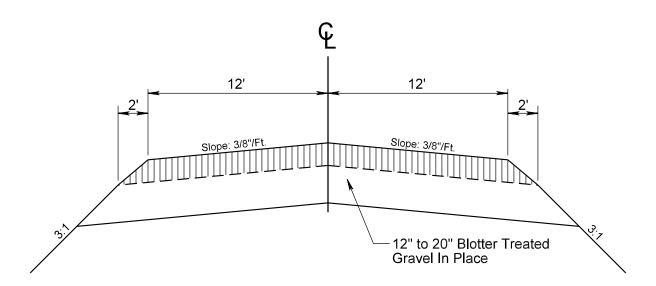
STATE OF	PROJECT	SHEET NO.	TOTAL SHEETS
SOUTH DAKOTA	P 0281(00)59	3	10

Plotting Date: 14-JUN-2006

# TYPICAL PROCESS IN PLACE SECTION

**SECTION 1** 

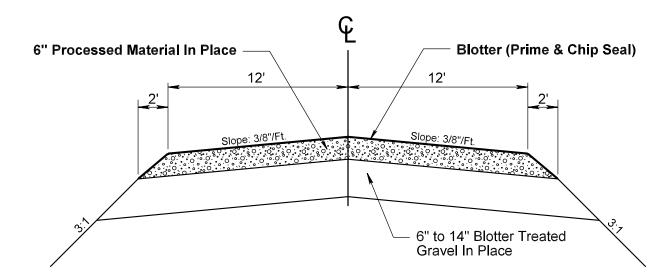
Sta. 0+00 to Sta. 158+05



Process In Place Surfacing 6" Depth

# TYPICAL RESURFACING SECTION SECTION 1

Sta. 0+00 to Sta. 158+05



STATE	PROJECT	SHEET	TOTAL
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SOUTH DAKOTA	P 0281(00)59	4	10

#### **SPECIFICATIONS**

Standard Specifications for Roads and Bridges, 2004 Edition and Required Provisions, Supplemental Specifications and/or Special Provisions as included in the Proposal.

#### **COMPLETION DATE**

All work shall be completed on or before September 15, 2006.

#### SURFACING THICKNESS DIMENSIONS

Plans quantities will be applied even though the thickness may vary from that shown on the plans.

At those locations where material must be placed to achieve a required elevation, plans quantities may be varied to achieve the required elevation.

#### **INTERSECTING ROADS AND ENTRANCES**

Intersecting roads and entrances shall be satisfactorily cleared of vegetation, shaped and compacted prior to placement of mainline surfacing. This work will be considered incidental to other contract items. Separate measurement and payment will not be made.

#### **SHOULDER WORK**

Prior to construction, Department of Transportation Maintenance Forces will spray the shoulders to kill existing vegetation. It is the Contractor's responsibility to notify the State a minimum of thirty days prior to starting work on the surface of the highway. The State assumes no responsibility for the effectiveness of the herbicide applied.

Vegetation and accumulated material on or adjacent to the existing roadway edge shall be removed to the satisfaction of the Engineer prior to Process in Place Surfacing. Any remaining windrow of accumulated material shall be spread evenly on the inslope adjacent to the asphalt shoulder, to the satisfaction of the Engineer, prior to the application of the flush seal.

Shoulder work shall be incidental to other contract items. Separate measurement and payment will not be made.

#### **WASTE DISPOSAL SITE**

The Contractor will be required to furnish a site(s) for the disposal of construction/demolition debris generated by this project.

Construction/demolition debris may not be disposed of within the State ROW.

All construction/demolition debris generated by this project shall be cleaned up and disposed of by the Contractor.

The waste disposal site(s) shall be managed and reclaimed in accordance with the following from the General Permit for Highway, Road, and Railway Construction/Demolition Debris Disposal Under the South Dakota Waste Management Program issued by the Department of Environment and Natural Resources.

The waste disposal site(s) shall not be located in a wetland, within 200 feet of surface water, or in an area that adversely affects wildlife, recreation, aesthetic value of an area, or any threatened or endangered species, as approved by the Engineer.

If the waste disposal site(s) is located such that it is within view of any ROW, the following additional requirements shall apply:

#### **WASTE DISPOSAL SITE (CONTINUED)**

- 1. Construction/demolition debris consisting of concrete, asphalt concrete or other similar materials shall be buried in a trench completely separate from wood debris. The final cover over the construction / demolition debris shall consist of a minimum of 1 foot of soil capable of supporting vegetation. Waste disposal sites provided outside of the State ROW shall be seeded in accordance with Natural Resources Conservation Service recommendations. Seeding recommendations may be obtained through the appropriate County NRCS Office. The Contractor shall control the access to waste disposal sites not within the State ROW through the use of fences, gates and placement of a sign or signs at the entrance to the site stating No Dumping Allowed.
- Concrete and asphalt concrete debris may be stockpiled within view of the ROW for a period of time not to exceed the duration of the project. Prior to project completion, the waste shall be removed from view of the ROW or buried and the waste disposal site reclaimed as noted above.

The above requirements will not apply to waste disposal sites that are covered by an individual solid waste permit as specified in SDCL 34A-6-58, SDCL 34A-6-1.13, and ARSD 74:27:10:06.

Failure to comply with the requirements stated above may result in civil penalties in accordance with South Dakota Solid Waste Law, SDCL 34A-6-1.31.

Cost for furnishing waste disposal site(s), disposing of waste, maintaining control of access (fence, gates & signs) and reclamation of the waste disposal site(s) shall be incidental to the contract unit prices for the various items.

#### **WATER SOURCE**

The Contractor shall not withdraw water directly from streams in watersheds of the James, Vermillion, and Big Sioux Rivers without prior approval from the SDDOT Environmental Office, contact Dave Graves at (605) 773-5727. Water may be obtained from other sources not directly connected to these streams such as stock dams, wetlands, or wells. This note does not relieve the Contractor of his/her responsibility to obtain the necessary permits from other agencies such as DENR (South Dakota Department of Environment and Natural Resources) and COE (Corps of Engineers).

#### PROCESS IN PLACE SURFACING

The processed material shall be stockpiled at a site, which may include the shoulders and inslopes, satisfactory to the Engineer.

Proper drainage shall be maintained so water will not pond on the mainline granular surfacing or embankment. Proper drainage will be to the satisfaction of the Engineer.

#### **ASPHALT CONCRETE COMPOSITE**

Mineral aggregate for the Asphalt Concrete shall conform to the requirements of the Standard Specifications for Class E, Type 1.

Asphalt Concrete may be obtained from a hot plant producing asphalt concrete for the SDDOT in accordance with Class Q, low, medium or high traffic volume or Superpave 12.5mm or 19mm asphalt concrete specifications.

- If the asphalt mixture used on the project is a Class Q-LVT, MVT, HVT, Superpave 12.5mm or 19mm from another project the job-mix formula for the mix shall apply.
- Testing requirements for the mineral aggregate shall be in accordance with Asphalt Concrete Composite specifications.

All other requirements in the Standard Specifications for Asphalt Concrete Composite shall apply.

The asphalt binder used in the mixture shall be PG 58-28, PG 58-34, PG 64-22, PG 64-28 or PG 64-34 Asphalt Binder.

#### **ADDITIONAL QUANTITIES**

Included in the Estimate of Quantities are 1000 tons of Asphalt Concrete Composite for patching of the existing surface on Davison County Road 4. Asphalt Concrete Composite shall be paved in numerous areas to strengthen the existing surface. Patches shall be matched up to cover both sides of the roadway at a depth of approximately 1".

Included in the Estimate of Quantities are 4 tons of SS-1h or CSS-1h Asphalt for Tack (Rate = 0.05 gallon per square yard) for patching throughout the project.

#### **TEMPORARY PAVEMENT MARKING**

The total length of no passing zone on this project is estimated to be 0.4 mile.

It is estimated that 32 DO NOT PASS and 32 PASS WITH CARE signs will be required to mark the no passing zones, should the Contractor elect to use these signs.

Use of DO NOT PASS and PASS WITH CARE signs will be allowed for a two week duration.

Cost for furnishing, installing and removing the DO NOT PASS and PASS WITH CARE signs shall be incidental to the contract unit price per mile for Temporary Pavement Marking.

Temporary road markers shall be used as the temporary pavement marking where marking will be obliterated by the work.

Cost for furnishing, applying and uncovering of the Temporary Road Markers shall be included in the contract unit price per mile for Temporary Pavement Marking.

In the absence of a signed lane closure or pilot car operation, Flagger symbol signs (W20-7a) and flaggers, or a shadow vehicle with rotating yellow lights or strobe lights shall be positioned on the roadway shoulder in advance of workers for both directions of traffic during the installation and removal of temporary road markers. The traffic control device used shall be moved intermittently to provide proper warning of the work operation. A ROAD WORK AHEAD (W20-1), a Workers symbol sign (W21-1a) or a BE PREPARED TO STOP (W3-4) warning sign shall be mounted on the rear of the shadow vehicle. The method of traffic control used by the Contractor for this work shall be approved by the Engineer.

Cost for the traffic control to install the Temporary Road Markers shall be incidental to the contract unit price per mile for Temporary Pavement Marking.

#### PERMANENT PAVEMENT MARKING

The Contractor shall advise the Engineer a minimum of 2 weeks prior to the application of the permanent pavement marking to allow the State to check and mark the location of no passing zones.

Application of permanent pavement marking shall be completed within 14 days following completion of the final surfacing.

#### **SEQUENCE OF OPERATIONS**

The following Sequence of Operation shall be followed by the Contractor unless an alternate Sequence of Operations is submitted in writing and approved by the Engineer.

- 1. Install traffic control as detailed in these plans.
- 2. Complete process in place surfacing & asphalt surface treatment on Davison County Road 41.
- 3. Complete patching with Asphalt Concrete Composite & asphalt surface treatment on Davison County Road 4.
- 4. Complete permanent pavement marking and No Passing Zone signing upgrade.

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#### **GENERAL MAINTENANCE OF TRAFFIC**

Removing, relocating, covering, salvaging and resetting of permanent traffic control devices, including delineation, shall be the responsibility of the Contractor. Cost for this work shall be incidental to the contract unit prices for the various items unless otherwise specified in the plans. Any delineators and signs damaged or lost shall be replaced by the Contractor at no cost to the State.

Storage of vehicles and equipment shall be outside the clear zone and as near as possible to the right-of-way line. Contractor's employees should mobilize at a location off the right-of-way and arrive at the work sites in a minimum number of vehicles necessary to perform the work.

Indiscriminate driving and parking of vehicles within the right-of-way will not be permitted. Any damage to the vegetation, surfacing, embankment, delineators and existing signs resulting from such indiscriminate use shall be repaired and/or restored by the Contractor, at no expense to the State, and to the satisfaction of the Engineer.

The Contractor shall provide documentation that all breakaway sign supports comply with FHWA NCHRP 350 crash-worthy requirements. The Contractor shall provide installation details at the preconstruction meeting for all breakaway sign support assemblies.

Sufficient traffic control devices have been included in these plans to sign one workspace. If the Contractor elects to work on additional sites simultaneously, the cost for additional traffic control devices shall be incidental to the contract unit price per unit for Traffic Control.

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#### FURNISHING AND APPLYING PAVEMENT MARKING PAINT

#### TWO LANE ROADWAY

8" WHITE 4"
4" YELLOW 5.
2" from CL 4.
4" YELLOW 2.
from CL 5.
8" WHITE 4.

NO PASSING ZONE LINE

10'

8" WHITE

12'

NO PASSING ZONE LINE

8" WHITE

ZONE OF LIMITED SIGHT DISTANCE FOR CAR Y

ZONE OF LIMITED SIGHT
DISTANCE FOR CAR X

Approximate paint application rates shall be as follows:



Typical pavement marking as shown on this sheet shall be applied throughout the entire length of undivided roadway.

Traffic Control shall be incidental to the cost of application. The striper and advance or trailing warning vehicle shall be equipped with flashing amber lights or advance warning arrow panel.

ESTIMATED QUAN	TITIES
PAVEMENT MARKING PAINT	QUANTITY
WHITE	935 GALLONS
YELLOW	106 GALLONS

Included in the above quantities are:

- 1 Gallon white for:
- 1 24" Stop Bar

NOTE: All pavement marking dimensions are based on 12' driving lanes.

Posted	Spacing of	Spacing of
Speed	Advance Warning	Channelizing
Prior to	Signs	Devices
Work	(Feet)	(Feet)
(M.P.H.)	(A)	(G)
0 - 30	200	25
35 - 40	350	25
45 - 50	500	50
55	750	50
60 - 65	1000	50

#### ■ Flagger

■ Channelizing Device

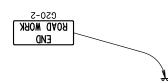
For low-volume traffic situations with short work zones on straight roadways where the flagger is visible to road users approaching from both directions, a single flagger may be used.

The ROAD WORK AHEAD and the END ROAD WORK signs may be omitted for short duration operations (I hour or less).

For tack and/or flush seal operations, when flaggers are not being used, the FRESH Oil sign (W21-2) shall be displayed in advance of the liquid asphalt

Flashing warning lights and/or flags may be used to call attention to the advance warning signs.

The channelizing devices shall be drums or type II barricades if traffic control must remain overnight or longer. 42" cones may be used in lieu of drums or type II barricades only along the centerline.



Channelizing devices are not required along the centerline adjacent to work area when pilot cars are utilized for escorting traffic through the work

Channelizing devices and flaggers shall be used at intersecting roads to control intersecting road traffic as required.

The buffer space shall be a sufficient length so that the channelizing devices are visible to approaching traffic.

800x One Tr XXX FEET (Optional) ONE LANE ROAD AHEAD ROAD WORK AHEAD July I, 2005 PLATE NUMBER

Warning sign sequence-

as below.

in opposite direction same

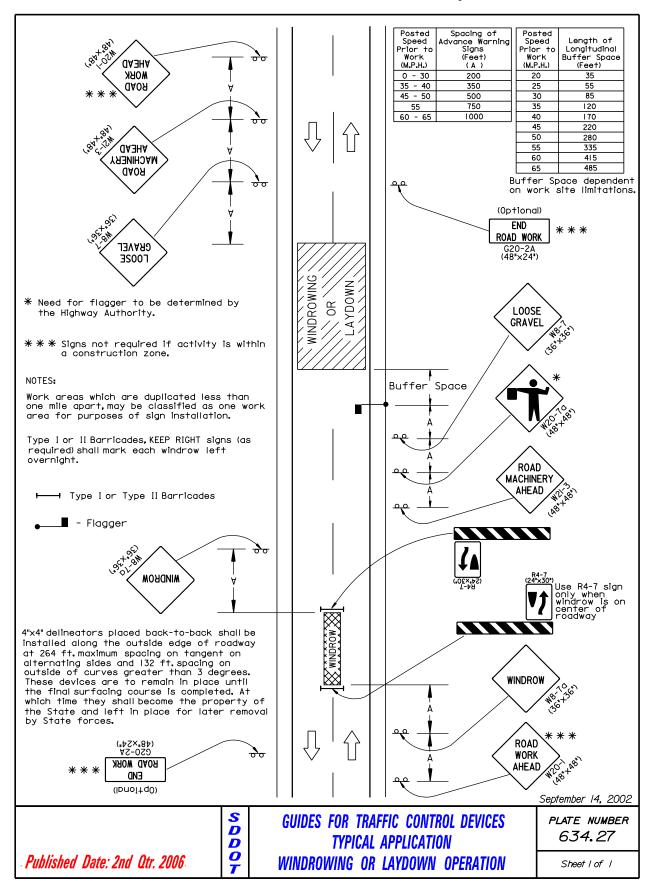
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**GUIDES FOR TRAFFIC CONTROL DEVICES** LANE CLOSURE WITH FLAGGER PROVIDED 634.23

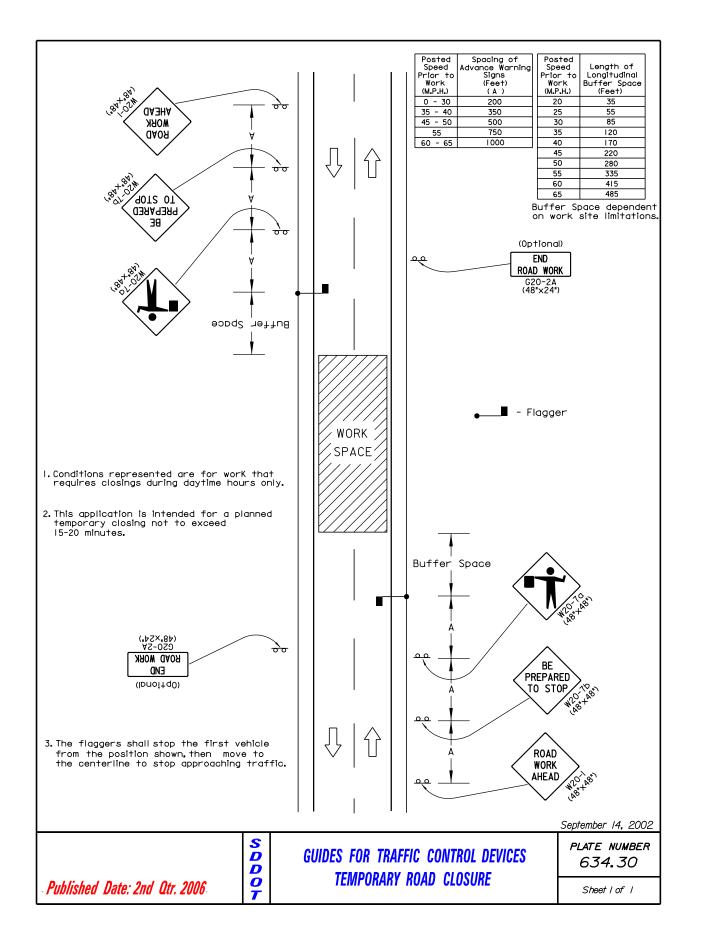
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PROJECT STATE OF SHEET TOTAL SHEETS P 0281(00)59 10 DAKOTA

#### Plotting Date: 14-JUN-2006



Published Date: 2nd Qtr. 2006



STATE OF	PROJECT	SHEET	TOTAL SHEETS
SOUTH DAKOTA	P 0281(00)59	8	10

Plotting Date: 14-JUN-2006

#### ITEMIZED LIST FOR TRAFFIC CONTROL

Section	SIGN CODE	SIGN SIZE	DESCRIPTION	NUMBER	UNITS PER	UNITS
G20-2a   36" x 16"   END ROAD WORK   2   17   34   34   34   34   34   34   34   3			22001	REQUIRED	SIGN	
R1-1	E5-1	36" x 32"	EXIT GORE SIGN		24	
R1-2	G20-2a	36" x 18"	END ROAD WORK	2	17	34
R2-1	R1-1	48" x 48"	STOP		34	
R2-5a   30" x 36"   REDUCED SPEED AHEAD   2   3   36   REP. RIGHT (SPMBOL)   2   18   36   36   REP. RIGHT (SPMBOL)   2   18   36   36   36   36   36   36   36   3	R1-2	48" x 48"	YIELD		34	
R4-7	R2-1	30" x 36"	SPEED LIMIT		23	
R5-1         48" x 48" N WONG WAY         34           R5-18         48" x 36" N WONG WAY         29           R10-6         24" x 38" STOP HERE ON RED         20           R11-2         48" x 36" N WONG WAY         20           R11-32         60" x 30" ROAD CLOSED MILES AHEAD LOCAL TRAFFIC ONLY         30           R11-44         60" x 30" ROAD CLOSED TO THRU TRAFFIC         30           W1-1         48" x 48" REF CARSON         34           W1-2         48" x 48" REF OR RIGHT TURN ARROW         34           W1-3         48" x 48" REVERSE TURN SIGN (LEFT OR RIGHT)         34           W1-3a         48" x 48" REVERSE TURN SIGN (LEFT OR RIGHT)         34           W3-3a         48" x 48" SIGNAL AHEAD (SYMBOL)         34           W3-3         48" x 48" SIGNAL AHEAD (SYMBOL)         34           W3-5         48" x 48" SIGNAL AHEAD (SYMBOL)         34           W4-1         48" x 48" NERGE (SYMBOL)         34           W5-2         48" x 48" NERGE (SYMBOL)         34           W5-2         48" x 48" NERGE (SYMBOL)         34           W8-3         36" x 36" NEXT_MILES         34           W8-1         48" x 48" NERGE (SYMBOL)         34           W8-2         48" x 48" NERGE (SYMBOL)         34 <td>R2-5a</td> <td>30" x 36"</td> <td>REDUCED SPEED AHEAD</td> <td></td> <td>23</td> <td></td>	R2-5a	30" x 36"	REDUCED SPEED AHEAD		23	
R5-1a	R4-7	24" x 30"	KEEP RIGHT (SYMBOL)	2	18	36
R10-6	R5-1	48" x 48"	DO NOT ENTER		34	
R11-2	R5-1a	48" x 36"	WRONG WAY		29	
R11-3a 60" x 30" ROAD CLOSED MILES AHEAD LOCAL TRAFFIC ONLY R11-4 60" x 30" ROAD CLOSED TO THRU TRAFFIC SW12-th 120" x 60" HIGHWAY WORKERS GIVEEM A BRAKE 80 80 80 81 81 81 81 81 81 81 81 81 81 81 81 81	R10-6	24" x 36"	STOP HERE ON RED		20	
R11-4   60" x 30"   ROAD CLOSED TO THRU TRAFFIC   30   80   WILDING   WILD	R11-2	48" x 30"	ROAD CLOSED		27	
SW12-1b   120" x 60"   HIGHWAY WORKERS GIVE'EM A BRAKE   80   34   W1-12   48" x 48"   W1-13	R11-3a	60" x 30"	ROAD CLOSED MILES AHEAD LOCAL TRAFFIC ONLY		30	
W1-1	R11-4	60" x 30"	ROAD CLOSED TO THRU TRAFFIC		30	
W1-2       48" x 48"       LEFT OR RIGHT CURVE ARROW       34         W1-3       48" x 48"       x 48"       ReVERSE TURN SIGN (LEFT OR RIGHT)       34         W1-4a       48" x 48"       x 48"	SW12-1b	120" x 60"	HIGHWAY WORKERS GIVE'EM A BRAKE		80	
W1-2       48" x 48"       LEFT OR RIGHT CURVE ARROW       34         W1-3       48" x 48"       x 48"       ReVERSE TURN SIGN (LEFT OR RIGHT)       34         W1-4a       48" x 48"       x 48"	W1-1	48" x 48"	LEFT OR RIGHT TURN ARROW		34	
W1-4a	W1-2		LEFT OR RIGHT CURVE ARROW		34	
W1-4a						
W3-1a					34	
W3-2a			·			
W3-3			` '			
W3-5			` '			
W4-1       48" x 48"       MERGE (SYMBOL)       34         W4-2       48" x 48"       LEFT OR RIGHT LANE ENDS (SYMBOL)       34         W5-2       48" x 48"       NARROW BRIDGE       34         W5-3       48" x 48"       ONE LANE BRIDGE       34         W7-3a       30" x 24"       NEXT			` ,			
W4-2         48" x 48" A8" A8" A8" A8ROW BRIDGE         34           W5-2         48" x 48" A8" A8ROW BRIDGE         34           W5-3         48" x 48" A8" A8ROW BRIDGE         34           W7-3a         30" x 24" BINDGE         18           W8-1         36" x 36" BIMP         27           W8-6         48" x 48" BIMP         27           W8-7a         36" x 36" BIMP         2         34         68           W8-7a         36" x 36" BIMP         2         27         54           W8-7a         36" x 36" BIMDEROP-OFF         34         48         48" x 48" BIMULDER DROP-OFF         34         48           W8-11         48" x 48" BIMULDER DROP-OFF         34         48         48         48" x 48" BIMULDER DROP-OFF         34         48           W20-1         48" x 48" BAUSORY SPEED PLATE         16         68         48<			l · · · · · · · · · · · · · · · · · · ·			
W5-2         48" × 48"         NARROW BRIDGE         34           W5-3         48" × 48"         ONE LANE BRIDGE         34           W7-3a         30" × 24"         NEXT_MILES         18           W8-1         36" × 36"         BUMP         27           W8-6         48" × 48"         TRUCK CROSSING         2         34         68           W8-7         36" × 36"         WINDROW         2         27         54           W8-9a         48" × 48"         WINDROW         2         27         54           W8-9a         48" × 48"         SHOULDER DROP-OFF         34         48           W8-11         48" × 48"         ADVISORY SPEED PLATE         16         7           W20-1         48" × 48"         ROAD WORK AHEAD         2         34         68           W20-2         48" × 48"         DETOUR AHEAD         34         68           W20-3         48" × 48"         ONE LANE ROAD AHEAD         34         68           W20-4         48" × 48"         ONE LANE CLOSED AHEAD         34         68           W20-7a         48" × 48"         WORKERS (SYMBOL)         34         68           W21-1a         48" × 48"         WORKERS (SYMB						
W5-3			` '			
W7-3a       30" x 24"       NEXTMILES       18         W8-6       48" x 48"       TRUCK CROSSING       2       34       68         W8-7       36" x 36"       LOOSE GRAVEL       2       27       54         W8-7a       36" x 36"       WINDROW       2       27       54         W8-9a       48" x 48"       UNEVEN LANES       34       34         W8-11       48" x 48"       UNEVEN LANES       34       48         W13-1       24" x 24"       ADVISORY SPEED PLATE       16       68         W20-1       48" x 48"       ROAD WORK AHEAD       2       34       68         W20-2       48" x 48"       ROAD CLOSED AHEAD       34       48         W20-3       48" x 48"       ROAD CLOSED AHEAD       34       68         W20-5       48" x 48"       ONE LANE ROAD AHEAD       2       34       68         W20-7a       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       68         W21-2       36" x 36"       FRESH OIL       2       2       34       68         W21-5       48" x 48"       RIGHT SHOULD	W5-3					
W8-1       36" x 36"       BUMP       27         W8-6       48" x 48"       TRUCK CROSSING       2       34       68         W8-7       36" x 36"       LOOSE GRAVEL       2       27       54         W8-7a       36" x 36"       WINDROW       2       27       54         W8-9a       48" x 48"       HOULDER DROP-OFF       34       34         W8-11       48" x 48"       UNEVEN LANES       34       34         W13-1       24" x 24"       ADVISORY SPEED PLATE       16       68         W20-1       48" x 48"       DETOUR AHEAD       2       34       68         W20-2       48" x 48"       DETOUR AHEAD       34       68         W20-3       48" x 48"       ROAD CLOSED AHEAD       34       68         W20-5       48" x 48"       ONE LANE ROAD AHEAD       2       34       68         W20-7a       48" x 48"       WARE ROAD AHEAD       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       68         W21-2       36" x 36"       RESHOIL       2       27       54         W21-5       48" x 48"       SHOULDER WORK       34       68						
W8-6       48" x 48"       TRUCK CROSSING       2       34       68         W8-7       36" x 36"       LOOSE GRAVEL       2       27       54         W8-7a       36" x 36"       WINDROW       2       27       54         W8-9a       48" x 48"       WINDROW       2       27       54         W8-11       48" x 48"       UNEVEN LANES       34       48         W13-1       24" x 24"       ADVISORY SPEED PLATE       16       8         W20-1       48" x 48"       ROAD WORK AHEAD       2       34       68         W20-2       48" x 48"       ROAD CLOSED AHEAD       34       68         W20-3       48" x 48"       ONE LANE ROAD AHEAD       2       34       68         W20-5       48" x 48"       ONE LANE ROAD AHEAD       34       68         W20-7a       48" x 48"       HAR X 48"       48" x 48"       468         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       68         W21-2       36" x 36"       FRESH OIL       2       34       68         W21-5       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5a       48" x 48"						
W8-7       36" x 36"       LOOSE GRAVEL       2       27       54         W8-7a       36" x 36"       WINDROW       2       27       54         W8-9a       48" x 48"       WOULDER DROP-OFF       34       34         W8-11       48" x 48"       UNEVEN LANES       34       34         W13-1       24" x 24"       ADVISORY SPEED PLATE       16       34         W20-1       48" x 48"       DETOUR AHEAD       2       34       68         W20-2       48" x 48"       ROAD CLOSED AHEAD       34       68         W20-3       48" x 48"       ROAD CLOSED AHEAD       34       68         W20-4       48" x 48"       CONE LANE ROAD AHEAD       2       34       68         W20-5       48" x 48"       LOR T. LANE CLOSED AHEAD       34       68         W20-7a       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       68         W21-2       36" x 36"       WORKERS (SYMBOL)       2       27       54         W21-5       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5       48" x 48				2		68
W8-7a       36" x 36"       WINDROW       2       27       54         W8-9a       48" x 48"       SHOULDER DROP-OFF       34       34         W8-11       48" x 48"       UNEVEN LANES       34       34         W13-1       24" x 24"       ADVISORY SPEED PLATE       16       2         W20-1       48" x 48"       ROAD WORK AHEAD       2       34       68         W20-2       48" x 48"       DETOUR AHEAD       34       34         W20-3       48" x 48"       ROAD CLOSED AHEAD       2       34       68         W20-4       48" x 48"       ONE LANE ROAD AHEAD       2       34       68         W20-5       48" x 48"       ONE LANE CLOSED AHEAD       34       68         W20-7a       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       68         W21-2       36" x 36"       WORKERS (SYMBOL)       2       2       7       54         W21-3       48" x 48"       RGAD MACHINERY AHEAD       2       34       68         W21-5       48" x 48"       RIGHT SHOULDER CLOSED       34       68         W21-5b <td></td> <td></td> <td></td> <td></td> <td></td> <td></td>						
W8-9a       48" x 48"       SHOULDER DROP-OFF       34         W8-11       48" x 48"       UNEVEN LANES       34         W13-1       24" x 24"       ADVISORY SPEED PLATE       16         W20-1       48" x 48"       ROAD WORK AHEAD       2       34       68         W20-2       48" x 48"       DETOUR AHEAD       34       34         W20-3       48" x 48"       ROAD CLOSED AHEAD       34       68         W20-4       48" x 48"       ONE LANE ROAD AHEAD       2       34       68         W20-5       48" x 48"       LT. OR RT. LANE CLOSED AHEAD       34       68         W20-7a       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       68         W21-2       36" x 36"       FRESH OIL       2       27       54         W21-3       48" x 48"       SHOULDER WORK       34       68         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       68         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       15       15						
W8-11       48" x 48"       UNEVEN LANES       34         W13-1       24" x 24"       ADVISORY SPEED PLATE       16         W20-1       48" x 48"       ROAD WORK AHEAD       2       34       68         W20-2       48" x 48"       DETOUR AHEAD       34       34         W20-3       48" x 48"       ROAD CLOSED AHEAD       34       68         W20-4       48" x 48"       ROAD CLOSED AHEAD       2       34       68         W20-5       48" x 48"       LT. OR RT. LANE CLOSED AHEAD       34       68         W20-7a       48" x 48"       FLAGGER       2       34       68         W21-1a       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-2       36" x 36"       FRESH OIL       2       27       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5a       48" x 48"       SHOULDER WORK       34       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       18       15         *******       12" x 36"       TYPE III BARRICADE - 6 FT. S				_		٠.
W13-1       24" x 24"       ADVISORY SPEED PLATE       16         W20-1       48" x 48"       ROAD WORK AHEAD       2       34       68         W20-2       48" x 48"       DETOUR AHEAD       34       34         W20-3       48" x 48"       ROAD CLOSED AHEAD       2       34       68         W20-4       48" x 48"       ONE LANE ROAD AHEAD       2       34       68         W20-5       48" x 48"       LT. OR RT. LANE CLOSED AHEAD       34       68         W20-7a       48" x 48"       FLAGGER       2       34       68         W20-7b       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       68         W21-2       36" x 36"       FRESH OIL       2       27       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED       34       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL						
W20-1       48" x 48"       ROAD WORK AHEAD       2       34       68         W20-2       48" x 48"       DETOUR AHEAD       34       34         W20-3       48" x 48"       ROAD CLOSED AHEAD       34       68         W20-4       48" x 48"       ONE LANE ROAD AHEAD       2       34       68         W20-5       48" x 48"       LT. OR RT. LANE CLOSED AHEAD       34       68         W20-7a       48" x 48"       FLAGGER       2       34       68         W20-7b       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       68         W21-2       36" x 36"       FRESH OIL       2       2       7       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       18       15         *******       12" x 36"       TYPE III OBJECT MARKER       15       2       20       40						
W20-2       48" x 48"       DETOUR AHEAD       34         W20-3       48" x 48"       ROAD CLOSED AHEAD       34         W20-4       48" x 48"       ONE LANE ROAD AHEAD       2       34       68         W20-5       48" x 48"       LT. OR RT. LANE CLOSED AHEAD       34       68         W20-7a       48" x 48"       FLAGGER       2       34       68         W20-7b       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       68         W21-2       36" x 36"       FRESH OIL       2       27       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5       48" x 48"       SHOULDER WORK       34       68         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       18       15         *******       12" x 36"       TYPE III OBJECT MARKER       15       2       20       40         *******       TYPE III BARRICADE				2		68
W20-3       48" x 48"       ROAD CLOSED AHEAD       34       68         W20-4       48" x 48"       ONE LANE ROAD AHEAD       2       34       68         W20-5       48" x 48"       LT. OR RT. LANE CLOSED AHEAD       34       68         W20-7a       48" x 48"       FLAGGER       2       34       68         W20-7b       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       34         W21-2       36" x 36"       FRESH OIL       2       27       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5       48" x 48"       SHOULDER WORK       34       68         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       48         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       18       15         *******       12" x 36"       TYPE III OBJECT MARKER       15       15         *******       TYPE III BARRICADE - 6 FT. SINGLE SIDED       2       20       40         *******				_		
W20-4       48" x 48"       ONE LANE ROAD AHEAD       2       34       68         W20-5       48" x 48"       LT. OR RT. LANE CLOSED AHEAD       34       68         W20-7a       48" x 48"       FLAGGER       2       34       68         W20-7b       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       54         W21-2       36" x 36"       FRESH OIL       2       27       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5       48" x 48"       SHOULDER WORK       34       68         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       48         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       18       18         *******       12" x 36"       TYPE III OBJECT MARKER       15       15         *******       TYPE III BARRICADE - 6 FT. SINGLE SIDED       2       20       40         *******       TYPE III BARRICADE - 8 FT. SINGLE SIDED       40						
W20-5       48" x 48"       LT. OR RT. LANE CLOSED AHEAD       34         W20-7a       48" x 48"       FLAGGER       2       34       68         W20-7b       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       34         W21-2       36" x 36"       RESH OIL       2       27       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5a       48" x 48"       SHOULDER WORK       34       8       34       8       48" x 48"       RIGHT SHOULDER CLOSED       34       8       8       8       8       9       34       8       8       8       8       9       9       34       8       8       8       8       8       9       9       34       8       8       8       8       8       9       9       34       8       8       8       8       8       9       9       34       8       8       8       8       9       9       34       8       8       9       9       34       8       9       9       9       9       9				2		68
W20-7a       48" x 48"       FLAGGER       2       34       68         W20-7b       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       34         W21-2       36" x 36"       FRESH OIL       2       27       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5       48" x 48"       SHOULDER WORK       34       34         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       18       15         *******       12" x 36"       TYPE III OBJECT MARKER       15       15         *******       *******       TYPE I BARRICADE - 6 FT. SINGLE SIDED       2       20       40         *******       *******       *******       *******       40       40				_		
W20-7b       48" x 48"       BE PREPARED TO STOP       2       34       68         W21-1a       48" x 48"       WORKERS (SYMBOL)       34       34         W21-2       36" x 36"       FRESH OIL       2       27       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5       48" x 48"       SHOULDER WORK       34       34         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       34         SPECIAL       30" x 24"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       18       15         *******       12" x 36"       TYPE III OBJECT MARKER       15       15         *******       *******       TYPE I BARRICADE - 6 FT. SINGLE SIDED       2       20       40         *******       *******       TYPE III BARRICADE - 8 FT. SINGLE SIDED       40       40				2		68
W21-1a       48" x 48"       WORKERS (SYMBOL)       34         W21-2       36" x 36"       FRESH OIL       2       27       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5       48" x 48"       SHOULDER WORK       34       34         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       18       18         *******       12" x 36"       TYPE III OBJECT MARKER       15       15         *******       *******       TYPE I BARRICADE - 6 FT. SINGLE SIDED       2       20       40         *******       *******       TYPE III BARRICADE - 8 FT. SINGLE SIDED       40       40						
W21-2       36" x 36"       FRESH OIL       2       27       54         W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5       48" x 48"       SHOULDER WORK       34       34         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       18       18         *******       12" x 36"       TYPE III OBJECT MARKER       15       15         *******       *******       TYPE I BARRICADE - 6 FT. SINGLE SIDED       2       20       40         *******       *******       TYPE III BARRICADE - 8 FT. SINGLE SIDED       40       40				_		
W21-3       48" x 48"       ROAD MACHINERY AHEAD       2       34       68         W21-5       48" x 48"       SHOULDER WORK       34       34         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34       34         SPECIAL       30" x 24"       FINES DOUBLED       18       18         ******       12" x 36"       TYPE III OBJECT MARKER       15       15         *******       *******       TYPE I BARRICADE - 6 FT. SINGLE SIDED       2       20       40         *******       *******       TYPE III BARRICADE - 8 FT. SINGLE SIDED       40       40			,	2		54
W21-5       48" x 48"       SHOULDER WORK       34         W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34         SPECIAL       30" x 24"       FINES DOUBLED       18         ******       12" x 36"       TYPE III OBJECT MARKER       15         ******       TYPE I BARRICADE - 6 FT. SINGLE SIDED       2       20       40         ******       TYPE III BARRICADE - 8 FT. SINGLE SIDED       40       40						
W21-5a       48" x 48"       RIGHT SHOULDER CLOSED       34         W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34         SPECIAL       30" x 24"       FINES DOUBLED       18         ******       12" x 36"       TYPE III OBJECT MARKER       15         ******       TYPE I BARRICADE - 6 FT. SINGLE SIDED       2       20       40         ******       TYPE III BARRICADE - 8 FT. SINGLE SIDED       40       40				_		~~
W21-5b       48" x 48"       RIGHT SHOULDER CLOSED AHEAD       34         SPECIAL       30" x 24"       FINES DOUBLED       18         ******       12" x 36"       TYPE III OBJECT MARKER       15         ******       TYPE I BARRICADE - 6 FT. SINGLE SIDED       2       20       40         ******       TYPE III BARRICADE - 8 FT. SINGLE SIDED       40       40						
SPECIAL       30" x 24"       FINES DOUBLED       18         ******       12" x 36"       TYPE III OBJECT MARKER       15         ******       ******       TYPE I BARRICADE - 6 FT. SINGLE SIDED       2       20       40         ******       TYPE III BARRICADE - 8 FT. SINGLE SIDED       40       40						
*****       12" x 36"       TYPE III OBJECT MARKER       15         *****       *****       TYPE I BARRICADE - 6 FT. SINGLE SIDED       2       20       40         ******       TYPE III BARRICADE - 8 FT. SINGLE SIDED       40       40						
*****						
***** TYPE III BARRICADE - 8 FT. SINGLE SIDED 40				2		<b>4</b> 0
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SOUTH DAKOTA P 0281(00)59 9 10

## PERMANENT SIGN TABLE FOR DAVISON COUNTY ROAD 4

							SI	GN DATA						Р	OST D	ATA			
				SI	GN	OFFSET*	SIGN		8 P	2	<sub>∞</sub> ⊢ ∺	POST	REMOVE	SALVAGE	Each			Foot	
		SIGN	SIGN SIZE		REA	RIGHT/	<b>FACES</b>	DESCRIPTION OF WORK	TYPE 2 BACK TO BACK	TYPE ;	TYPE 3 OBJECT MARKER	LENGTHS (Ft)	AND	TRAFFIC	REF	3 Lb/Ft	4 X 4	4 X 6	
STATION	DESCRIPTION	CODE	(FT)	(Sc	qFt)	LEFT		52301 1131V 31 VV31V	F BAG	⊭	T OB MA	INSIDE OUTSIDE	RESET			CHANNEL	WOOD	WOOD	ı
			` '							632F2520	632F2530								
OUD AVE.	<i>()))))))))))))</i>	(////////	///////////////////////////////////////	002202	002202				00222010	00222020	COLLEGE	(//////////////////////////////////////	00210020	11023020	F				_
24+31	NPZ PENNANT	W14-3	4X 4X 3		5.60	6'L	SOUTH	Install new sign and support.										16	
136+00	NPZ PENNANT							begin existing SB						1					
137+66	NPZ PENNANT	W14-3	4X 4X 3		5.60	6'L		begin dot SB ZONE - ENDS 152+08										16	
161+90	NPZ PENNANT	W14-3	4X 4X 3		5.60	6'L	NORTH	begin dot NB ZONE - ENDS 147+47										16	
164+20	NPZ PENNANT						SOUTH	begin existing NB						1					
179+30	NPZ PENNANT						NORTH	begin existing SB						1					
181+51		W14-3	4X 4X 3		5.60	6'L	NORTH	begin dot SB ZONE - ENDS 189+57										16	
196+20	NPZ PENNANT	W14-3	4X 4X 3		5.60			ZONE ENDS210+84											
196+20	NPZ PENNANT	W14-3	4X 4X 3		5.60	6'L	NORTH	begin dot SB ZONE {THRU 201+02 & 203+57 BEGIN (255') TO} 210+84										16	
199+38		W14-3	4X 4X 3		5.60	6'L		begin dot NB ZONE - ENDS 191+58										16	
218+45	NPZ PENNANT							begin existing SB						1					
219+41		W14-3			5.60			begin dot SB ZONE - ENDS 226+43										16	
220+71		W14-3			5.60	6'L		begin dot NB ZONE - ENDS 206+05											
236+44		W14-3	4X 4X 3		5.60	6'L		begin dot NB ZONE - ENDS 226+47										16	
238+70	NPZ PENNANT							begin existing NB						1					
436+00	NPZ PENNANT							begin existing SB						1					
454+95		W14-3	4X 4X 3			6'L	SOUTH								Ш.				
458+35	NPZ PENNANT							begin existing SB						1	Ш.				Ĺ
459+78		W14-3	4X 4X 3		5.60	6'L		begin dot SB ZONE - ENDS 464+78							Щ.			16	
475+02		W14-3	4X 4X 3		5.60	6'L		begin dot NB ZONE - ENDS 469+83							Щ.			16	
478+00	NPZ PENNANT							begin existing NB						1	<u> </u>				
491+10	NPZ PENNANT							begin existing SB						1	Щ.				
490+32		W14-3	4X 4X 3		5.60	6'L		begin dot SB ZONE - ENDS 495+32							<u> </u>			16	
492+41		W14-3	4X 4X 3		5.60	6'L		ZONE ENDS 495+32						1	<u> </u>		igwdow		
502+00	NPZ PENNANT				<b>5</b> 00			begin existing SB							ــــــ		igwdown	40	<u> </u>
507+07		W14-3	4X 4X 3		5.60	6'L		begin dot NB ZONE - ENDS 502+07							<b>↓</b>	igsquare	igwdow	16	<u> </u>
507+50	NPZ PENNANT							begin existing NB						1	<b>↓</b>	igsquare	igwdow		<u> </u>
507+60		W14-3	4X 4X 3			6'L	SOUTH								—	igwdown	igwdapprox		<u> </u>
523+37	NPZ PENNANT	14/4/4			5.00			begin existing NB						1	<b>↓</b>	igsquare	igwdow	40	<u> </u>
523+92	NPZ PENNANT	W14-3	4X 4X 3		5.60	6'L		begin dot NB ZONE - ENDS 523+92							<b>↓</b>	igsquare	igwdow	16	<u> </u>
558+25	NPZ PENNANT	14/4/4	4)/ 4)/ 0		5.00	OII		begin existing SB						1	—	igwdown	igwdapprox		<u> </u>
563+40	NPZ PENNANT		4X 4X 3	-	5.60	6'L		750' ZONE INTO STOP						1	—	igwdapprox	igwdown		—
563+40		W14-3	4X 4X 3		5.60	6'L		begin dot SB ZONE - ENDS at STOP						4		igwdapprox	igwdot		_
570+90	NPZ PENNANT						SOUTH	begin existing NB						1	₩	igwdown	igwdown		_
															<del> </del>	<b>↓</b>	$\longmapsto$		_
															<u> </u>	igwdapprox	$\longmapsto$		_
															—	igwdapprox	igwdapprox		<u> </u>
															<b>—</b>	igwdapprox	igwdapsilon		<u> </u>
															Щ.		igwdown		<u> </u>
															Щ.	igsquare	igwdapprox		1
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	TOTALS T	THIS S	HEET	0.00	100.80				0	0	0		0	15	0	0	0	208	1

STATE	PROJECT	-	TOTAL SHEETS
SOUTH	D 0004/00\E0		
DAKOTA	P 0281(00)59	10	10

### PERMANENT SIGN TABLE FOR DAVISON COUNTY ROAD 41

	SIGN DATA													POST DATA									
						OFFSET*			× 10	2	ST ER	POST	REMOVE	SALVAGE	Each		F	oot					
	STATION	DESCRIPTION		SIGN SIZE (FT)		REA qFt)	RIGHT/ LEFT		DESCRIPTION OF WORK	TYPE 2 BACK TO BACK	TYPE 2	TYPE OBJE( MARK)	POST LENGTHS (Ft) INSIDE OUTSIDE	AND RESET	TRAFFIC SIGN	REF POST (	3 Lb/Ft CHANNEL	4 X 4 WOOD	4 X 6 WOOD	,			
	265 ST				632E3203	632E3205				632E2510	632E2520	632E2530		632E3520	110E5020	631E1650	632E1130	632E1500 6	32E1505				
		NPZ PENNANT	W14-3	4X 4X 3		5.60	6'L		begin dot EB ZONE - ENDS 138+42										16				
		NPZ PENNANT	10/4 4 0	4)/ 4)/ 0		5.00	OII		begin existing EB						1				40				
	123+22	NPZ PENNANT	VV14-3	4X 4X 3		5.60	6'L	NORTH	begin dot WB ZONE - ENDS 116+69						- 1	$\longrightarrow$			16				
	118+90 106+82	NPZ PENNANT NPZ PENNANT	\\/11 2	4Y 4Y 3		5.60	6'1	NODTH	begin existing WB begin dot EB ZONE - ENDS 112+94						- 1	$\vdash$			16	_			
	100+82	NPZ PENNANT	VV 14-3	4/ 4/ 3	<del> </del>	3.00	O L	NORTH	begin existing EB	+					1	$\longrightarrow$			10				
	104+20	NPZ PENNANT	W14-3	4X 4X 3	1		6'L	WEST	begin existing LB	+					· ·	_			-+				
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TOTALS THIS SHEET			HEET	0.00	16.80				0	0	0		0	3	0	0	0	48					
		ALS ALL SHI			1											ightarrow			256	_			