

STATE OF SOUTH DAKOTA
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED PROJECT 000I-469 INTERSTATE 90 W MEADE COUNTY

WIDEN MEDIAN SHOULDER, RESET GUARDRAIL AND INSTALL
HIGH TENSION CABLE GUARDRAIL BETWEEN SERVICE ROAD
AND INTERSTATE 90

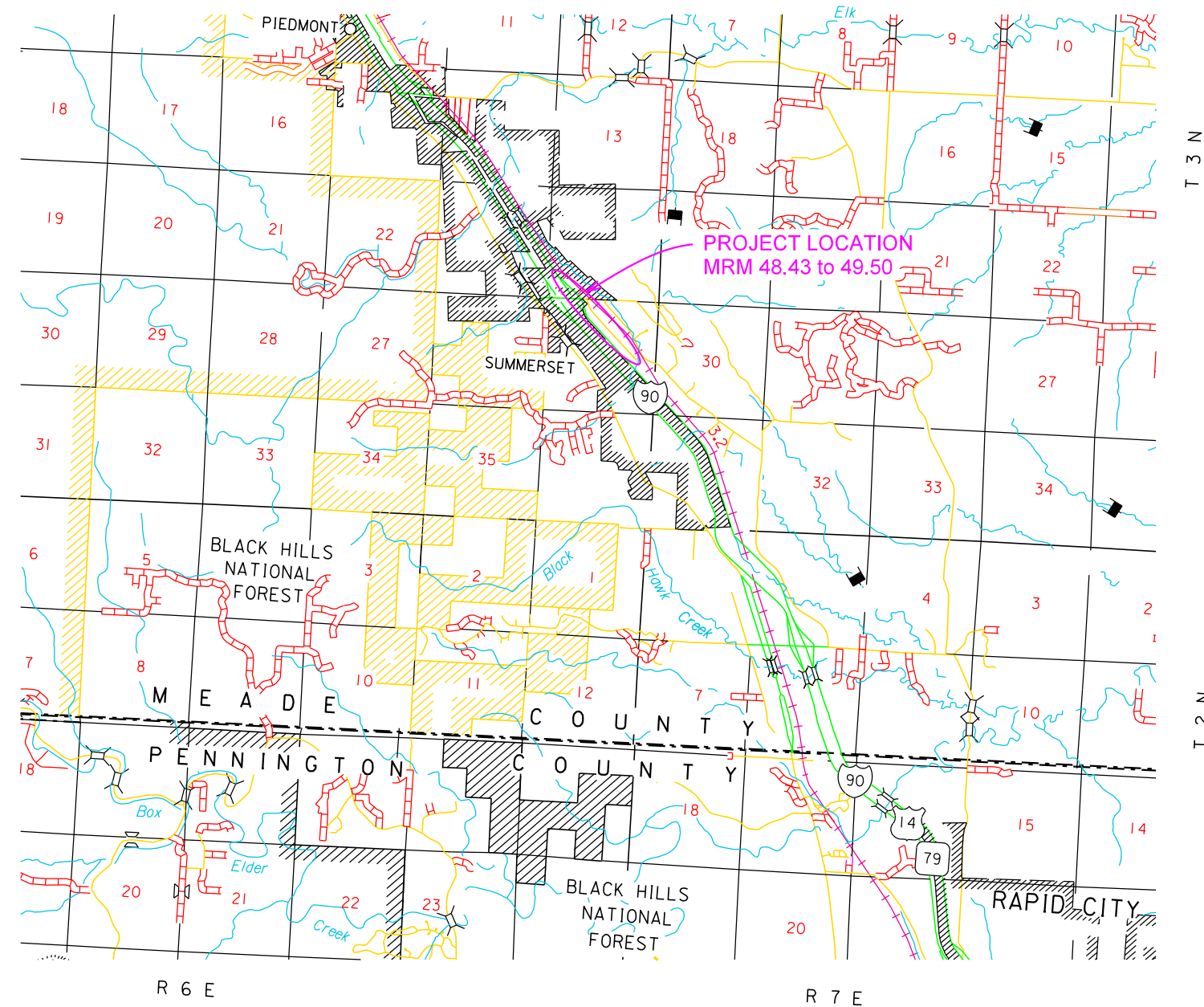
PCN i61c

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	0001-469	1	21

Plotting Date: 03/30/2020

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DESIGN DESIGNATION - EXIT 48

AADT (2019)	11070
AADT (2039)	14026
DHV	2060
D	51%
DHV T%	4.7%
AADT T%	10.4%
V	75 mph

STORM WATER PERMIT
None Required

ESTIMATE OF QUANTITIES

BID ITEM NUMBER	ITEM	QUANTITY	UNIT
009E0010	Mobilization	Lump Sum	LS
110E1010	Remove Asphalt Concrete Pavement	533.0	SqYd
110E6006	Remove High Tension 4 Cable Guardrail for Reset	398	Ft
110E6016	Remove High Tension 4 Cable Guardrail Anchor Assembly for Reset	2	Each
120E0010	Unclassified Excavation	150	CuYd
120E6200	Water for Granular Material	12.0	MGal
230E0100	Remove and Replace Topsoil	Lump Sum	LS
260E1010	Base Course	633.0	Ton
320E1200	Asphalt Concrete Composite	178.0	Ton
320E7012	Grind 12" Rumble Strip or Stripe in Asphalt Concrete	0.2	Mile
629E0110	High Tension Cable Guardrail	4,000	Ft
629E0211	Reset High Tension 4 Cable Guardrail	398	Ft
629E0290	High Tension Cable Guardrail Anchor Assembly	2	Each
629E0295	Reset High Tension Cable Guardrail Anchor Assembly	2	Each
633E1222	High Build Waterborne Pavement Marking Paint, 4" Yellow	1,200	Ft
634E0010	Flagging	100.0	Hour
634E0110	Traffic Control Signs	492.0	SqFt
634E0120	Traffic Control, Miscellaneous	Lump Sum	LS
634E0275	Type 3 Barricade	4	Each
634E0420	Type C Advance Warning Arrow Board	2	Each
634E0640	Temporary Pavement Marking	1,920	Ft
734E0154	12" Diameter Erosion Control Wattle	60	Ft

SPECIFICATIONS

Standard Specifications for Roads and Bridges, 2015 Edition and Required Provisions, Supplemental Specifications, and Special Provisions as included in the Proposal.

UTILITIES

The Contractor will contact the involved utility companies through South Dakota One Call (1-800-781-7474) prior to starting work. It will be the responsibility of the Contractor to coordinate work with the utility owners to avoid damage to existing facilities.

If utilities are identified near the improvement area through the SD One Call Process as required by South Dakota Codified Law 49-7A and Administrative Rule Article 20:25, the Contractor will contact the Project Engineer to determine modifications that will be necessary to avoid utility impacts.

ENVIRONMENTAL COMMITMENTS

The SDDOT is committed to protecting the environment and uses Section A Environmental Commitments as a communication tool for the Engineer and Contractor to ensure that attention is given to avoid, minimize, and/or mitigate an environmental impact. Environmental commitments to various agencies and the public have been made to secure approval of this project. An agency with permitting authority can delay a project if identified environmental impacts have not been adequately addressed. Unless otherwise designated, the Contractor's primary contact regarding matters associated with these commitments will be the Project Engineer. These environmental commitments are not subject to change without prior written approval from the SDDOT Environmental Office.

Additional guidance on SDDOT's Environmental Commitments can be accessed through the Environmental Procedures Manual found at: <http://www.sddot.com/resources/Manuals/EnvironProcManual.pdf>

For questions regarding change orders in the field that may have an effect on an Environmental Commitment, the Project Engineer will contact the Environmental Office at 605-773-3098 or 605-773-4336 to determine whether an environmental analysis and/or resource agency coordination is necessary.

COMMITMENT E: STORM WATER

Construction activities constitute less than 1 acre of disturbance.

Action Taken/Required:

At a minimum and regardless of project size, appropriate erosion and sediment control measures must be installed to control the discharge of pollutants from the construction site.

Construction activities constitute 1 acre or more of earth disturbance and/or work in a waterway.

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
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COMMITMENT H: WASTE DISPOSAL SITE

The Contractor will furnish a site(s) for the disposal of construction and/or demolition debris generated by this project.

Action Taken/Required:

Construction and/or demolition debris may not be disposed of within the Public ROW.

The waste disposal site(s) will be managed and reclaimed in accordance with the following from the General Permit for Construction/Demolition Debris Disposal Under the South Dakota Waste Management Program issued by the Department of Environment and Natural Resources.

The waste disposal site(s) will not be located in a wetland, within 200 feet of surface water, or in an area that adversely affects wildlife, recreation, aesthetic value of an area, or any threatened or endangered species, as approved by the Environmental Office and the Project Engineer.

If the waste disposal site(s) is located such that it is within view of any ROW, the following additional requirements will apply:

1. Construction and/or demolition debris consisting of concrete, asphalt concrete, or other similar materials will be buried in a trench completely separate from wood debris. The final cover over the construction and/or demolition debris will consist of a minimum of 1 foot of soil capable of supporting vegetation. Waste disposal sites provided outside of the Public ROW will be seeded in accordance with Natural Resources Conservation Service recommendations. The seeding recommendations may be obtained through the appropriate County NRCS Office. The Contractor will control the access to waste disposal sites not within the Public ROW with fences, gates, and placement of a sign or signs at the entrance to the site stating “No Dumping Allowed”.

2. Concrete and asphalt concrete debris may be stockpiled within view of the ROW for a period of time not to exceed the duration of the project. Prior to project completion, the waste shall be removed from view of the ROW or buried and the waste disposal site reclaimed as noted above.

The above requirements will not apply to waste disposal sites that are covered by an individual solid waste permit as specified in SDCL 34A-6-58, SDCL 34A-6-1.13, and ARSD 74:27:10:06.

Failure to comply with the requirements stated above may result in civil penalties in accordance with South Dakota Solid Waste Law, SDCL 34A-6-1.31.

All costs associated with furnishing waste disposal site(s), disposing of waste, maintaining control of access (fence, gates, and signs), and reclamation of the waste disposal site(s) will be incidental to the various contract items.

COMMITMENT I: HISTORICAL PRESERVATION OFFICE CLEARANCES

State Historical Preservation Office (SHPO or THPO) concurrence has been obtained for this project.

Action Taken/Required:

All earth disturbing activities outside the project limits will require a cultural resource review prior to scheduling the pre-construction meeting. This work includes, but is not limited to: Contractor furnished material sources, material processing sites, stockpile sites, storage areas, plant sites, and waste areas.

The Contractor will arrange and pay for a record search and when necessary, a cultural resource survey. The Contractor has the option to contact the state Archaeological Research Center (ARC) at 605-394-1936 or another qualified archaeologist, to obtain either a records search or a cultural resources survey. A record search might be sufficient for review if the site was previously surveyed; however, a cultural resources survey may need to be conducted by a qualified archaeologist.

The Contractor will provide ARC with the following: a topographical map or aerial view of which the site is clearly outlined, site dimensions, project number, and PCN. If applicable, provide evidence that the site has been previously disturbed by farming, mining, or construction activities with a landowner statement that artifacts have not been found on the site.

The Contractor will submit the cultural resources survey report to SDDOT Environmental Office, 700 East Broadway Avenue, Pierre, SD 57501-2586. SDDOT will submit the information to the appropriate SHPO/THPO. Allow **30 Days** from the date this information is submitted to the Environmental Engineer for SHPO/THPO review.

In the event of an inadvertent discovery of human remains, funerary objects, or if evidence of cultural resources is identified during project construction activities, then such activities will immediately cease and the Project Engineer will be immediately notified. The Project Engineer will contact the SDDOT Environmental Office to determine an appropriate course of action.

The Contractor is responsible for obtaining any additional permits and clearances for Contractor furnished material sources, material processing sites, stockpile sites, storage areas, plant sites, and waste areas that affect wetlands, threatened and endangered species, or waterways. The Contractor will not utilize a site known or suspected of having contaminated soil or water. The Contractor will provide the required permits and clearances to the Project Engineer at the preconstruction meeting.

UNCLASSIFIED EXCAVATION

Unclassified Excavation is provided on the project for removing embankment material behind the guardrail and placing this material for new surfacing materials in accordance with the typical sections. Any excess material will be handled as waste.

Plans quantity will be the basis of payment for the Unclassified Excavation quantity. If changes are made in the field during construction, measurements will be taken and the quantity will be adjusted accordingly.

WATER FOR EMBANKMENT AND GRANULAR MATERAIL COMPACTION

Water for compaction of earth embankments will be applied at the rate of 10 gallons per cubic yard of Unclassified Excavation.

Water for Granular Material will be placed at the rate of 12 gallons per ton of granular material for compaction.

A quantity of Water for Granular Material is provided for completing this work.

REMOVE AND RESET HIGH TENSION 4 CABLE GUARDRAIL

Prior to removal of the existing high tension cable guardrail for reset, the Contractor will document the existing layout. The Contractor will record the post spacing, height of guardrail, list the components needed for the anchor assembly and any other items necessary for re-installation. This information will be furnished to the Project Engineer prior to removal. All costs associated with this work will be incidental to the contract unit price per foot "Remove High Tension 4 Cable Guardrail for Reset".

HIGH TENSION CABLE GUARDRAIL

The Contractor will furnish and install a high tension guardrail system that meets the Test Level 3 crash testing requirements of National Cooperative Highway Research Program (NCHRP) 350 or current Manual for Assessing Safety Hardware (MASH). The maximum dynamic deflection of the system will be less than 8 feet and the maximum post spacing will be 16 feet unless specified otherwise in the plans.

The high tension cable guardrail system will be in compliance with Specifications Section 6.9 Buy America.

The Contractor will install the system according to the manufacturer's installation recommendations except where stated otherwise in the plans. A copy of the detail drawings and installation instructions for the high tension cable guardrail and anchor assemblies will be given to the Engineer a minimum of 4 weeks prior to installation of the high tension cable guardrail system.

All posts will be galvanized and inserted into driven galvanized steel sleeves with soil plates.

Delineation of the high tension cable guardrail will be in conformance with standard plate 632.40.

The cables provided will be pre-stretched in the factory.

The Contractor will check and adjust the tension of the cables a minimum of 3 weeks after installation and not longer than 6 weeks after installation. Cost for this work will be incidental to the contract unit price per foot for High Tension Cable Guardrail.

The lengths of high tension cable guardrail stated in the plans were based on a non-effective length of 26' at each end of the "run" of guardrail when installed adjacent to two-way traffic roadways. The length and location of the high tension cable guardrail at each site will need to be adjusted during construction as necessary if a system with a different non-effective length is used and it will be approved by the Design Engineer before installation.

The Contractor will provide a signed letter of compliance to the Engineer upon completion of the high tension cable guardrail installation(s) stating that the high tension cable barrier system has been installed in conformance to the installation instructions, specifications, and at a minimum meets the Test Level 3 crash test requirements of NCHRP 350 or MASH.

The high tension cable guardrail will be measured along the centerline of the cable guardrail from center of anchor assembly to center of anchor assembly to the nearest foot.

All costs for furnishing and installing the high tension cable guardrail system including all labor, materials, and equipment will be incidental to the contract unit price per foot for High Tension Cable Guardrail.

HIGH TENSION CABLE GUARDRAIL ANCHOR ASSEMBLY

The beginning and end of each "run" of high tension cable guardrail will terminate with an anchor assembly that meets the Test Level 3 crash testing requirements of NCHRP 350 or MASH.

The footing(s) for the anchor assembly will be designed to allow for 1 inch maximum of lateral deflection. The allowable design soil pressure will be 1000 psf. The top 2 feet of soil pressure will be neglected in the design of the footing(s). The footing(s) will be a minimum of 5' deep. The footing(s) design will be submitted through proper channels to the Office of Bridge Design for a one-time approval. Any changes to the anchor assembly that could affect footing size including configuration changes such as different number of cables and different number of footings will be resubmitted for approval. The approval will be obtained a minimum of 4 weeks prior to construction of the anchor footing(s).

Delineation of the high tension cable guardrail anchor assembly will be in conformance with standard plate 632.40.

All costs for furnishing and installing the High Tension Cable Guardrail Anchor Assembly including all labor, equipment, and materials which include the anchor footing(s), hardware, and all attachments to the anchor footing(s), will be incidental to the contract unit price per each for High Tension Cable Guardrail Anchor Assembly.

TABLE OF MATERIAL QUANTITIES

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	000I-469	4	21

Location	Remove High Tension 4 Cable Guardrail for Reset Ft	Remove High Tension 4 Cable Anchor Assembly for Reset Each	Reset High Tension Cable Guardrail Anchor Assembly Ft	Reset High Tension Cable Guardrail Anchor Assembly Each	High Tension Cable Guardrail (Ft)	High Tension Cable Guardrail Assembly (Each)	Remove Asphalt Concrete Pavement SqYd	Unclassified Excavation CuYd	Base Course Ton	Asphalt Concrete Composite Ton
MRM 48.43 WB Median Bridge Columns	398	2	398	2	4000	2	533	150	633	178
MRM 48.43 to 49.6 WB										

REMOVE AND REPLACE TOPSOIL

Topsoil will be salvaged and stockpiled prior to constructing the guardrail embankment. Limits of this work, depth of salvage, and stockpile location will be directed by the Engineer. Following completion of construction, topsoil will be spread evenly over the disturbed areas.

All costs associated with removing and replacing the topsoil will be incidental to the contract lump sum price for "Remove and Replace Topsoil".

EROSION CONTROL

All costs for the erosion control work for furnishing, placing, and maintaining erosion control including equipment, labor, permanent seeding, fertilizing, and fiber mulching will be incidental to the contract lump sum price for "Erosion Control".

The limits of erosion control work will be for all locations disturbed during construction. These limits will be determined by the Engineer during construction.

Type F Permanent Seed Mixture will consist of the following:

Grass Species	Variety	Pure Live Seed (PLS) (Pounds/Acre)
Western Wheatgrass	Arriba, Flintlock, Rodan, Rosana, Walsh	7
Green Needlegrass	Lodorm, AC Mallard Ecovar	4
Sideoats Grama	Butte, Pierre	3
Blue Grama	Bad River	2
Oats or Spring Wheat: April through May; Winter Wheat: August through November		10
Total:		26

Mycorrhizal inoculum will consist of mycorrhizal fungi spores and mycorrhizal fungi-infected root fragments in a solid carrier. The carrier may include organic materials, calcinated clay, or other materials consistent with application and good plant growth. The supplier will provide certification of the fungal species claimed and the live propagule count. The inoculum will include the following fungal species:

- 25% *Glomus intraradices*
- 25% *Glomus aggregatum or deserticola*
- 25% *Glomus mosseae*
- 25% *Glomus etunicatum*

All seed will be inoculated by the seed supplier with a minimum of 100,000 live propagules of mycorrhizal fungi per acre. All costs of inoculating the seed will be incidental to the contract lump sum price for Erosion Control.

The mycorrhizal inoculum will be as shown below or an approved equal:

Product	Manufacturer
MycoApply	Mycorrhizal Applications, Inc. Grants Pass, OR Phone: 1-866-476-7800 www.mycorrhizae.com
AM 120 Multi Species Blend	Reforestation Technologies Int. Gilroy, CA Phone: 1-800-784-4769 www.reforest.com

The Contractor will apply an all-natural slow release fertilizer prior to seeding or placing sod. The all-natural fertilizer will have a minimum guaranteed analysis of 4-4-4 and be USDA Certified BioBased. It should provide a minimum of 4% (N) nitrogen with a minimum water insoluble nitrogen (WIN) fraction of 2.07%, a minimum of 4% (P2O5) available phosphate, a minimum of 4% (K2O) soluble potash, and a maximum carbon to nitrogen ratio (C:N ratio) of 5:1. The all-natural fertilizer will be free of weed-seed and pathogens accomplished through thermophilic composting, and not mechanical or chemical sterilization, to assure presence of beneficial soil microbiology. The fertilizer will have a near neutral pH, a low salt index, a low biological oxygen demand, contain organic humic and fulvic acids, and have high aerobic organism counts. The fertilizer will also be stable, free of bad odors, and be unattractive as a food source for animals. It should also be in a granular form that is easily spread.

The fertilizer will be applied at a rate of 1,500 pounds per acre in accordance with the manufacturer's recommended method of application.

The all-natural slow release fertilizer will be as shown below or an approved equal:

Product	Manufacturer
Sustane	Sustane Corporate Headquarters Cannon Falls, Minnesota Phone: 1-800-352-9245 www.sustane.com
Perfect Blend	Perfect Blend, LLC Bellevue, WA Phone: 1-866-456-8890 www.perfect-blend.com

Fiber mulch will be applied in a separate operation following permanent seeding.

An additional 2% by weight of tackifier will be added to the fiber mulch product selected from the approved product list. If the product selected has guar gum tackifier included, then the additional 2% of tackifier will be guar gum. If the product selected has synthetic tackifier included, then the additional 2% of tackifier will be synthetic.

The Contractor will allow the fiber mulch to cure a minimum of 18 hours prior to watering or any storm event to ensure proper cohesion between the soil and fiber particles.

All costs for the additional tackifier added to the fiber mulch including labor, equipment, and materials will be incidental to the contract lump sum price for Erosion Control.

The fiber mulch provided will be from the approved product list. The approved product list for fiber mulch may be viewed at the following internet site:

<http://sddot.com/business/certification/products/Default.aspx>

EROSION CONTROL WATTLE

The Contractor will provide certification that the erosion control wattles do not contain noxious weed seeds.

Erosion control wattles will remain on the project to decompose.

(60') of 12" Diameter Erosion Control Wattles are provided for temporary erosion and sediment control in highway ditch channels to prevent sediment from leaving the project limits.

The erosion control wattle provided will be from the approved product list. The approved product list for erosion control wattle may be viewed at the following internet site:

http://sddot.com/business/certification/products/Default.aspx

HIGH BUILD WATERBORNE PAVEMENT MARKING PAINT

All materials will be applied as per manufacturer's recommendations.

This material will consist of a durable high build, low VOC, fast drying, waterborne traffic paint with a 100% acrylic polymer (Arkema DT-400, Dow HD-21A, or equivalent). The Contractor will provide certification that the material is one of the following products or an equivalent as approved by the Operations Traffic Engineer:

- Diamond Vogel's Waterborne High Build Polymer Marking Paint
- Ennis-Flint's High Build Polymer Marking Paint

No further testing of this material will be required. Reflective media will consist of glass beads.

RATES OF MATERIALS FOR HIGH BUILD WATERBORNE PAVEMENT MARKING PAINT

Solid 4" line = 27.8 Gals/Mile
Glass Beads = 8 Lbs/Gal.

All cost for materials, labor and equipment necessary to furnish and install the pavement markings will be incidental to the contract unit price for the respective High Build Waterborne Pavement Marking Paint items.

SEQUENCE OF OPERATIONS

The Contractor will submit a sequence of operations for approval two weeks prior to the preconstruction meeting. Approval of an alternate sequence of operations will only be allowed when the proposed changes meet with the Department's intent for traffic control and sequencing of the work. An alternate sequence will be submitted for review a minimum of one week prior to potential implementation.

The guardrail work along the service road will be completed from the service road side. Lane closures will not be allowed on I-90 for the installation of guardrail along the service road.

GENERAL TRAFFIC CONTROL

Existing guide, route, informational logo, regulatory, and warning signs will be temporarily reset and maintained during construction. Removing, relocating, covering, salvaging, and resetting of existing traffic control devices, including delineation, will be the responsibility of the Contractor. Cost for this work will be incidental to the contract unit prices for the various items unless otherwise specified in the plans. Any delineators and signs damaged or lost will be replaced by the Contractor at no cost to the State.

All temporary traffic control sign locations will be set in the field by the Contractor and verified by the Engineer prior to installation.

All temporary speed limit signs will have a minimum mounting height of 5 feet in rural locations, even when mounted on portable supports.

All construction operations will be conducted in the general direction of traffic movement.

If there is a discrepancy between the traffic control plans, standard plates, and the MUTCD, whichever is more stringent will be used, as determined by the Engineer.

Unless otherwise stated in these plans, work will not be allowed during hours of darkness.

Traffic Control Signs, as shown in the Estimate of Quantities, are estimates. Contractor's operation may require adjustments in quantities, either more or less. Payment will be for those signs actually ordered by the Engineer and used.

Fixed location signing placed more than 4 calendar days prior to the start of construction will be covered or laid down until the time of construction. The covers must be approved by the Engineer prior to installation. The cost of materials, labor, and equipment necessary to complete this work will be incidental to other contract items. No separate payment will be made.

All fixed location signs, sign posts, and breakaway bases will be removed within 7 calendar days following pavement marking.

All haul trucks will be equipped with an additional flashing amber light that is visible from the backside of the haul truck. The costs for the flashing amber lights will be incidental to the various related contract bid items.

Traffic will be maintained on the driving lanes. Use of the shoulder as a driving lane will not be permitted. Any damage to the shoulder due to rerouted traffic or Contractor's equipment will be repaired at no expense to the Department.

Construction vehicles will exit or enter the construction work zone at locations identified by the Engineer. At no time will construction vehicles utilize the maintenance crossovers or the Interstate median to exit or enter Interstate traffic.

Temporary Flexible Vertical Markers (Tabs) will be used for lane closure tapers or lane shift tapers and will be installed at 5' spacing.

Routing traffic onto the mainline shoulders during any phase of the construction will not be allowed.

Damage to the shoulders, median, or ditch due to the Contractor's operations will be repaired by the Contractor to the satisfaction of the Engineer at no expense to the State. This includes the apparent routing of traffic onto the shoulders around the work zones.

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	000I-469	5	21

TRAFFIC CONTROL SIGNS

Sufficient traffic control devices have been included in these plans to sign one workspace on each route. If the Contractor elects to work on additional locations simultaneously, the cost for additional traffic control devices will be incidental to the contract unit price per square foot for "Traffic Control Signs".

WORK ZONE SPEED REDUCTION

The Department is required to obtain a speed reduction resolution prior to the installation of any SPEED LIMIT (R2-1) signs shown on standard plate 634.63 or as shown in the plans. To provide adequate time for the resolution to be enacted, the Contractor will inform the Engineer a minimum of 3 weeks prior to the scheduled installation of any work zone speed reduction signs on the project. The information provided by the Contractor will include the anticipated date of sign installation, the newly reduced speed limit, the location of the work zone, and the anticipated completion date of work requiring the speed reduction.

PRESS RELEASE ANNOUNCEMENTS

The SDDOT will prepare a press release to be released 5 days prior to any phase change or any other major change that affects traffic flow. The SDDOT will be responsible to keep law enforcement, emergency services, and the traveling public notified of changes in project access. The Contractor will provide the Engineer with pertinent information 7 days prior to any phase change or any other major change that affects traffic flow.

INVENTORY OF TRAFFIC CONTROL DEVICES

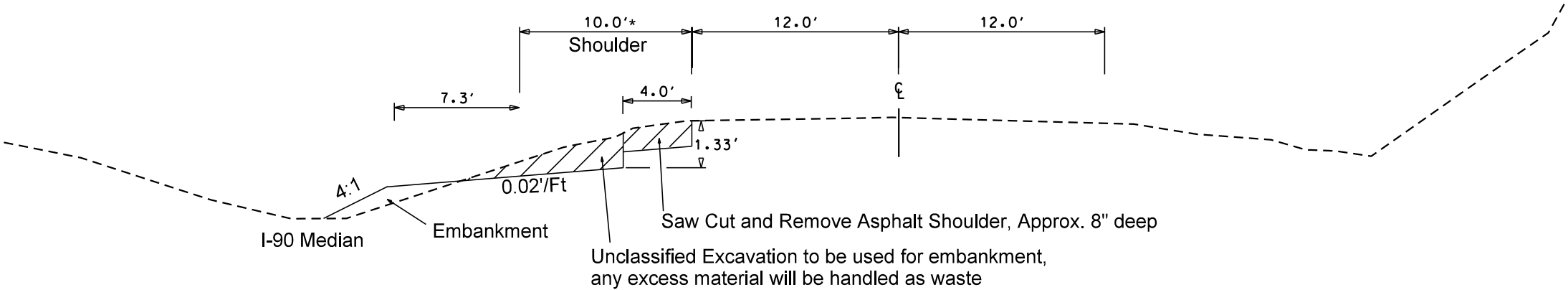
SIGN CODE	SIGN DESCRIPTION	NUMBER	SIGN SIZE	SQFT PER SIGN	SQFT
R2-1	SPEED LIMIT 65	6	36" x 48"	12.0	72.0
R2-1	SPEED LIMIT 45	4	36" x 48"	12.0	48.0
R2-1	SPEED LIMIT 75	2	36" x 48"	12.0	24.0
R2-6aP	FINES DOUBLE (plaque)	2	36" x 24"	6.0	12.0
W3-5	SPEED REDUCTION AHEAD (45 MPH)	2	48" x 48"	16.0	32.0
W3-5	SPEED REDUCTION AHEAD (65 MPH)	4	48" x 48"	16.0	64.0
W4-2	LEFT or RIGHT LANE ENDS (symbol)	4	48" x 48"	16.0	64.0
W20-1	ROAD WORK AHEAD	4	48" x 48"	16.0	64.0
W20-5	LEFT or RIGHT LANE CLOSED AHEAD	4	48" x 48"	16.0	64.0
W20-7	FLAGGER (symbol)	2	48" x 48"	16.0	32.0
G20-2	END ROAD WORK	2	48" x 24"	8.0	16.0
		EXPRESSWAY / INTERSTATE TRAFFIC CONTROL SIGNS SQFT			492.0

TYPICAL SECTIONS

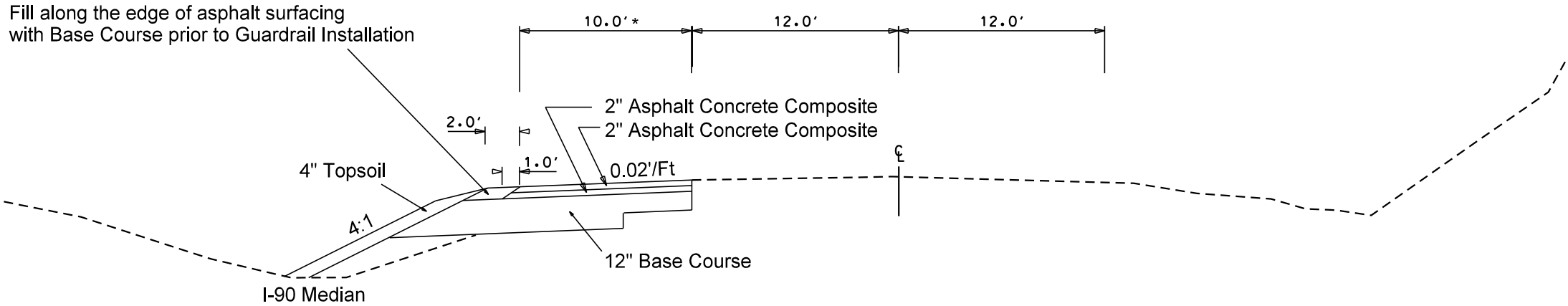
11+00 to 23+00
I-90 Westbound

*Transition width from 4' to 10' from 11+00 to 13+54
*Transition width from 10' to 4' from 17+52 to 23+00

GRADING SECTION



SURFACING SECTION



1:200
Plot Scale -
Plotted From -

LEGEND

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	000I-469	7	21

Plotting Date: 11/20/2019

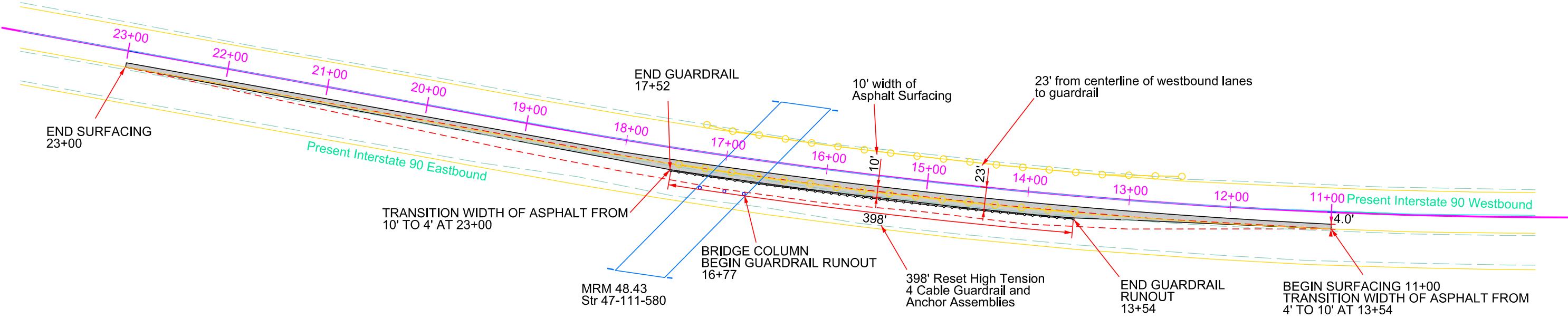
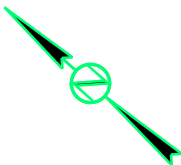
Anchor		Hedge		Septic Tank		State and National Line	
Antenna		Highway ROW Marker		Shrub Tree		County Line	
Approach		Interstate Close Gate		Sidewalk		Section Line	
Assumed Corner		Iron Pin		Sign Face		Quarter Line	
Azimuth Marker		Irrigation Ditch		Sign Post		Sixteenth Line	
BBQ Grill/ Fireplace		Lake Edge		Slough Or Marsh		Property Line	
Bearing Tree		Lawn Sprinkler		Spring		Construction Line	
Bench Mark		Mailbox		Stream Gauge		ROW Line	
Box Culvert		Manhole Electric		Street Marker		New ROW Line	
Bridge		Manhole Gas		Subsurface Utility Exploration Test Hole		Cut and Fill Limits	
Brush		Manhole Miscellaneous		Telephone Fiber Optics		Control of Access	
Buildings		Manhole Sanitary Sewer		Telephone Junction Box		New Control of Access	
Bulk Tank		Manhole Storm Sewer		Telephone Pole		Proposed ROW	
Cattle Guard		Manhole Telephone		Television Cable Jct Box		(After Property Disposal)	
Cemetery		Manhole Water		Television Tower			
Centerline		Merry-Go-Round		Test Wells/Bore Holes		Drainage Arrow	
Cistern		Microwave Radio Tower		Traffic Signal			
Clothes Line		Miscellaneous Line		Trash Barrel			
Commercial Sign Double Face		Miscellaneous Property Corner		Tree Belt			
Commercial Sign One Post		Miscellaneous Post		Tree Coniferous		Remove Concrete Pavement	
Commercial Sign Overhead		Overhang Or Encroachment		Tree Deciduous		Remove Concrete Driveway Pavement	
Commercial Sign Two Post		Overhead Utility Line		Tree Stumps		Remove Asphalt Concrete Pavement	
Concrete Symbol		Parking Meter		Triangulation Station		Remove Concrete Sidewalk	
Control Point		Pedestrian Push Button Pole		Underground Electric Line		Remove Concrete Median Pavement	
Creek Edge		Pipe With End Section		Underground Gas Line		Remove Concrete Curb and/or Gutter	
Curb/Gutter		Pipe With Headwall		Underground High Pressure Gas Line			
Curb		Pipe Without End Section		Underground Sanitary Sewer			
Dam Grade/Dike/Levee		Playground Slide		Underground Storm Sewer			
Deck Edge		Playground Swing		Underground Tank			
Ditch Block		Power And Light Pole		Underground Telephone Line			
Doorway Threshold		Power And Telephone Pole		Underground Television Cable			
Drainage Profile		Power Meter		Underground Water Line			
Drop Inlet		Power Pole		Warning Sign One Post			
Edge Of Asphalt		Power Pole And Transformer		Warning Sign Two Post			
Edge Of Concrete		Power Tower Structure		Water Fountain			
Edge Of Gravel		Propane Tank		Water Hydrant			
Edge Of Other		Property Pipe		Water Meter			
Edge Of Shoulder		Property Pipe With Cap		Water Tower			
Electric Transformer/Power Junction Box		Property Stone		Water Valve			
Fence Barbwire		Public Telephone		Water Well			
Fence Chainlink		Railroad Crossing Signal		Weir Rock			
Fence Electric		Railroad Milepost Marker		Windmill			
Fence Miscellaneous		Railroad Profile		Wingwall			
Fence Rock		Railroad ROW Marker		Witness Corner			
Fence Snow		Railroad Signs					
Fence Wood		Railroad Switch					
Fence Woven		Railroad Track					
Fire Hydrant		Railroad Trestle					
Flag Pole		Rebar					
Flower Bed		Rebar With Cap					
Gas Valve Or Meter		Reference Mark					
Gas Pump Island		Regulatory Sign One Post					
Grain Bin		Regulatory Sign Two Post					
Guardrail		Retaining Wall					
Guide Sign One Post		Riprap					
Guide Sign Two Post		River Edge					
Gutter		Rock And Wire Baskets					
Guy Pole		Rockpiles					
Haystack		Satellite Dish					

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	000I-469	8	21

Plotting Date: 03/03/2020

MRM 48.43
Westbound
Str 47-111-580

Remove and Reset High Tension Guardrail
and Anchor Assemblies - 398'
13+54 L to 17+52 L



4" Asphalt Concrete Composite & 12" Base Course

Plot Scale - 1:100

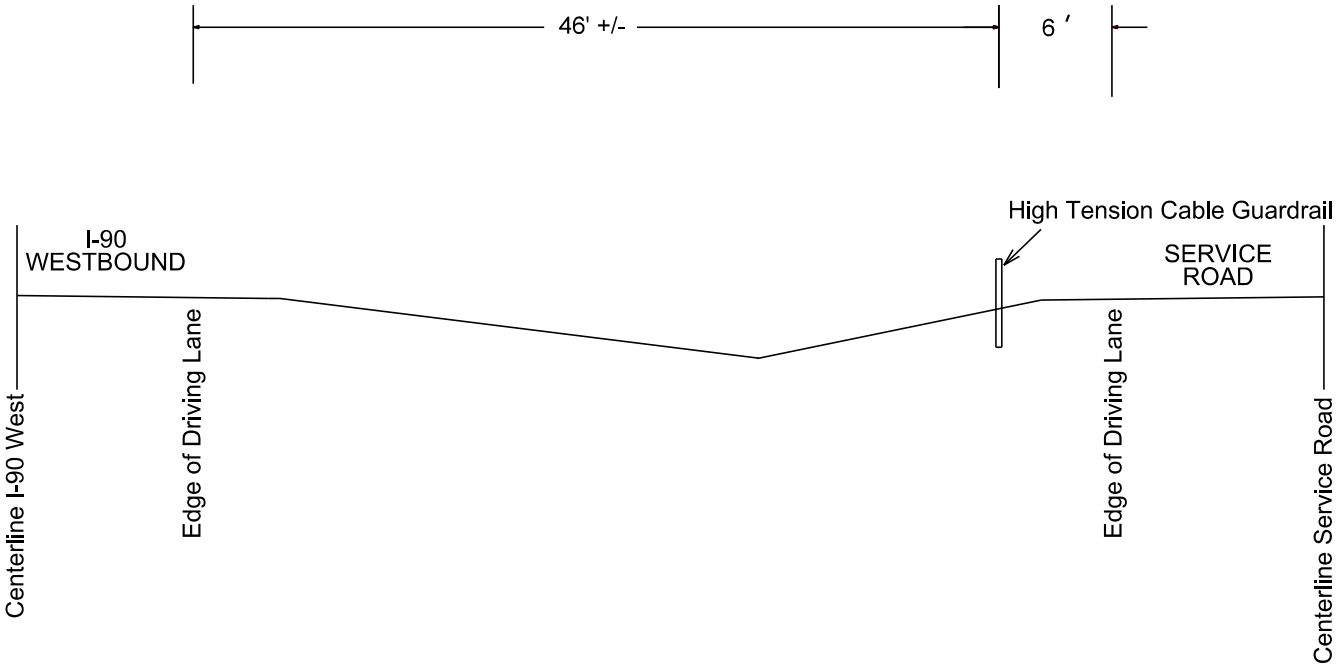
Plotted From - TRR011951

File - ...PlanSheet.dgn

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	000I-469	9	21

Plotting Date: 03/30/2020

SERVICE ROAD HIGH TENSION CABLE GUARDRAIL TYPICAL SECTION



Ditch Cross-Section High Tension Cable Guardrail Installation Detail

HIGH TENSION GUARDRAIL LAYOUT

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	0001-469	10	21
Plotting Date: 04/02/2020			

Exit 48

MRM 48 +0.375

High Tension Guardrail

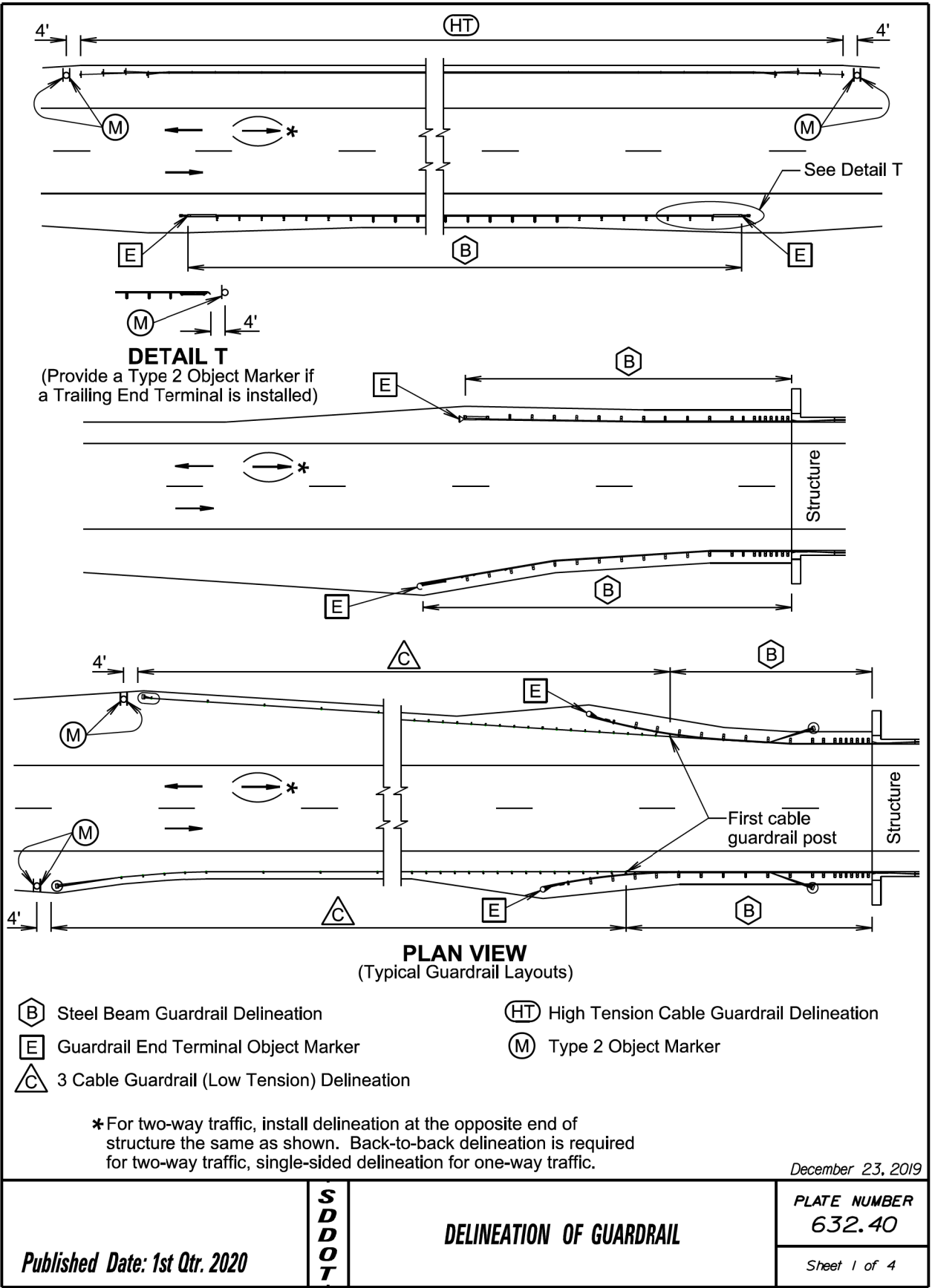
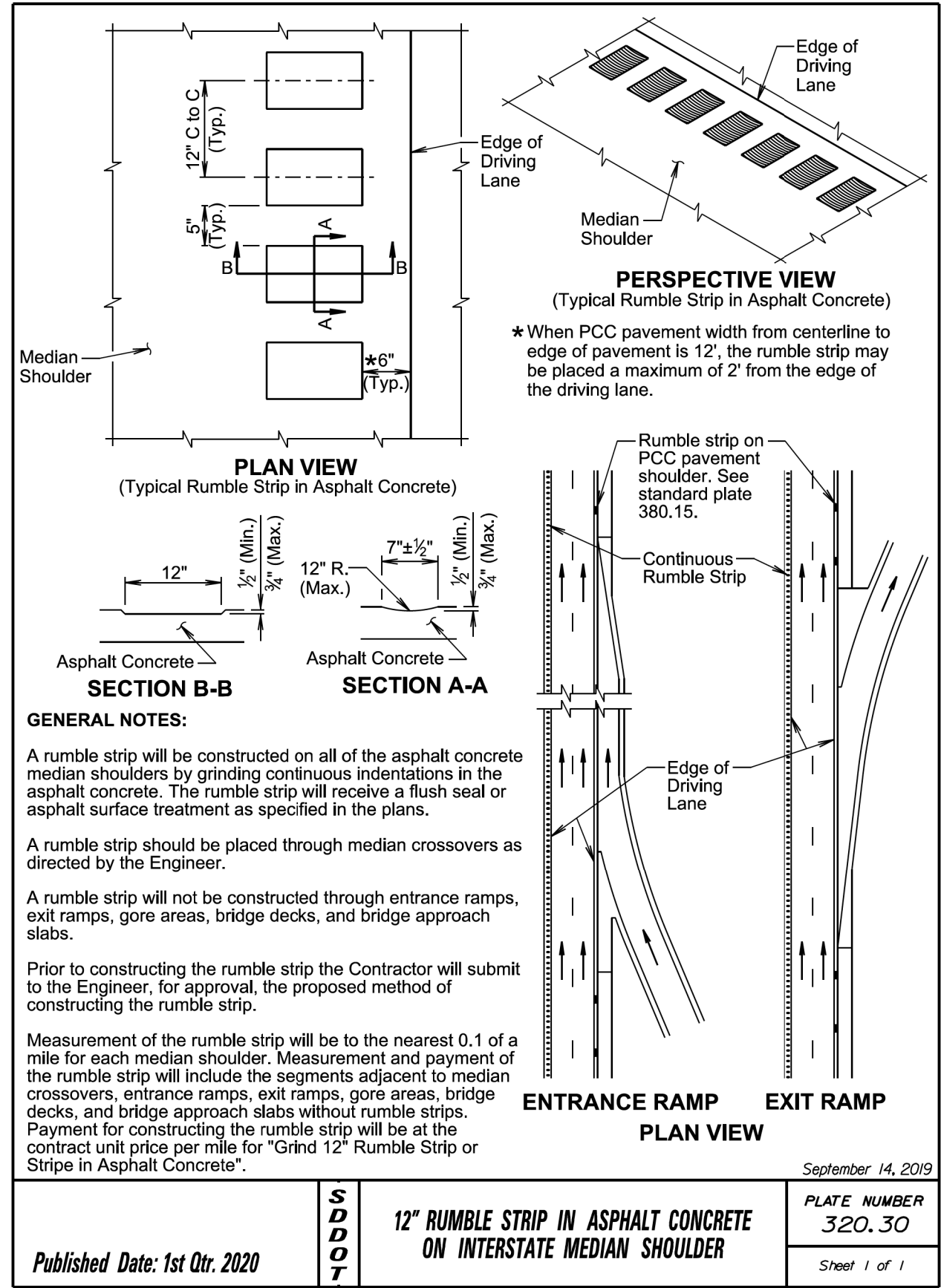


MRM 49+0.593

Plot Scale - 1:100

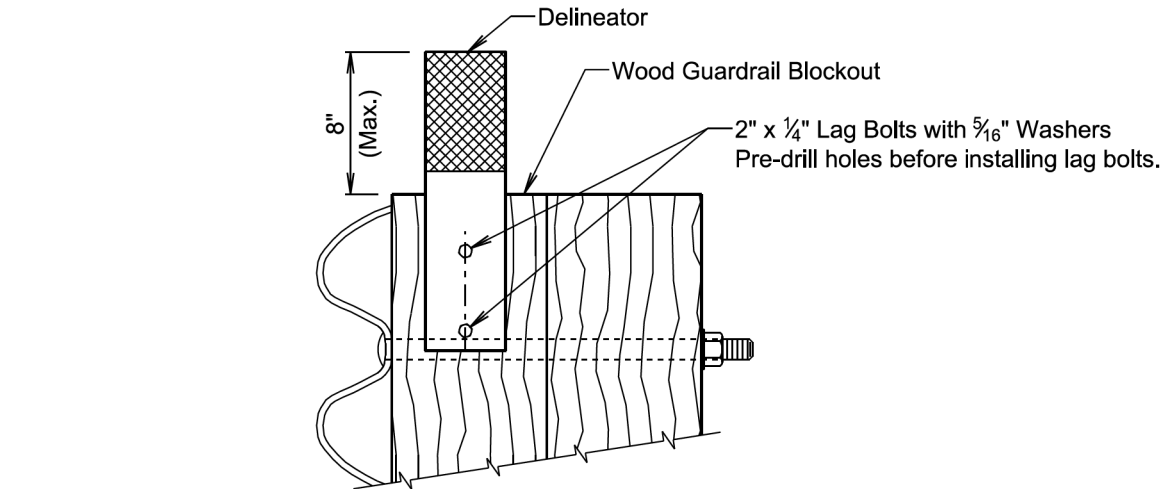
Plotted From - TRRC11951

File - ... Cable Rail Layouts.dgn

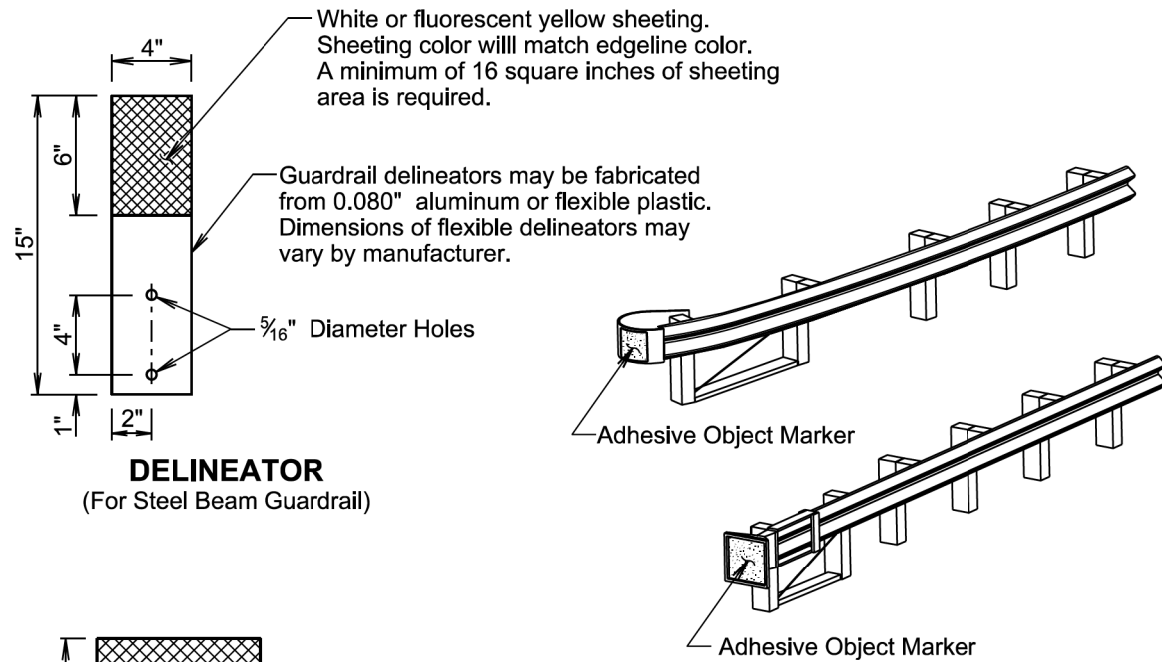


STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	000I-469	12	21

Plotting Date: 03/30/2020

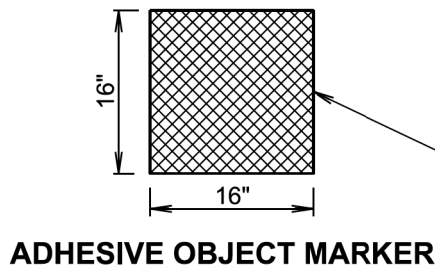


B STEEL BEAM GUARDRAIL DELINEATION



DELINEATOR
(For Steel Beam Guardrail)

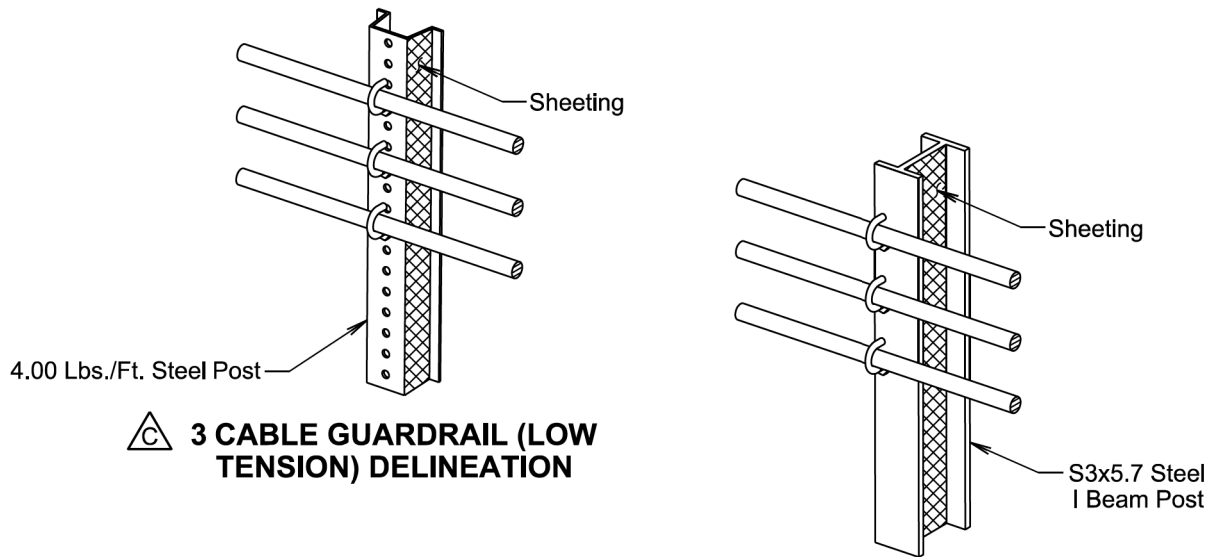
E GUARDRAIL END TERMINAL OBJECT MARKER



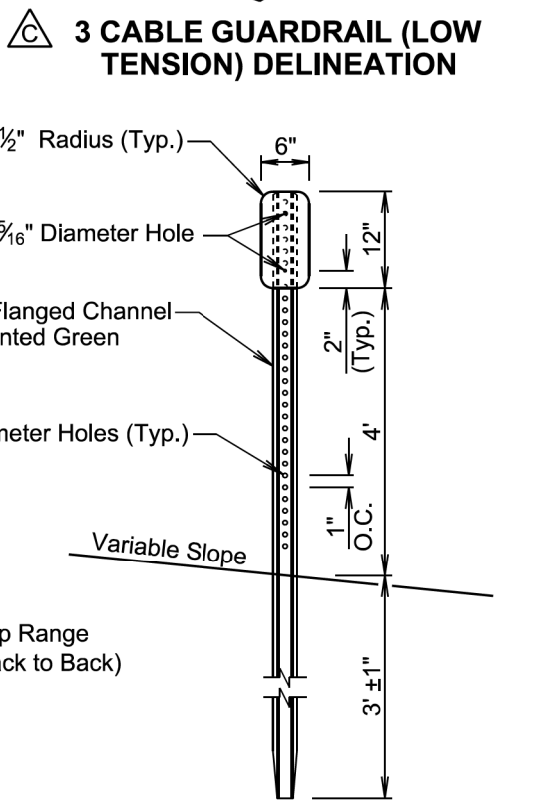
Adhesive object marker dimensions may vary due to shape of terminal end. A minimum of 256 square inches of object marker sheeting area is required. The sheeting will be fluorescent yellow.

December 23, 2019

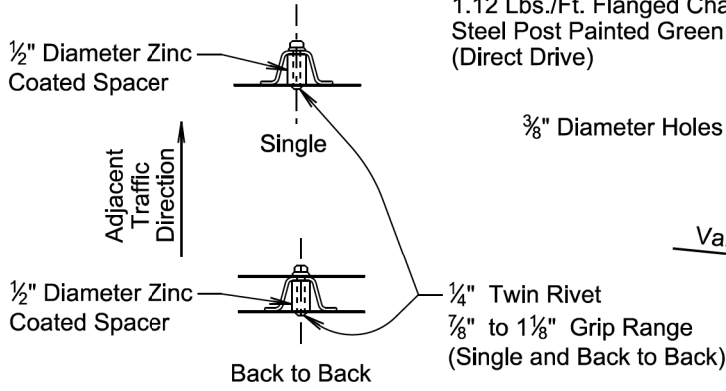
Published Date: 1st Qtr. 2020	S D D O T	DELINEATION OF GUARDRAIL	PLATE NUMBER 632.40
			Sheet 2 of 4



C 3 CABLE GUARDRAIL (LOW TENSION) DELINEATION



C 3 CABLE GUARDRAIL (LOW TENSION) DELINEATION



PLAN VIEW
(Type 2 Object Marker Details and Post Orientation)

ELEVATION VIEW
(Type 2 Object Marker)
(For Marking 3 Cable Guardrail (Low Tension) Anchor, High Tension Cable Guardrail Anchor, and Trailing End Terminal)

December 23, 2019

Published Date: 1st Qtr. 2020	S D D O T	DELINEATION OF GUARDRAIL	PLATE NUMBER 632.40
			Sheet 3 of 4

STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	000I-469		

Plotting Date: 03/30/2020

GENERAL NOTES:

The delineation of high tension cable guardrail will be reflective sheeting placed back to back on every other post cap or cable spacer. The sheeting will be type XI in conformance with ASTM D4956. The color of the reflective sheeting shall be the same as the nearest pavement marking.

The delineators for steel beam guardrail and sheeting on 3 cable guardrail (low tension) posts will be covered with a minimum of 16 square inches of reflective sheeting. The reflective sheeting will be type XI in conformance with ASTM D4956. Along two-way roadways the sheeting will be on both sides of the delineators and guardrail posts and will be white in color. For one-way roadways the sheeting will only be required on the side facing traffic and the color will be the same as the nearest pavement marking, yellow on the left side of the roadway and white on the right side.

When steel beam guardrail is attached to a bridge the first delineator will be attached to the post nearest the bridge.

At bridges with guardrail less than 200 feet in length, a minimum of 4 delineators will be placed in addition to the end terminal yellow object marker. The spacing between the delineators will be approximately one third of the length of the guardrail.

At bridges with guardrail 200 feet and greater in length, including bridges that have steel beam guardrail transitioning to 3 cable guardrail (low tension), the delineators will be placed at a spacing of approximately 50 feet. Delineation will extend throughout the length of the guardrail system.

Steel beam guardrail that is not attached to a bridge and is less than 200 feet in length, a minimum of 4 delineators will be placed in addition to the end terminal yellow object markers. The spacing between the delineators will be approximately one third of the length of the guardrail.

Steel beam guardrail that is not attached to a bridge and is 200 feet and greater in length, including steel beam guardrail transitioning to 3 cable guardrail (low tension), the delineators will be placed at a spacing of approximately 50 feet. Delineation will extend throughout the length of the guardrail system.

All costs for furnishing and installing single or back to back guardrail delineation on 3 cable guardrail and steel beam guardrail will be included in the contract unit price per each for "Guardrail Delineator".

All costs for furnishing and installing the reflective sheeting on the cable spacers or post caps for the high tension cable guardrail will be incidental to the respective high tension cable guardrail contract item.

An adhesive object marker will be placed on the end of the W beam guardrail or MGS end terminal. The adhesive object marker dimensions may vary due to the shape of the terminal end. A minimum of 256 square inches of object marker reflective sheeting area is required. The reflective sheeting will be fluorescent yellow type XI sheeting in conformance with ASTM D4956. All costs for furnishing and installing the adhesive object marker will be incidental to various contract items.

A type 2 object marker will be placed adjacent to the 3 cable guardrail (low tension) anchor, high tension cable guardrail anchor, and trailing end terminal at the location noted on sheet 1 of this standard plate. The type 2 object marker (6" x 12") will have fluorescent yellow type XI sheeting in conformance with ASTM D4956. All costs for furnishing and installing the type 2 object marker including the steel post, 6" x 12" reflective panel, and hardware will be included in the contract unit price per each for "Type 2 Object Marker" for single-sided and "Type 2 Object Marker Back to Back" for back to back type 2 object markers.

December 23, 2019

<i>Published Date: 1st Qtr. 2020</i>	S D D O T	DELINEATION OF GUARDRAIL	PLATE NUMBER 632.40
			Sheet 4 of 4

The signs illustrated are not required if the work space is behind a barrier, more than 2 feet behind the curb, or 15 feet or more from the edge of any roadway.

The signs illustrated shall be used where there are distracting situations; such as: vehicles parked on shoulder, vehicles accessing the work site via the highway, and equipment traveling on or crossing the roadway to perform work operations.

The ROAD WORK AHEAD sign may be replaced with other appropriate signs, such as the SHOULDER WORK sign. The SHOULDER WORK sign may be used for work adjacent to the shoulder.

* If the work space is on a divided highway, an advance warning sign should also be placed on the left side of the directional roadway.

For short term, short duration, or mobile operations, all signs and channelizing devices may be eliminated if a vehicle with an activated flashing or revolving yellow light is used.

Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)
0 - 30	200
35 - 40	350
45 - 50	500
55	750
60 - 80	1000



A



April 15, 2015

<i>Published Date: 1st Qtr. 2020</i>	S D D O T	GUIDES FOR TRAFFIC CONTROL DEVICES WORK BEYOND THE SHOULDER	PLATE NUMBER 634.01
			Sheet 1 Of 1

Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A)	Spacing of Channelizing Devices (Feet) (G)
0 - 30	200	25
35 - 40	350	25
45	500	25
50	500	50
55	750	50
60 - 65	1000	50

- Flagger
- Channelizing Device

For low-volume traffic situations with short work zones on straight roadways where the flagger is visible to road users approaching from both directions, a single flagger may be used.

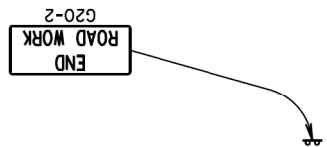
The ROAD WORK AHEAD and the END ROAD WORK signs may be omitted for short duration operations (1 hour or less).

For tack and/or flush seal operations, when flaggers are not being used, the FRESH OIL sign (W21-2) shall be displayed in advance of the liquid asphalt areas.

Flashing warning lights and/or flags may be used to call attention to the advance warning signs.

The channelizing devices shall be drums or 42" cones.

Channelizing devices are not required along the centerline adjacent to work area when pilot cars are utilized for escorting traffic through the work area.

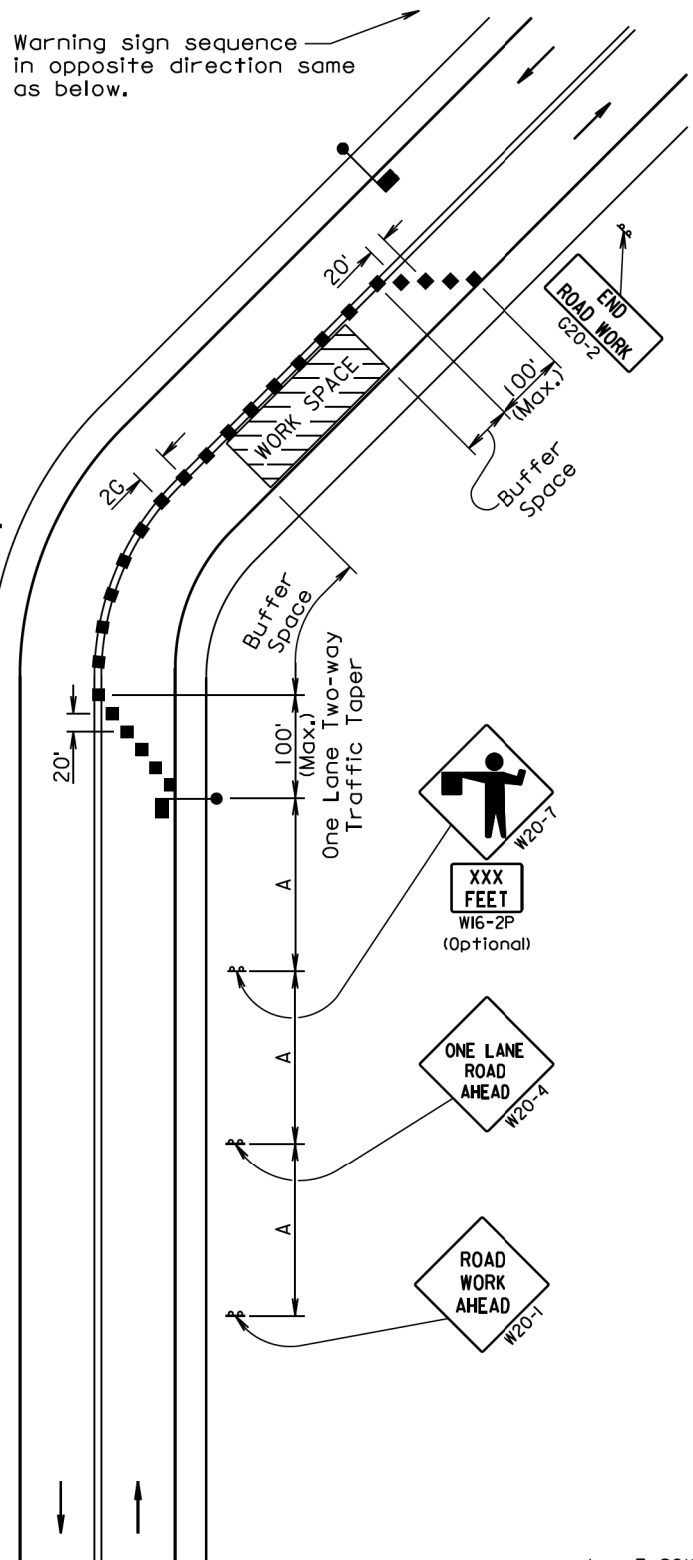


Channelizing devices and flaggers shall be used at intersecting roads to control intersecting road traffic as required.

The buffer space should be extended so that the two-way traffic taper is placed before a horizontal or vertical curve to provide adequate sight distance for the flagger and queue of stopped vehicles.

The length of A may be adjusted to fit field conditions.

Warning sign sequence in opposite direction same as below.



June 3, 2016

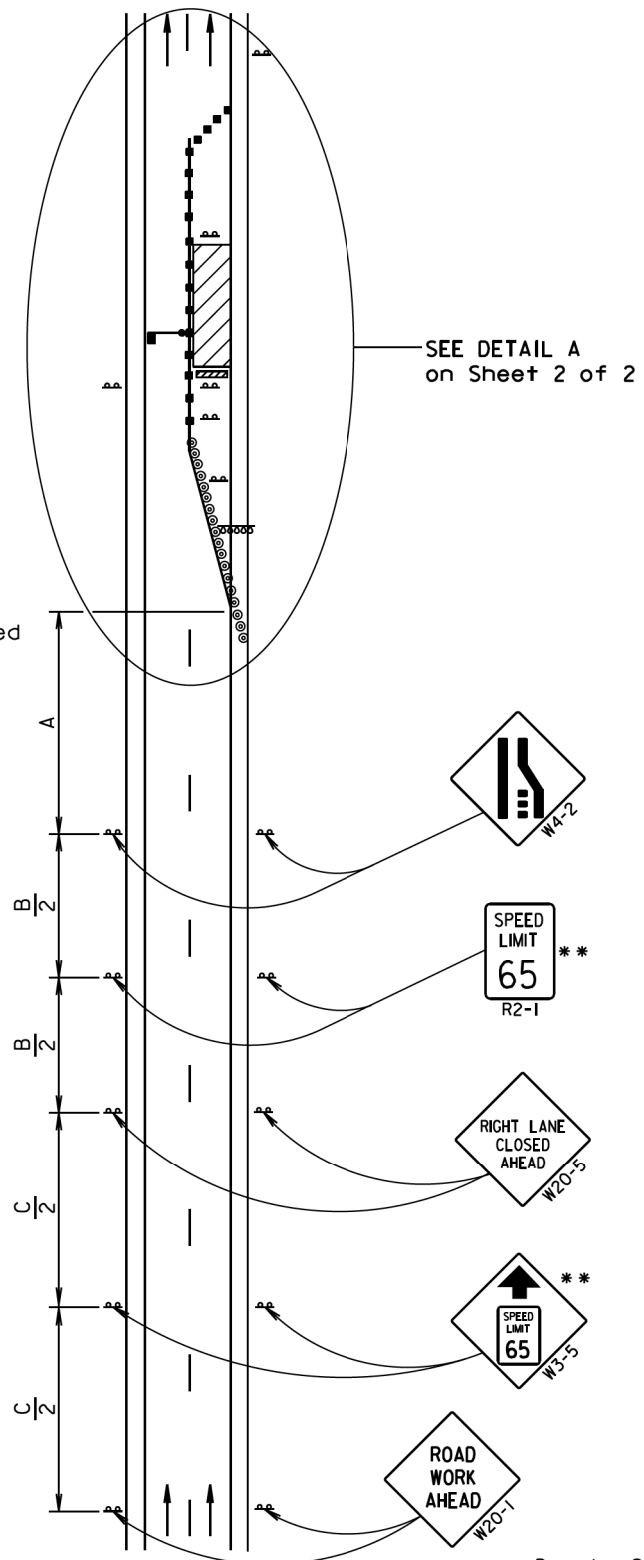
Published Date: 1st Qtr. 2020	S D D O T	GUIDES FOR TRAFFIC CONTROL DEVICES LANE CLOSURE WITH FLAGGER PROVIDED	PLATE NUMBER 634.23
			Sheet 1 of 1

Posted Speed Prior to Work (M.P.H.)	Spacing of Advance Warning Signs (Feet) (A) (B) (C)
0 - 30	200
35 - 40	350
45 - 50	500
55	750
60 - 65	1000
	(A) (B) (C)
70 - 80	1000 1500 2640

- ** Speed appropriate for location.
- Reflectorized Drum
- Channelizing Device

ROAD WORK AHEAD sign is only required in advance of the first lane closure.

High speed is defined as having a posted speed limit greater than 45 mph.



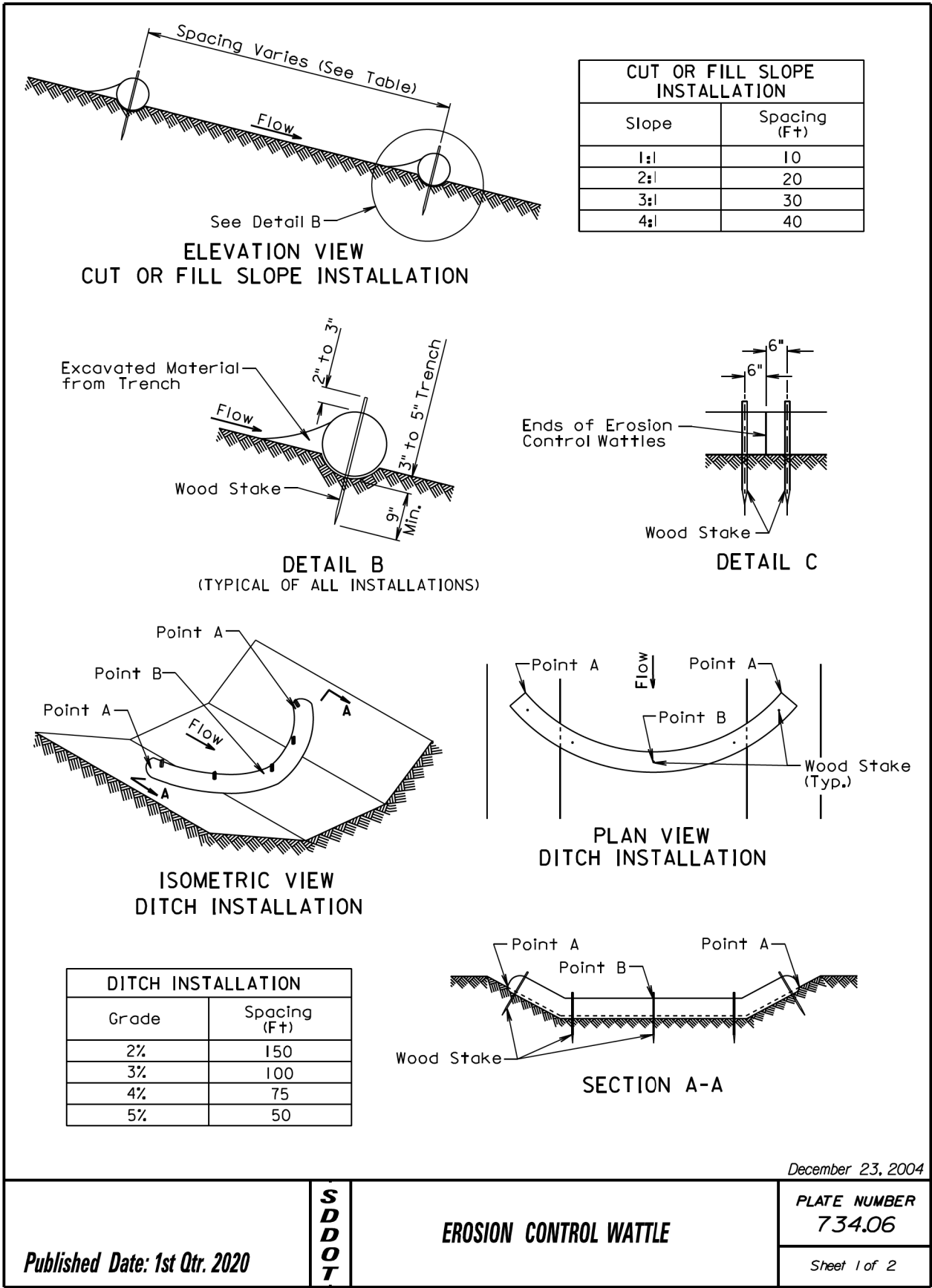
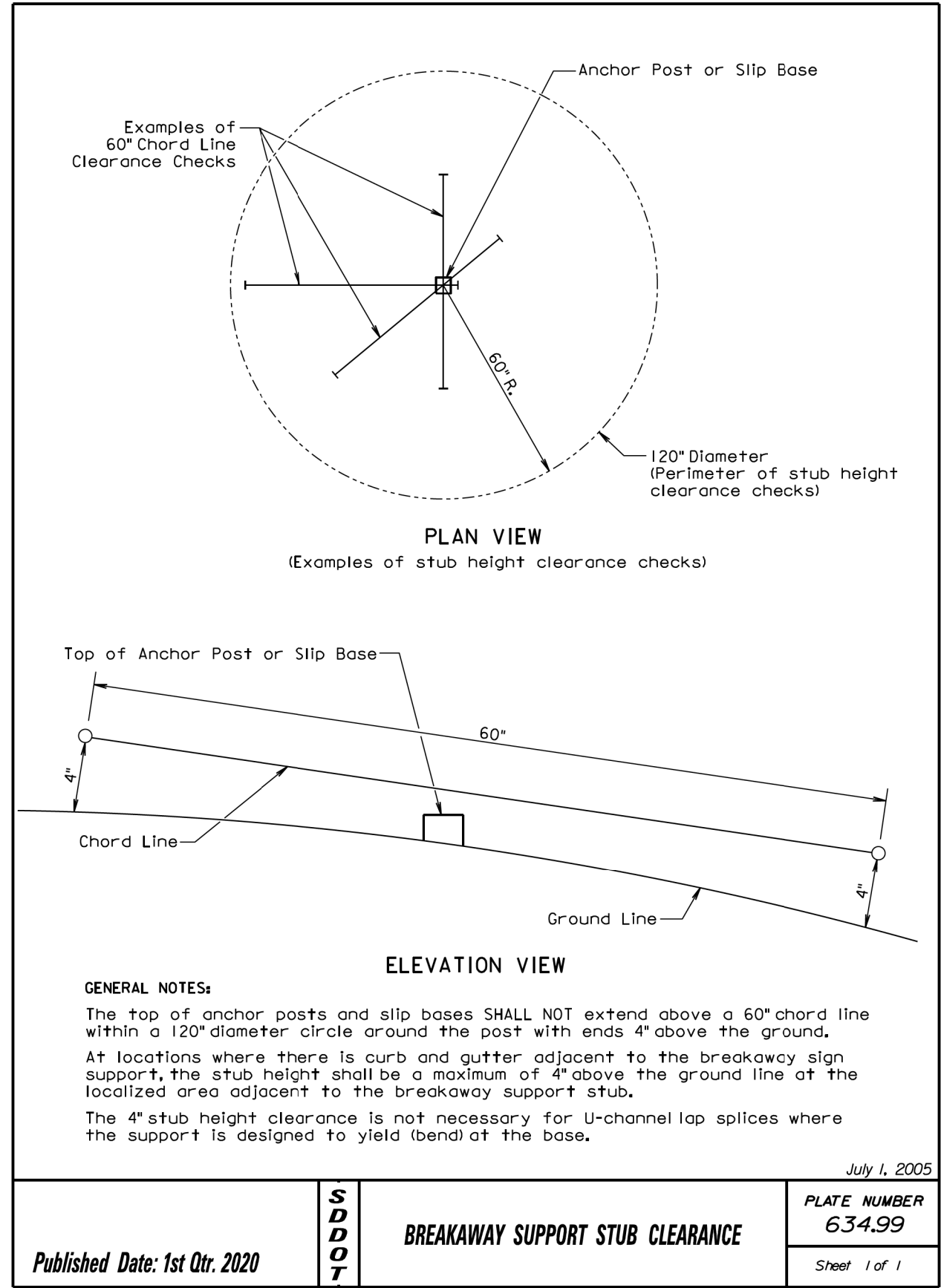
SEE DETAIL A on Sheet 2 of 2

December 23, 2019

Published Date: 1st Qtr. 2020	S D D O T	WORK ZONE SPEED REDUCTION FOR INTERSTATE AND HIGH SPEED MULTI-LANE HIGHWAYS	PLATE NUMBER 634.63
			Sheet 1 of 2

<table><tr><th>Posted Speed Prior to Work (M.P.H.)</th><th>Spacing of Channelizing Devices (Feet) (G)</th><th>Taper Length (Feet) (L)</th></tr><tr><td>0 - 30</td><td>25</td><td>180</td></tr><tr><td>35 - 40</td><td>25</td><td>320</td></tr><tr><td>45</td><td>25</td><td>600</td></tr><tr><td>50</td><td>50 *</td><td>600</td></tr><tr><td>55</td><td>50 *</td><td>660</td></tr><tr><td>60 - 65</td><td>50 *</td><td>780</td></tr><tr><td>70 - 80</td><td>50 *</td><td>960</td></tr></table> <p>* Spacing is 40' for 42" cones. **Speed appropriate for location. ***Use speed limit designated for the condition when workers are present in the work space. Signs will be covered or removed when workers are not present.</p> <p>● Flagger (As Necessary) ⊙ Reflectorized Drum ■ Channelizing Device</p> <p># The Work Space will be a minimum of 500' from the end of the taper.</p> <p>The FLAGGER sign will be used whenever there is a Flagger present.</p> <p>The channelizing devices will be 42" cones or drums.</p> <p>42" cones may be used in place of the drums shown in the taper if setup will not be used during night time hours.</p> <p>4" white temporary pavement marking tape for right lane closures, 4" yellow temporary pavement marking tape for left lane closures, or temporary raised pavement markers at 5' spacing will be installed in the taper when the lane is closed overnight, and along the tangent section where the skip lines do not exist and the lane is closed for more than 3 days.</p>	Posted Speed Prior to Work (M.P.H.)	Spacing of Channelizing Devices (Feet) (G)	Taper Length (Feet) (L)	0 - 30	25	180	35 - 40	25	320	45	25	600	50	50 *	600	55	50 *	660	60 - 65	50 *	780	70 - 80	50 *	960		<p>December 23, 2019</p> <table><tr><td rowspan="2">SDOT</td><td rowspan="2">WORK ZONE SPEED REDUCTION FOR INTERSTATE AND HIGH SPEED MULTI-LANE HIGHWAYS</td><td>PLATE NUMBER 634.63</td></tr><tr><td>Sheet 2 of 2</td></tr></table> <p>Published Date: 1st Qtr. 2020</p>	SDOT	WORK ZONE SPEED REDUCTION FOR INTERSTATE AND HIGH SPEED MULTI-LANE HIGHWAYS	PLATE NUMBER 634.63	Sheet 2 of 2
Posted Speed Prior to Work (M.P.H.)	Spacing of Channelizing Devices (Feet) (G)	Taper Length (Feet) (L)																												
0 - 30	25	180																												
35 - 40	25	320																												
45	25	600																												
50	50 *	600																												
55	50 *	660																												
60 - 65	50 *	780																												
70 - 80	50 *	960																												
SDOT	WORK ZONE SPEED REDUCTION FOR INTERSTATE AND HIGH SPEED MULTI-LANE HIGHWAYS	PLATE NUMBER 634.63																												
		Sheet 2 of 2																												

<p>RURAL DISTRICT</p>	<p>RURAL DISTRICT WITH SUPPLEMENTAL PLATE</p>				
<p>URBAN DISTRICT</p>	<p>RURAL DISTRICT 3 DAY MAXIMUM (Not applicable to regulatory signs)</p>				
<p>* If the bottom of supplemental plate is mounted lower than 7 feet above a pedestrian walkway, the supplemental plate should not project more than 4" into the pedestrian facility.</p>					
<table><tr><td rowspan="2">SDOT</td><td rowspan="2">CRASHWORTHY SIGN SUPPORTS (Typical Construction Signing)</td><td>PLATE NUMBER 634.85</td></tr><tr><td>Sheet 1 of 1</td></tr></table> <p>Published Date: 1st Qtr. 2020</p>	SDOT	CRASHWORTHY SIGN SUPPORTS (Typical Construction Signing)	PLATE NUMBER 634.85	Sheet 1 of 1	<p>September 22, 2014</p>
SDOT			CRASHWORTHY SIGN SUPPORTS (Typical Construction Signing)	PLATE NUMBER 634.85	
	Sheet 1 of 1				



STATE OF SOUTH DAKOTA	PROJECT	SHEET	TOTAL SHEETS
	000I-469	17	21

Plotting Date: 03/30/2020

GENERAL NOTES:

At cut or fill slope installations, wattles shall be installed along the contour and perpendicular to the water flow.

At ditch installations, point A must be higher than point B to ensure that water flows over the wattle and not around the ends.

The Contractor shall dig a 3" to 5" trench, install the wattle tightly in the trench so that daylight can not be seen under the wattle, and then compact the soil excavated from the trench against the wattle on the uphill side. See Detail B.

The stakes shall be 1"x2" or 2"x2" wood stakes, however, other types of stakes such as rebar may be used only if approved by the Engineer. The stakes shall be placed 6" from the ends of the wattles and the spacing of the stakes along the wattles shall be 3' to 4'.

Where installing running lengths of wattles, the Contractor shall butt the second wattle tightly against the first and shall not overlap the ends. See Detail C.

The Contractor and Engineer shall inspect the erosion control wattles once every week and within 24 hours after every rainfall event greater than 1/2". The Contractor shall remove, dispose, or reshape the accumulated sediment when necessary as determined by the Engineer.

Sediment removal, disposal, or necessary shaping shall be as directed by the Engineer. All costs for removing accumulated sediment, disposal of sediment, and necessary shaping shall be incidental to the contract unit price per cubic yard for "Remove Sediment".

All costs for furnishing and installing the erosion control wattles including labor, equipment, and materials shall be incidental to the contract unit price per foot for the corresponding erosion control wattle bid item.

All costs for removing the erosion control wattle from the project including labor, equipment, and materials shall be incidental to the contract unit price per foot for "Remove Erosion Control Wattle".

December 23, 2004

Published Date: 1st Qtr. 2020	S D D O T	EROSION CONTROL WATTLE	PLATE NUMBER 734.06
			Sheet 2 of 2

